

**ATTACHMENTS FOR: AGENDA NO. 10/20
COUNCIL MEETING**

Meeting Date: Tuesday 25 August 2020
Location: Online Audio Visual Meeting
Time: 7.00pm

ATTACHMENTS FOR COUNCIL MEETING

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Planning Proposal

2-6 Chatham Road, West Ryde | February 2017

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1. Introduction

This Planning Proposal has been prepared by SAMA Design on behalf of Tesco Projects Pty Ltd to seek Council support to amend the height and floorspace controls of the Ryde Local Environment Plan 2012 for 2-6 Chatham Road, West Ryde.

The proposed amendment would enable the existing service station to be redeveloped in a form consistent with the intent of the zoning and strategic planning for the West Ryde Town Centre. Key outcomes would include:

- replaces a low amenity site with a high quality mixed use development;
- provides a continuous high quality commercial frontage to Chatham Rd, a key commercial street and entry point to the West Ryde town centre;
- increases housing diversity in the area with 1, 2 and 3 bedroom apartments to suit to the changing demographics and lifestyles as identified in the Ryde Local Planning Strategy;
- helps to deliver the increased housing targets set out in the North District Plan in accordance with the criteria for urban renewal;
- maximises the existing transport and civic infrastructure by increasing the provision of employment and housing located within walking distance of the town centre and high frequency bus and rail public transport; and
- provides a more compatible land use for transitioning between the town centre and residential areas.

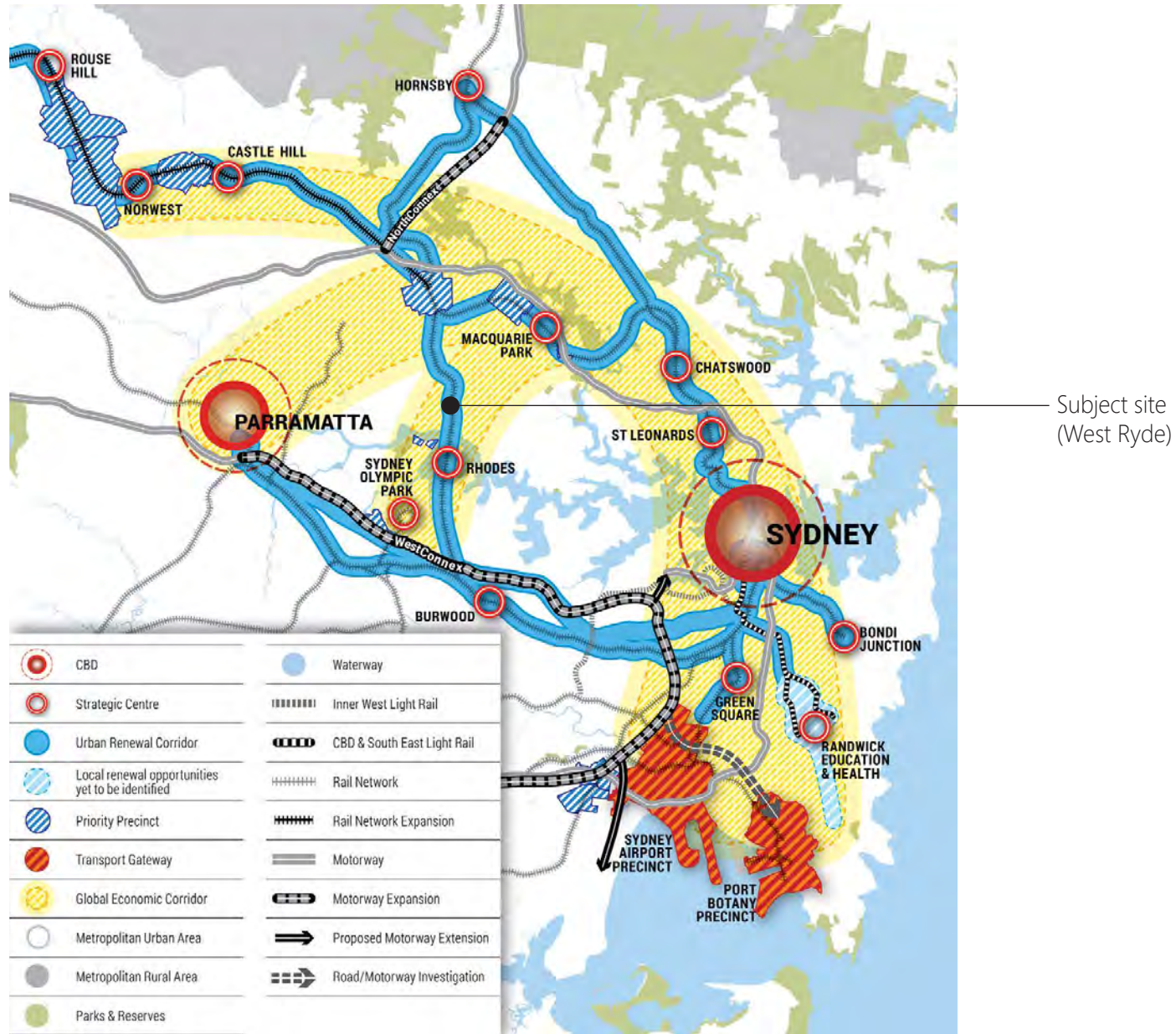
This report has been prepared having regard to the NSW Planning and Infrastructure's 'A Guide to Preparing Planning Proposals' and 'A Guide to Preparing Local Environmental Plans'. As required by Section 55 of the Environmental Planning and Assessment Act 1979 (EP&A Act), the report includes consideration of the following:

- Description of the subject site and context.
- Indicative site plan showing sufficient detail to demonstrate the suitability of the site for increased development potential.
- Statement of the objectives and intended outcomes of the proposal.
- Explanation of the provisions of the proposal.
- Summary of the justification of the proposal.
- Description of the stakeholder consultation undertaken to date to inform the preparation of this planning proposal request.

In support of the Planning Proposal, the following information is also provided:

- Traffic and Parking Assessment (Appendix A)
- Preliminary Site Investigation (Appendix B)
- Preliminary Flooding and Drainage Advice (Appendix C)

Overall the report demonstrates the urban design and strategic merits of the proposed LEP amendments and consistency with state and local policies, SEPPs and Ministerial Directions.



Subject site
(West Ryde)

Figure 1. Strategic Plan for Central and Northern Sydney in *A Plan for Growing Sydney* (2014).

1.1 Strategic Context

A PLAN FOR GROWING SYDNEY (2014)

This Plan sets out the overarching land use strategy to accommodate population growth and infrastructure in the Sydney metropolitan area over the next 20 years while creating a high amenity and liveable city.

The Plan sets out four key goals supported by directions and actions. Goal 2 and 3 of the Plan focus on improved housing choice and community design achieved largely through strategic infill redevelopment. This Planning Proposal is consistent with the directions and actions of these goals:

- provision of housing choice in well-serviced locations, particularly at train stations and within transport corridors. The subject site is located within an identified Urban Renewal Corridor (refer Figure 1) and is within a 5 minute walk/ 400m of the West Ryde Train Station, fulfilling this criteria.
- provision of greater housing diversity within existing communities to suit changing household sizes, lifestyles and budgets. The Planning Proposal will enable the provision of 1, 2 and 3 bedroom apartments with walkable access to amenities and public transport, increasing the diversity of housing in an area that is predominantly detached housing.
- deliver a healthy built environment with walkable neighbourhoods, mixed uses and high amenity civic spaces. The Planning Proposal will enable a development form that provides a new civic space, active commercial street frontages with awnings over the footpath and an enhanced public domain that improves the overall town centre.

Refer to Section 5.2.1 for further discussion on this strategy.

THE PLANNING PRINCIPLES THAT WILL GUIDE HOW SYDNEY GROWS

PRINCIPLE 1: INCREASING HOUSING CHOICE AROUND ALL CENTRES THROUGH URBAN RENEWAL IN ESTABLISHED AREAS

Increasing housing close to centres and stations makes it easier to walk or cycle to shops or services; travel to work or other centres; reduces traffic congestion; and makes our neighbourhoods more community oriented.

Increasing the variety of housing available makes it easier for people to find a home that suits their lifestyle, household size and their budget.

Locating new housing in centres delivers a range of economic, environmental and social benefits to the community. Research by the Organisation for Economic Cooperation and Development (OECD) has similarly found that productivity benefits arise from a more compact city.

Planning Principles in *A Plan for Growing Sydney (2014)*.

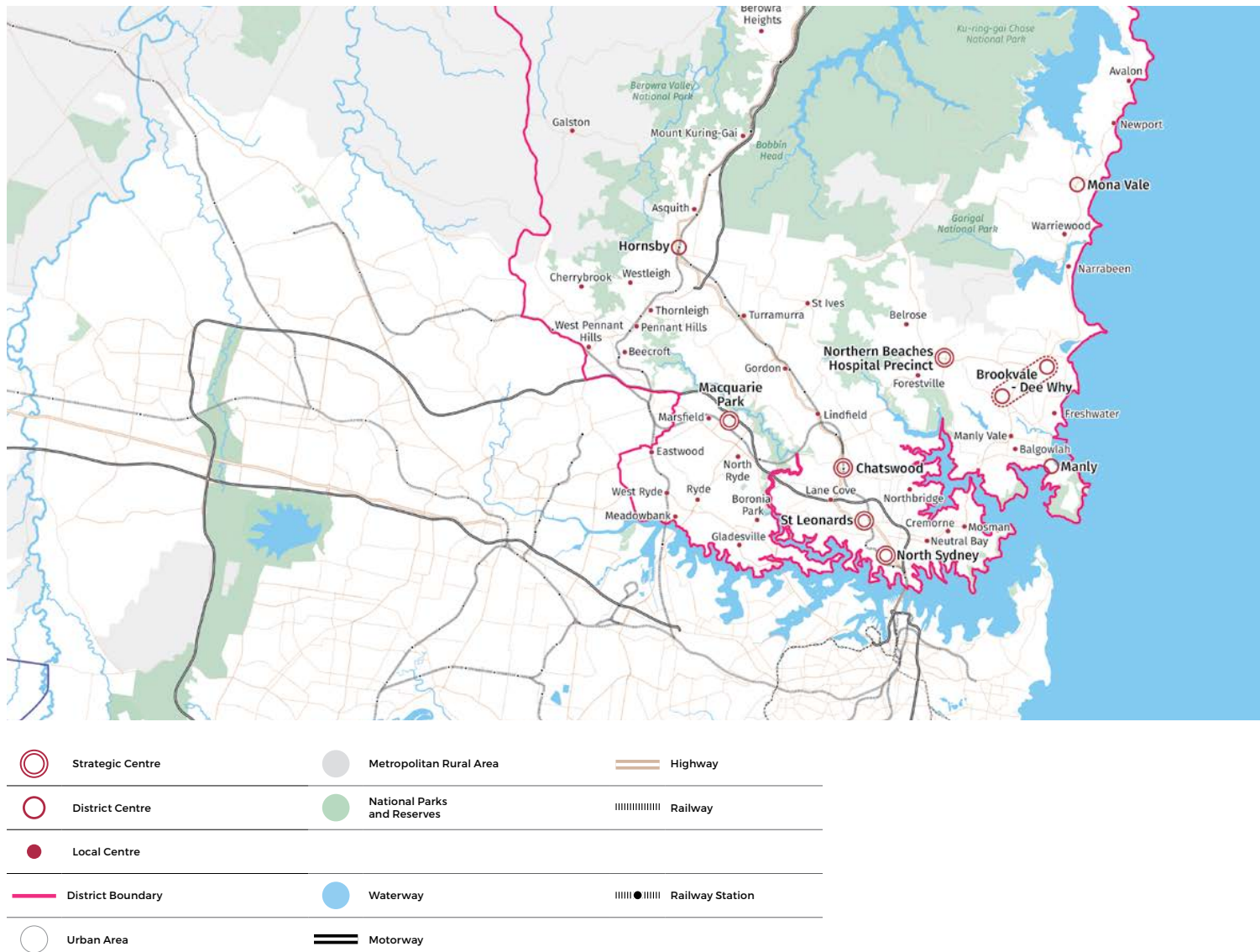


Figure 2. Draft North District Plan (2016).

DRAFT NORTH DISTRICT PLAN (2016)

The draft District Plan provides the 20-year vision, priorities and actions for the North District. Significantly, the Department of Planning and Environment has increased the 20 year forecast for additional dwellings in Greater Sydney by 9% to 725,000, with 97,000 dwellings targeted within the North District. The District Plan sets a shorter term target of 25,950 additional dwellings in the District over the next 5 years, with 7,600 planned within the City of Ryde.

The District Plan states in Action L3 that Ryde Council will *"investigate local opportunities to address demand and diversity in and around local centres and infill areas"*.

In addition to housing targets, the District Plan sets out a range of criteria for urban renewal which this Planning Proposal is consistent with:

- Alignment with investment in regional and district infrastructure.
- Accessibility to jobs, particularly strategic and district centres.
- Within walking distance of centres with regional transport.
- The feasibility of development, including financial viability across a range of housing configurations (one, two and three+ bedrooms) and consistency with market demand.
- Proximity to services including schools and health facilities.
- Consideration of heritage and cultural elements, visual impacts, natural elements such as flooding, special land uses and other environmental constraints.

- Consideration of local features such as topography, lot sizes, strata ownership and the transition between the different built forms.
- Delivery considerations such as staging, enabling infrastructure, upgrades or expansions of social infrastructure such as local schools, open space and community facilities.

Overall, the District Plan sets out the need for increased housing compared with the targets previously planned for, and articulates the strategic criteria for locating this growth.

As discussed in Section 5.2, the Planning Proposal is consistent with the District Plan.

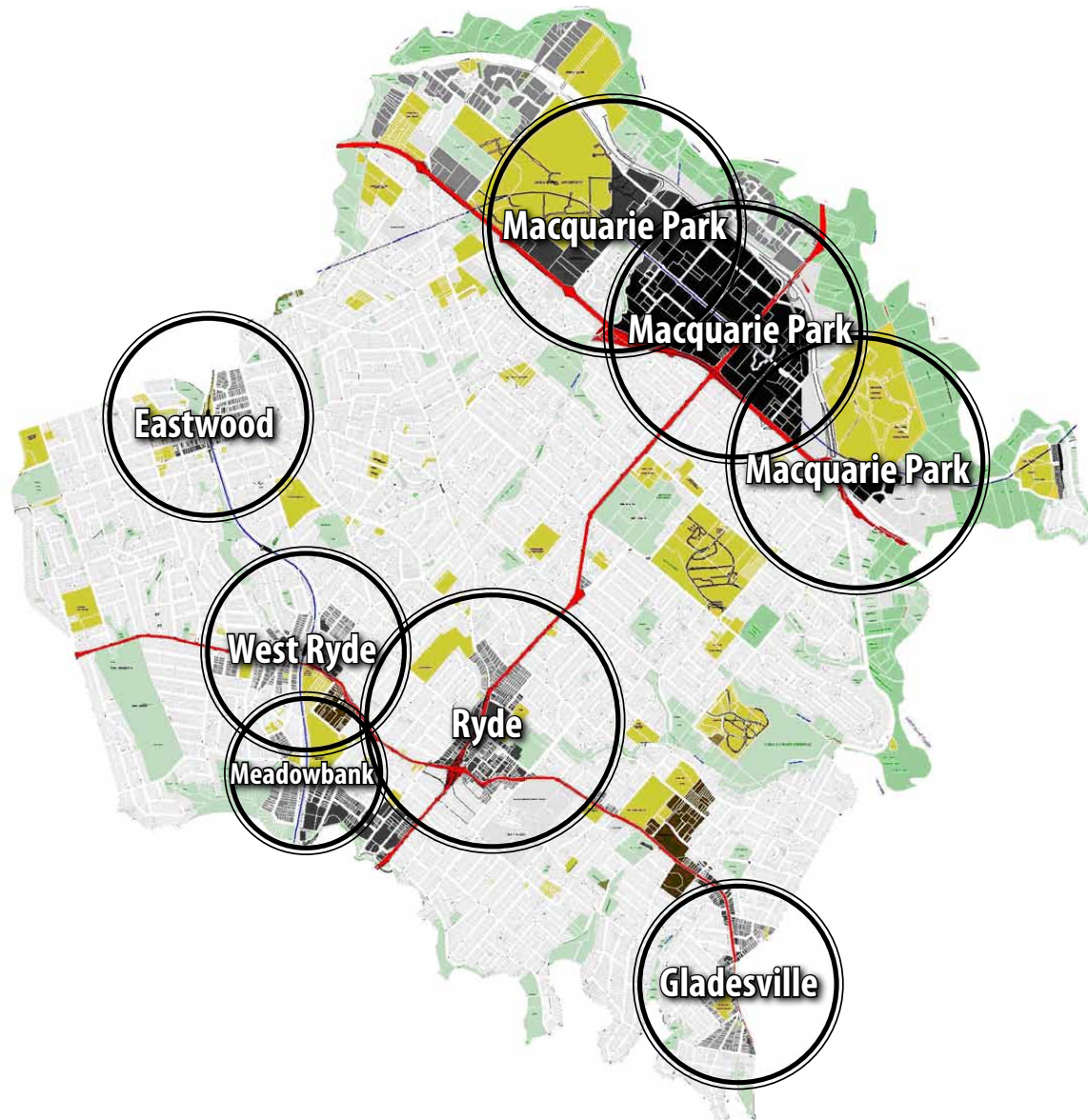


Figure 3. Key Centres and Corridors in *Ryde Local Planning Study (2010)*.

RYDE 2025 COMMUNITY STRATEGIC PLAN (2013)

This Plan sets the overall direction and long-term planning for the economic, social and environmental growth of the Ryde LGA and outlines seven key outcomes with associated goals and strategies, many of which are supported by this Planning Proposal, in particular:

- Outcome 1: City of Liveable Neighbourhoods - A range of well-planned clean and safe neighbourhoods, and public spaces, designed with a strong sense of identity and place.
- Outcome 2: City of Wellbeing - A healthy community, with all supported throughout their life by services, facilities and people.
- Outcome 3: City of Prosperity - Creating urban centres which champion business, innovation and technology to stimulate economic growth and local jobs.
- Outcome 5: City of Connections - Access and connection to, from and within the City of Ryde. Providing safe, reliable and affordable public and private travel, transport and communication infrastructure.

RYDE LOCAL PLANNING STUDY 2010

The Planning Study was prepared to guide the future growth of Ryde to 2021 and inform the preparation of the comprehensive Ryde LEP. It provides an analysis of the existing and future demographics and housing needs, and identifies strategic centres and corridors for growth. Notably, the provision of more housing supply and diversity in identified centres is a recurring theme.

The Study concludes with a range of Strategic Directions which are consistent with the objectives of the Planning Proposal, for example:

- continue to focus and support growth in centres through a mix of land uses and building types which provide for housing, jobs and services;
- strengthen the unique identity of individual centres;
- enhance the attractiveness of centres through improvements to the public domain;
- improve access to a range of housing types which meet the needs of present and future residents; and
- encourage walking, cycling and public transport use by focusing growth in centres, by improving public domain and by enhancing community safety.

Refer to Section 5.2.2 for further discussion on these local strategies.

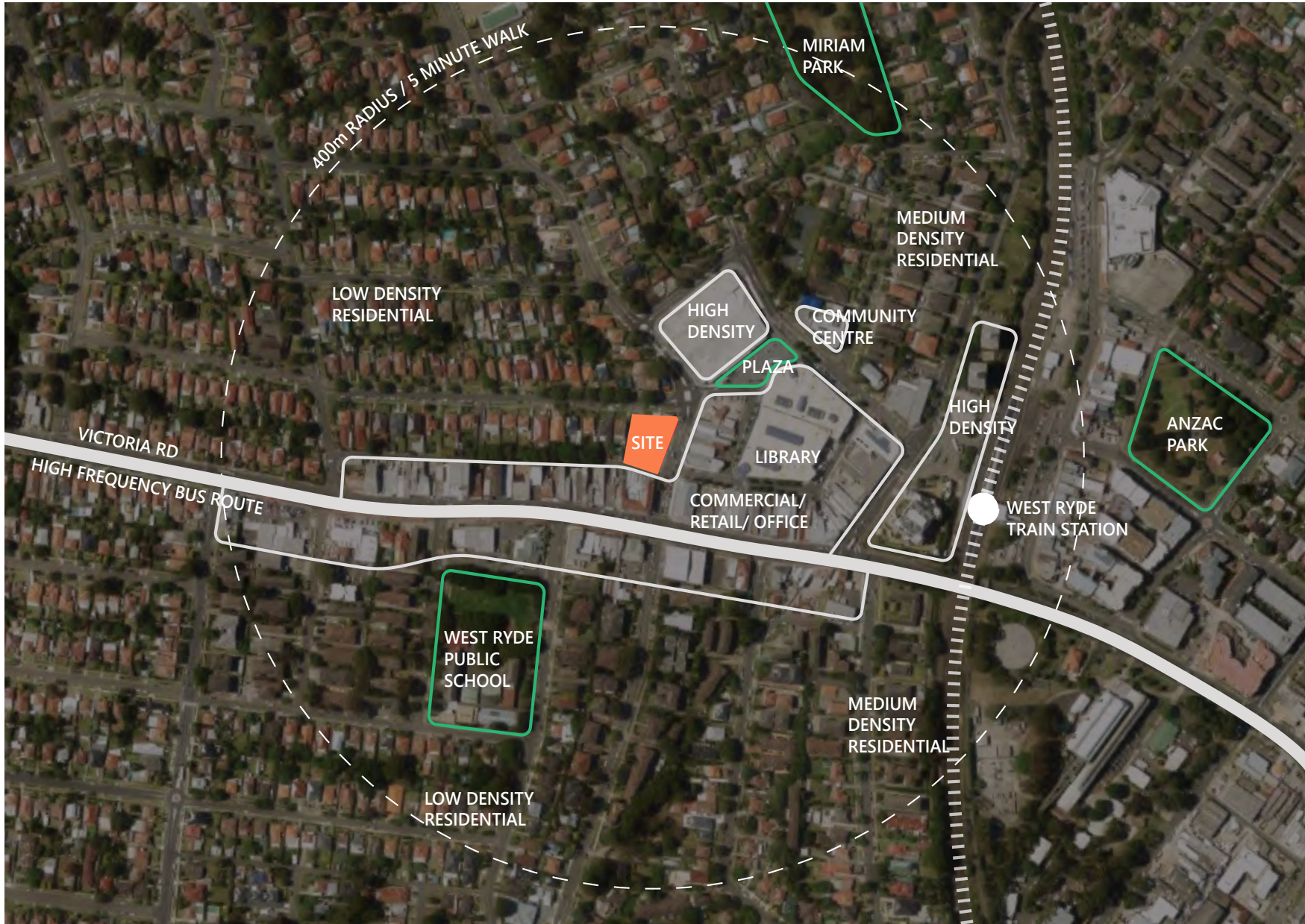


Figure 4. Local Context Plan.

1.2 Surrounding Context

LAND USES

The site forms part of the West Ryde Town Centre, a village centre, and is on the edge of Retail Core Precinct identified in the DCP.

The town centre contains 2 major supermarkets, a central plaza, library, community centre and commercial, retail, hotel and office uses. There are also a number of apartment buildings.

The site is approximately 300m from West Ryde Public School and 400m/ 5 minute walk from two large open spaces for recreation - Anzac Park and Miriam Park.

ADJACENT DEVELOPMENT

To the south and east of the site is commercial and retail development forming part of the West Ryde Town Centre. Buildings range in height from 2-10 storeys and are typically large footprint with zero setbacks and continuous frontages with awnings over the footpath. The site is within 50m of the main plaza in the town centre.

To the rear of the site (west) is low density residential development comprising single houses and a leafy suburban character. The property abutting the rear boundary of the site is Council-owned and currently used by Christian Community Aid as family day care centre.

PUBLIC TRANSPORT

The site is within 400m/ 5 minute walk of the West Ryde Train Station which provides frequent rail services to the Sydney CBD

and Strathfield (south) and Epping and Hornsby (north).

The site is also 50m from high frequency bus services on Victoria Road which provide access east-west to Parramatta and the Sydney CBD.

ACCESS

The site is bordered by three streets:

- Chatham Road, a commercial street and key entry point into the West Ryde Town Centre, providing access to underground parking for Coles and the apartments above. It has a single travel lane and kerbside parking in each direction. It is a mixed use commercial street including hotel, office, commercial, retail, restaurant and residential uses. The streetscape is characterised by zero setbacks, continuous frontages with awnings, wide footpaths, regular street tree planting and building heights ranging from 2 - 8 storeys.
- Dickson Avenue is a local residential street, also with a single travel lane and kerbside parking in each direction. The streetscape is characterised by 1-2 storey residential buildings with 6-8m front setbacks and regular street tree planting.
- Dickson Lane is a one-way (westbound) service lane to the commercial properties fronting Victoria Street, the main arterial road in the district.



Figure 5. Dickson Avenue viewed east - typical residential street with West Ryde apartments visible in background.



Figure 6. Chatham Road viewed north - typical commercial street in West Ryde.



Figure 7. Apartment developments in West Ryde ranging 8-10 storeys.

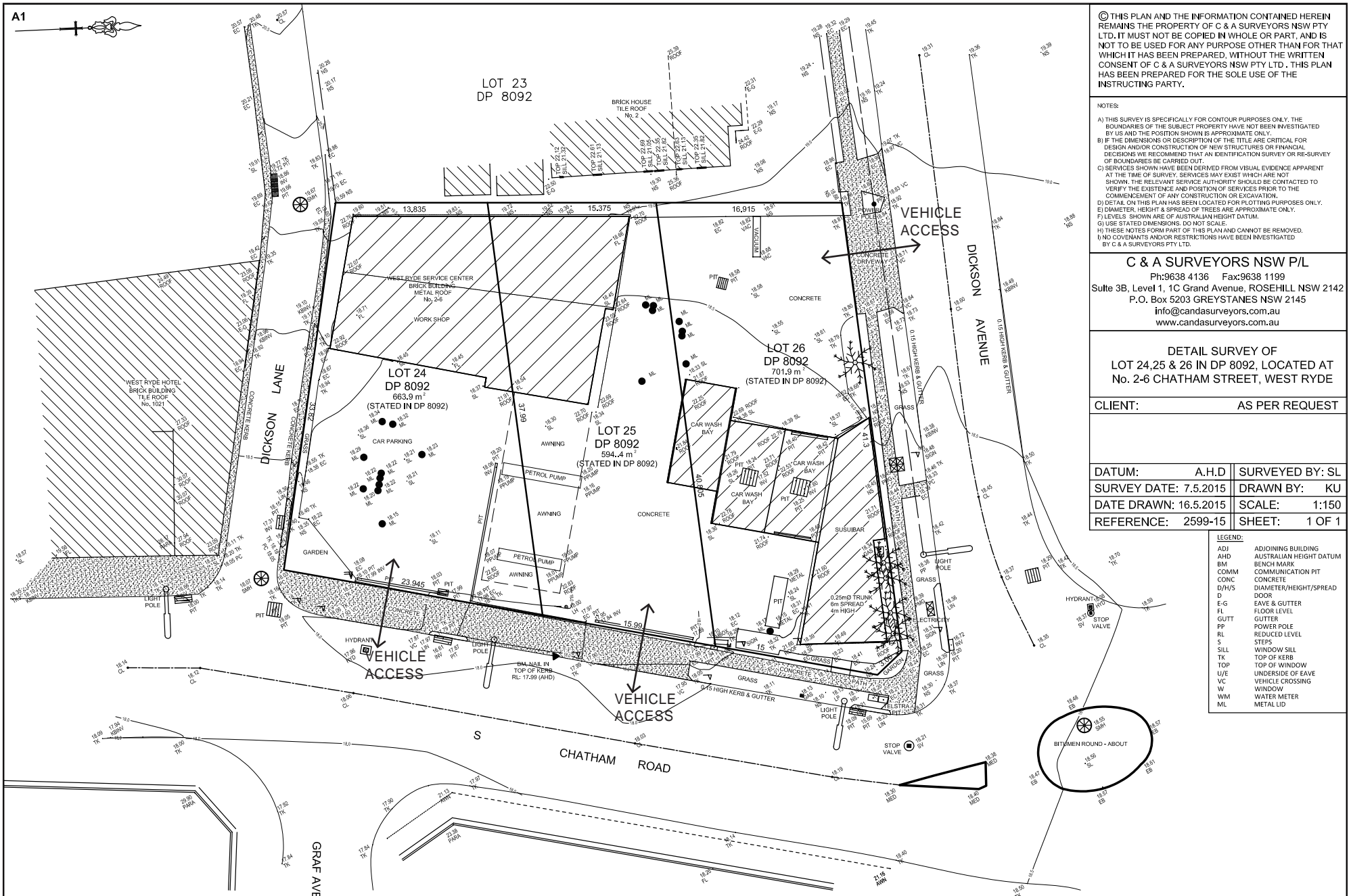




Figure 8. Subject site viewed from south-east corner with mixed-use apartment development to the right and Dickson Lane to the left.



Figure 9. Subject site viewed from the north-east corner.



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DETAIL SURVEY OF
LOT 24,25 & 26 IN DP 8092, LOCATED AT
No. 2-6 CHATHAM STREET, WEST RYDE

CLIENT: AS PER REQUEST

DATUM:	A.H.D	SURVEYED BY:	SL
SURVEY DATE:	7.5.2015	DRAWN BY:	KU
DATE DRAWN:	16.5.2015	SCALE:	1:150
REFERENCE:	2599-15	SHEET:	1 OF 1

LEGEND:

ADJONING BUILDING	ADJONING BUILDING
AHD	AUSTRALIAN HEIGHT DATUM
BM	BENCH MARK
COMM	COMMUNICATION PIT
CONC	CONCRETE
D/H/S	DIAMETER/HEIGHT/SPREAD
D	DOOR
E-G	EAVE & GUTTER
FLL	FLOOR LEVEL
GUTT	GUTTER
PP	POWER POLE
RL	REDUCED LEVEL
S	STEPS
SILL	WINDOW SILL
TK	TOP OF KERB
TOP	TOP OF WINDOW
U/E	UNDERSIDE OF EAVE
VC	VEHICLE CROSSING
W	WINDOW
WM	WATER METER
ML	METAL LID

Figure 10. Existing site plan (source: C&A Surveyors NSW, 2015).

1.3 The Site

DESCRIPTION

The subject site consists of 3 lots comprising a total area of 1,967sqm:

- 2 Chatham Road (Lot 24 on DP 8092)
- 4 Chatham Road (Lot 25 on DP 8092)
- 6 Chatham Road (Lot 26 on DP 8092)

The site has been developed as one consolidated parcel, with existing buildings overlapping the lot boundaries.

EXISTING DEVELOPMENT

There is currently a service station, car wash and restaurant operating on the site. The buildings are single storey and have large setbacks to Chatham Road, except for the restaurant which addresses the north-east corner of the site. The existing form presents a relatively low level of amenity to the town centre.

ACCESS

The site has three street frontages: Chatham Road, a commercial street; Dickson Avenue, a local residential street; and Dickson Lane, a one-way (westbound) laneway.

There are three vehicle access points to the site: two on Chatham Road and one on Dickson Avenue.

Footpaths are provided on Chatham Road and Dickson Avenue but are severely impacted by the three driveways.

Parking is provided at ground level to both the Chatham Road frontage and the Dickson Avenue frontage.

SITE CONDITIONS

The site is relatively flat with a level change of 1.6m (19.6m on the western boundary to 18.0m on Chatham Road).

There is no vegetation on the site or in the street verges, except for several medium sized palm trees on Dickson Avenue (located within the property boundary).

GEOTECHNICAL

The service station has been operating on the site since 1970 and previous to this the site was used for residential purposes. Preliminary investigations show there are six Underground Storage Tanks (USTs) on the site. Severe oil staining and cracking in the slab was observed around the USTs and the petrol bowsers. Deposited/ dumped soil of unknown quality and quantity and leaking chemical drums were also observed on the site. Refer to Figure 11 for site layout.

A preliminary conceptual site model (CSM) was derived for the site which identified potential contamination sources and concluded that there is the potential for contamination to be present on the site.

It is recommended that a Detailed Site Investigation (DSI) and Hazardous Materials Survey be undertaken to quantify any potential contamination and develop appropriate remedial and risk management measures. Due to the restricted access while the service station remains in operation, the DSI is recommended as part of the demolition/ redevelopment as a condition of development approval.

Refer to *Appendix B - Preliminary Site Investigation* for further information on geotechnical investigations.

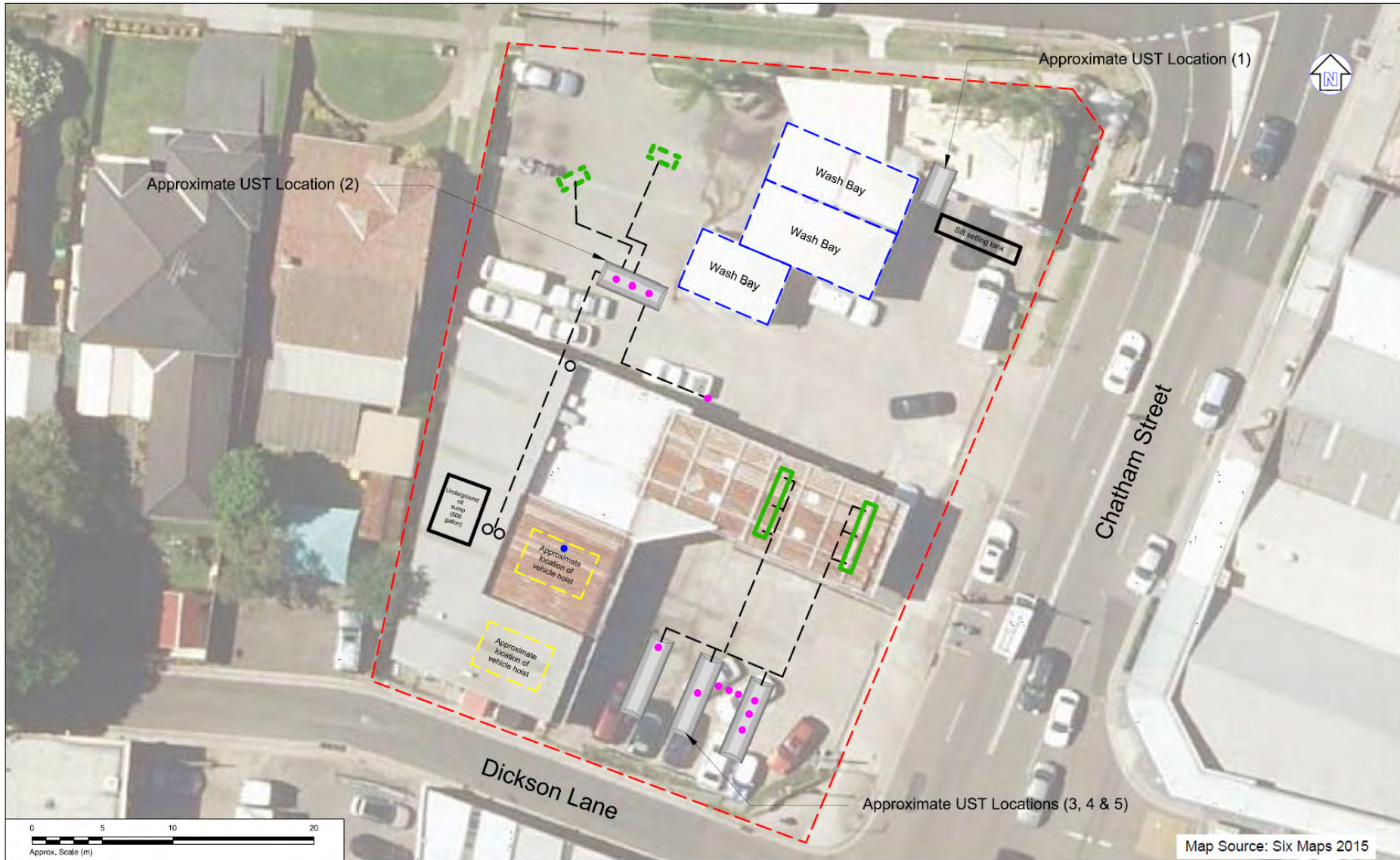


Figure 11. Site layout (source: Environmental Investigations Australia, 2016).

LEGEND	
	Approximate location of UST fill and dip points
	Observed steel plate (commonly used to cover USTs) (See Appendix B - Photograph 16)
	Approximate location of tank vents as seen during site inspection
	Approximate petrol bowser locations
	Approximate location of previous bowser locations
	Approximate UST locations
	(1) 1000 Gallon UST - Diesel Fuel (2) 11.7 KL UST - Premium Unleaded (3) 34 KL UST - E10 Fuel (4) 26 KL UST - Unleaded Fuel (5) 17 KL UST - Diesel Fuel
	Approximate location of UST infrastructure
	Approximate site boundary

STORMWATER

Under Ryde Council's *Parramatta River – Ryde Sub-catchments Flood Study and Floodplain Risk Management Plan 2013*, the site falls within a low to medium flood risk precinct (refer Figure 12).

The 1%AEP (Annual Exceedance Probability) flood depth is approximately 0.25m (refer Figure 13). Allowing for Council's minimum freeboard requirement of +0.5m, it is recommended that initial planning be based on the minimum ground floor level being +600mm above the adjacent top of kerb level along the building frontage.

The Probable Maximum Flood (PMF) flood depth ranges from 1m to over 2m. The lowest PMF flood depth is in the southwest corner of the site (Dickson Lane). It is recommended that the entry to any basement car parking be planned for this location.

The site falls within Council's Onsite Stormwater Detention (OSD) Zone 2 which means that the minimum Site Storage Requirement (SSR) is 255m³/ha. A minimum OSD volume of 50m³ will be required for this site.

Refer to *Appendix C - Preliminary Flood and Drainage Advice* for further information.

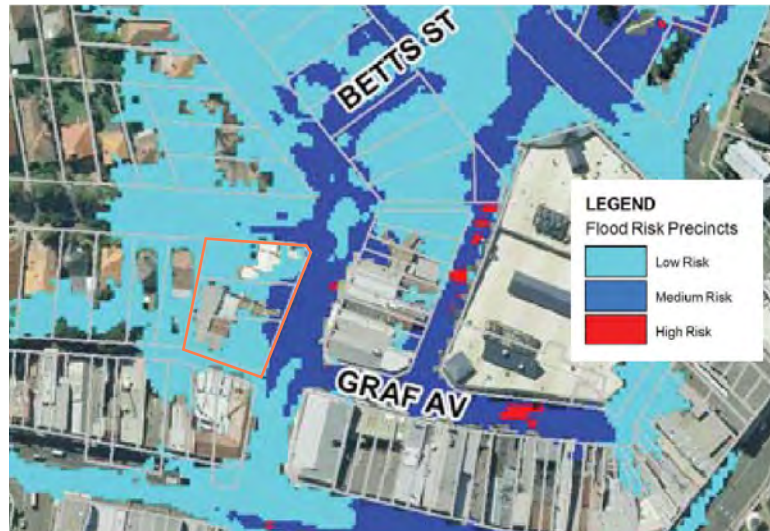
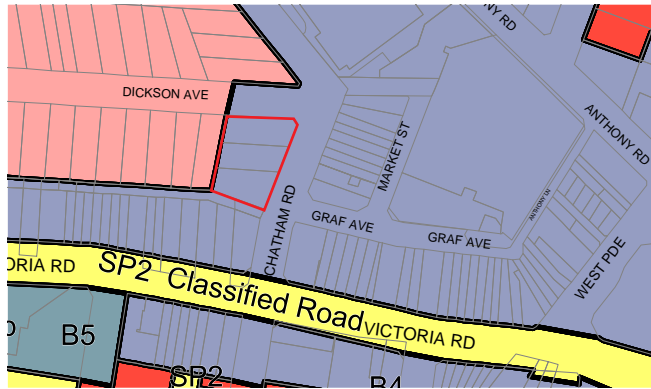


Figure 12. Flood Risk Precinct (source: *Parramatta River – Ryde Sub-catchments Flood Study and Floodplain Risk Management Plan 2013*).

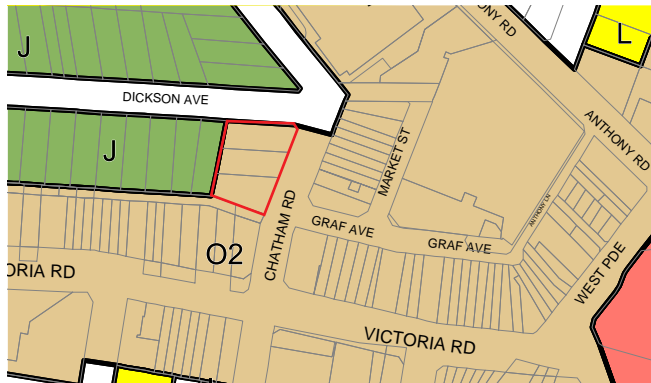


Figure 13. 1%AEP Peak Flood Depth (source: *Parramatta River – Ryde Sub-catchments Flood Study and Floodplain Risk Management Plan 2013*).



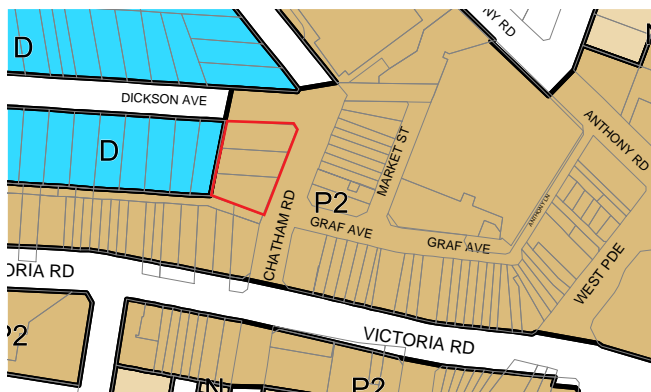
Zone

- B4 Mixed Use
- B5 Business Development
- R2 Low Density Residential



Maximum Building Height (m)

- J 9.5
- O2 15.5



Maximum Floor Space Ratio (n:1)

- P2 1.25
- D 0.50

Figure 14. Existing planning controls under the RLEP.

1.4 Existing Planning Controls

RYDE LOCAL ENVIRONMENT PLAN 2014 (RLEP)

Under the RLEP the site is zoned B4 Mixed Use. The purpose of the zone is to provide a mixture of compatible land uses and integrate suitable business, office, residential, retail and other development in accessible locations so as to maximise public transport patronage and encourage walking and cycling.

The site currently has a maximum building height of 15.5m and a maximum floorspace ratio of 1.25:1. This would enable a maximum of 4 storey development (allowing for a commercial ground floor with 4m floor to ceiling height).

The adjoining properties have the same zoning and building controls except for the rear boundary (west) which is zoned R2 Low Density Residential with a maximum building height of 9m and a maximum floorspace ratio of 0.5:1.

RYDE DEVELOPMENT CONTROL PLAN 2014 (DCP)

Section 4.3 of the DCP sets out the objectives for the West Ryde Town Centre including mixed use higher density development in and around the town centre, improved pedestrian amenity; a sense of community place; enhanced public spaces; and high quality built form.

The site is within the Retail Core Precinct which is identified for high intensity mixed use development with retail, commercial and civic uses with residential units above. The edge of the precinct performs a transitional role, with new development to be suitably designed to maintain the amenity of adjoining residential land uses. New development is also to maximise and improve visual and physical connections across the Precinct and to ensure community focal points in the private domain interact with the surrounding network of civic spaces and linkages.

The DCP sets out a range of more detailed development controls relating to building design, access and parking, and the public domain, which are addressed further in Section 5.2.2.

While the conceptual design shown in this Planning Proposal is generally consistent with the controls, a full compliance assessment will be undertaken at development stage when detailed design is undertaken.

RYDE PUBLIC DOMAIN TECHNICAL MANUAL (PDTM)

Section 4 of the PDTM provides an overview of the character areas and existing conditions of West Ryde and provides a range of public domain design controls such as landscaping, paving, streetscape, pedestrian networks, safety and open spaces.

The PDTM encourages future development to include new publicly accessible open spaces; active streets with open and attractive shopfronts; and consideration to views.

The Planning Proposal will enable the objectives of the PDTM to be achieved with the conversion of an inactive vehicle-dominated street frontage into an active, pedestrian-oriented commercial frontage. It will also be compliant with detailed aspects relating to the subject site include the provision of Plane Trees and granite pavers on Chatham Road.



2. Indicative Concept Design





Figure 15. Indicative concept plan.

1. Mixed-use apartments.
2. Apartment lobby entrance.
3. Commercial frontages with awnings.
4. Vehicle access ramp to basement parking.
5. Communal open space.
6. Roof garden.

Metrics (all figures are approximate):

- Site Area: 1967m²
- Gross Floor Area: 6027m²
- Commercial/ Retail Floorspace: 750sqm
- Apartments: 75
 - 24 x 1 bedroom (32%)
 - 48 x 2 bedroom (64%)
 - 3 x 3 bedroom (4%)
- Open Space: 680m² (34% of the site)
- Deep Soil Zones: 220m² (11% of the site)
- Street Setbacks: 4m to Chatham Rd, 0m others
- Rear setback: 6m minimum, 9m minimum for habitable rooms above 4 storeys
- Height: 7 storeys/ 24m
- Floorspace Ratio: 3:1
- Parking: 106 car bays in basement levels

0m 5 10 15 20



2.1 Indicative Concept Plan

A concept plan has been prepared to demonstrate the vision and design rationale for the site and to undertake preliminary testing of the building envelope and compliance with SEPP 65 and the Apartment Design Guide. The design is indicative only and is subject to refinement at the development stage when the detailed design will be resolved. However the concept design is important to help to envisage future development on the site and inform decisions on the appropriate planning controls for the site.

The concept is for a 7 storey mixed-use development with commercial/ retail ground floor and apartments above.

The ground floor is intended to provide continuous active open frontages to Chatham Rd such as cafes/ restaurants, small retail outlets and/or small office-based uses. As a result of discussions with Council a 4m setback is proposed to Chatham Rd which, combined seamlessly with the verge space, will provide an 8m wide linear space for landscaping, seating and lighting.

The combination of commercial frontages, awnings and widened landscape civic space will improve the physical street definition compared to the existing service station; create more street activity and interest; and improve the amenity of the streetscape and walkability of the town centre.

The indicative building design uses setbacks to define the apartment entries mid-block and also to break up the building mass. The design also shows the envisaged articulation and detailing of the facade with balcony and window placement, feature walls and greenery to ensure that the entire facade is interesting, of high quality and complements the street and town centre.

Internally a range of 1, 2 and 3 bedroom apartments are proposed with two lobby accesses to Chatham Road. A dual aspect east-west layout is proposed to suit the depth of the site and to provide good solar aspect to apartments, with 76% receiving a minimum 2hrs of sunlight.

A height of 7 storeys is proposed, providing a transition from the 8-10 storey apartment heights existing in the town centre, to the 5 storeys permitted on Victoria Road, and to the lower density residential areas further west. The proposal does not impact on SEPP 65 solar access requirements for adjoining properties and the elevation of the surrounding residential area combined with the presence of mature tree canopies means there is little visual impact associated with the proposed building height.

The western setback is proposed to be landscaped and used as communal open space for the apartments, also adding landscape amenity to the residential street (Dickson Ave).

All parking will be located underground in 3 basement levels, providing a total of 106 car bays plus bike racks. Access is via Dickson Lane to avoid impacting traffic in West Ryde town centre and on Dickson Ave, a residential street. The laneway access also avoids interruptions to the pedestrians paths with driveways.

2.2 Design Rationale

Note - concept design is indicative only.

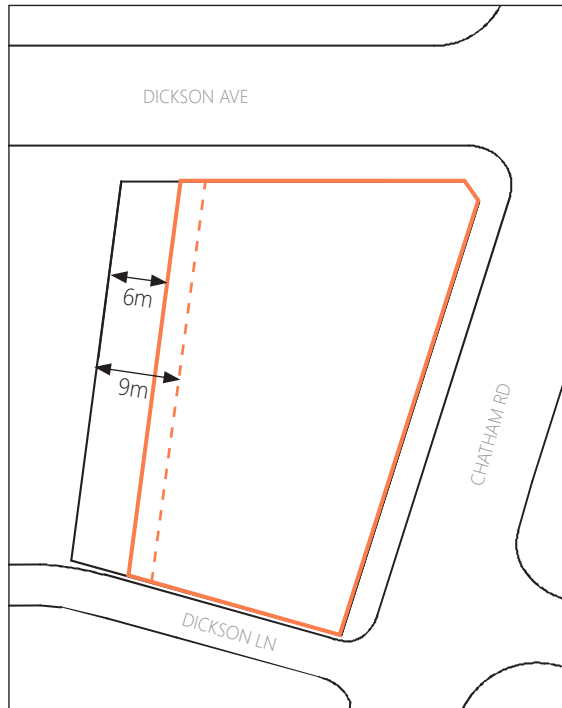


Figure 16. Setbacks.

0m street setbacks permitted.
 6m rear setback (habitable rooms G - L3).
 9m rear setback (habitable rooms L4 - L6).

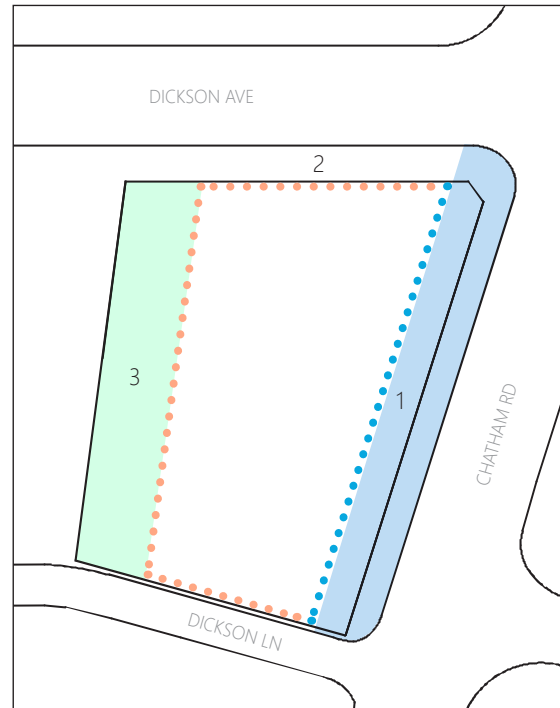


Figure 17. Land Use Interfaces

1. Commercial frontages to main street + additional 4m setback to increase civic space.
2. Residential frontages.
3. Landscaped rear setback as buffer.

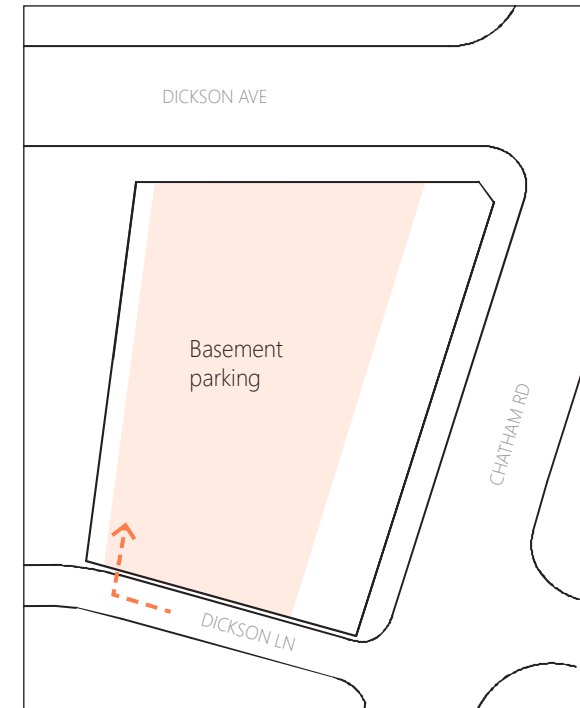


Figure 18. Access and Parking.

Vehicle access from laneway diverts traffic from town centre and residential streets and avoids driveways interrupting footpaths.
 106 car bays in 3 levels basement parking.

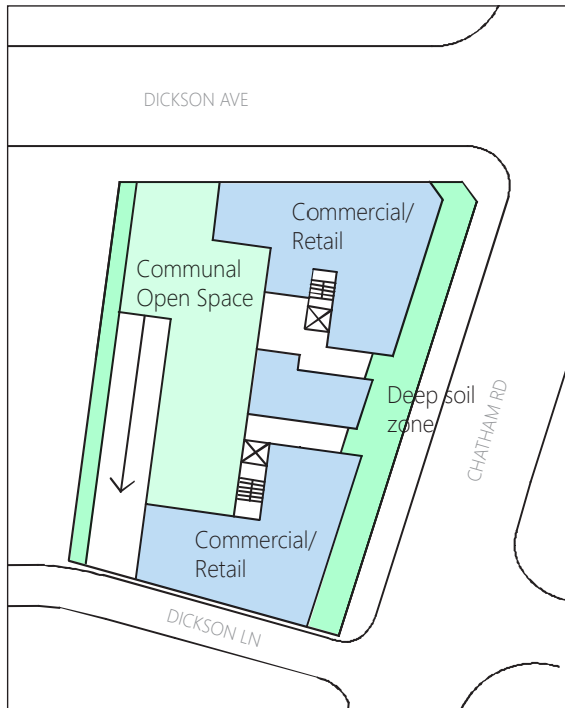


Figure 19. Conceptual ground floor.

750m² commercial/ retail floorspace
 680m² (34%) open space
 (includes. 220m² / 11% deep soil zone)
 FSR = 3:1



Figure 20. Conceptual level 1 - 3.

1 and 2 bedroom apartments.
 76% min 2hr solar access.
 64% cross ventilation.

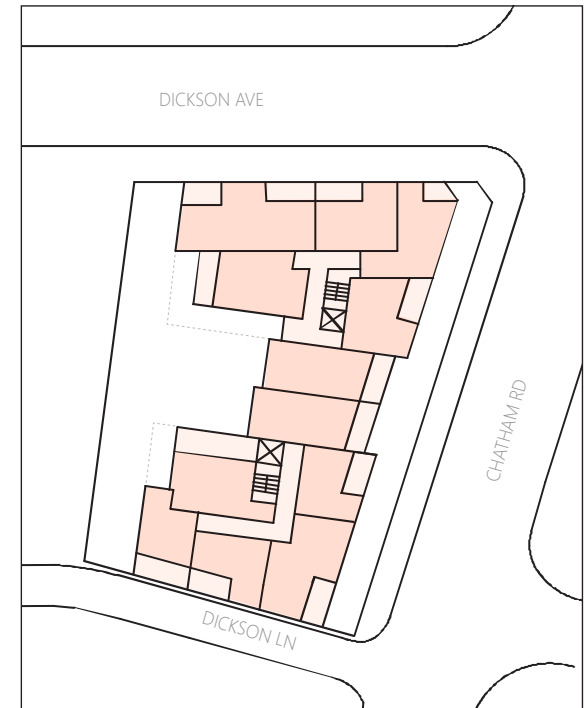


Figure 21. Conceptual level 4 - 6.

1, 2 and 3 bedroom apartments.
 76% min 2hr solar access.
 64% cross ventilation.

2.3 Indicative Massing



Figure 22. Indicative view



Figure 23. Indicative view



Figure 24. Indicative view

2.4 Initial Shadow Testing

Note - concept design is indicative only.

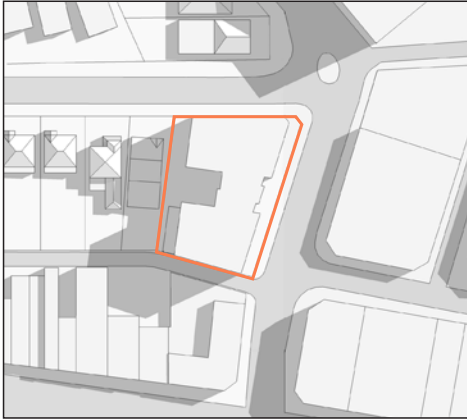


Figure 25. 21st March 9am.

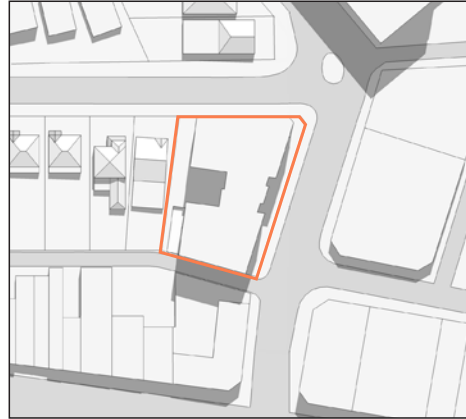


Figure 26. 21st March 12pm.

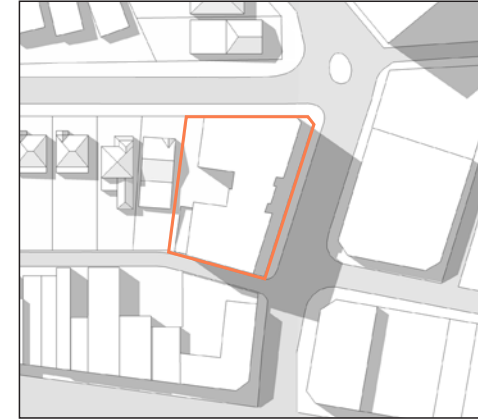


Figure 27. 21st March 3pm.

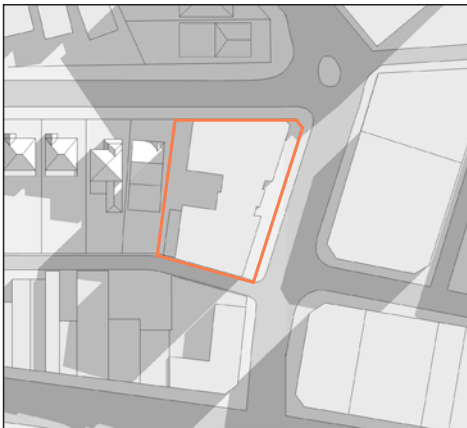


Figure 28. 21st June 9am.

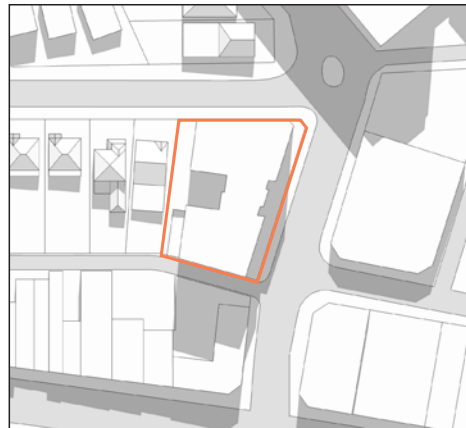


Figure 29. 21st June 12pm.



Figure 30. 21st June 3pm.

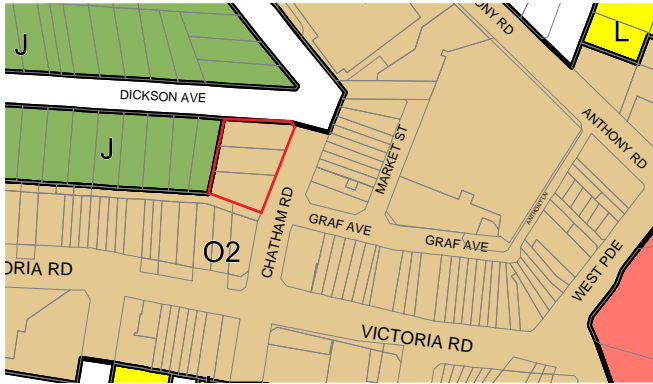


Figure 31. RLEP 2014 Building Height Map

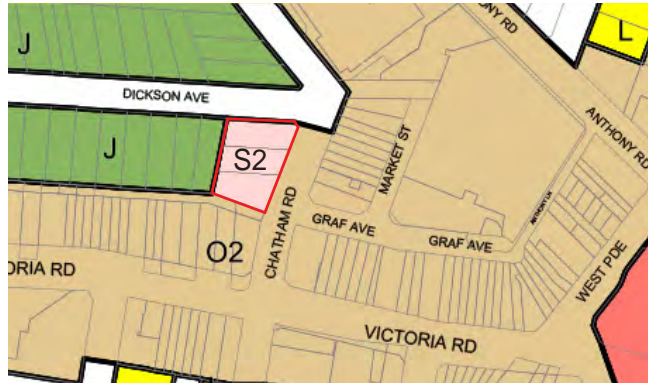


Figure 32. Proposed Building Height Map

Maximum Building Height (m)

J	9.5
O2	15.5
S2	24

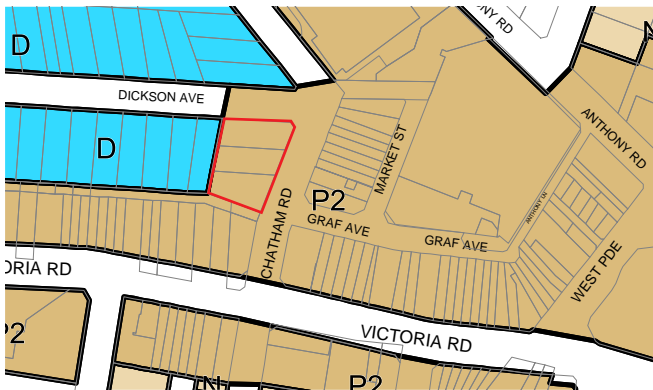


Figure 33. RLEP 2012 Floorspace Ratio Map

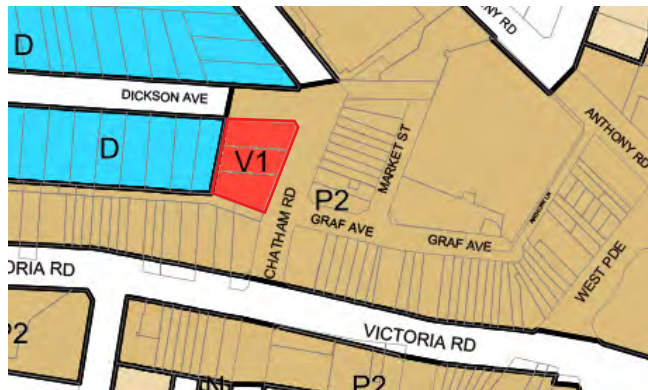


Figure 34. Proposed Floorspace Ratio Map

Maximum Floor Space Ratio (n:1)

P2	1.25
D	0.50
V1	3.00

3. Intended Objectives or Outcomes

The intended outcome of the Planning Proposal is to amend the height and floorspace ratios of 2-6 Chatham Road to enable mixed-use redevelopment of the site.

The key objectives for the redevelopment are:

- to provide a high quality mixed use development (commercial, retail and residential) that reflects the existing zoning and strategic planning for West Ryde town centre;
- to help revitalise West Ryde with quality buildings and high amenity public domain;
- to maximise the provision of housing within walking distance of public transport and the commercial and civic amenities of the town centre; and
- to be a commercially viable project.

4. Explanation of LEP Provisions

The proposed outcome will be achieved by:

- Amending the Ryde LEP 2012 Height of Buildings Map in accordance with the proposed height map, shown at Figure 32 which indicates a maximum permissible height of 24 metres on the site; and
- Amending the Ryde LEP 2012 Floor Space Ratio Map in accordance with the proposed floor space ratio map, shown at Figure 34 which indicates a maximum permissible floor space ratio of 3:1 on the site.

These provisions would allow for a 7 storey development as indicated in the concept plan (assumes 4.5m ground floor, 3.1m for apartments, plus roof plant).

5. Justification

5.1 Need for the Planning Proposal

5.1.1 IS THIS PLANNING PROPOSAL A RESULT OF ANY STRATEGIC STUDY OR REPORT?

The Planning Proposal not specifically the result of a strategic study or reports, however is consistent with the studies and reports that have been undertaken at metropolitan and local level:

- As part of the district planning process, the Department of Planning and Environment has increased the forecast for new housing in Greater Sydney by 9% to 725,000 dwellings by 2031. In the draft North District Plan there is a target for 7,600 new dwellings in the City of Ryde within the next 5 years. The *Ryde Local Planning Study (2010)* concluded the current planning controls could potentially deliver 10,787 additional dwellings in Ryde between 2009 - 2031. It could be assumed that by now, (after 7 years of development since the study was prepared) the capacity for new dwellings in Ryde is substantially less than this figure. This would mean that in order to deliver the 7,600 dwelling target set in the District Plan, all the potential housing growth identified to 2031 would need to be developed in the next 5 years (9 years earlier than expected), which is unlikely. This means that planning controls will need to be amended to accommodate increased housing.
- Both the *Ryde Local Planning Study (2010)* and the Department of Planning and Environment's *A Plan for*

Growing Sydney (2014) forecast an increase in the proportion of households with singles and couples with no children. This comprises both younger residents as well as older residents looking to downsize. The studies identify a need for more apartment housing in the district, which has predominantly detached dwellings, to cater for this changing demographic.

- Both metropolitan and local strategic planning seek to focus apartment development within existing centres and around train stations. This encourages compact, walkable town centres and the convenient and efficient use of public transport infrastructure.
- The site meets the requirements for the strategic location of urban renewal - it is located 400m/ 5 minute walk from West Ryde train station and 50m from frequent bus services on Victoria Road and is nestled among the core of West Ryde town centre with commercial, retail, and civic amenities on the doorstep. The site is already zoned for mixed-use development.
- The existing service station offers poor amenity for the town centre and for the adjoining residential area in terms of noise, odour, activity and visual appearance. The concept plan demonstrates how the site could be developed in a high quality form that enhances the amenity of the town centre and the public domain.

In summary, there is a demonstrated need for higher density development in the area and the site offers an appropriate location for this density. The Planning Proposal demonstrates conceptually how the site could be developed with a positive outcome.

5.1.2 IS THE PLANNING PROPOSAL THE BEST MEANS OF ACHIEVING THE OBJECTIVES OR INTENDED OUTCOMES, OR IS THERE A BETTER WAY?

While the existing zoning and strategic planning for the site are appropriate for redevelopment, the height and floorspace controls set out in the LEP restricts future development to a low scale and low yield building form. Higher density development is required in order to deliver a high quality mixed-use development, new public space and increased housing within walking distance of the town centre and public transport. Higher density is also required for the project to be feasible, particularly because of the cost to decommission the existing service station.

The current restrictions on development potential cannot be overcome through alternative mechanisms such as policy or DCP amendments, therefore an amendment to the RLEP 2012 is required to facilitate redevelopment.

The LGA-wide Ryde LEP 2012 has recently been implemented and is not due for review in the near future, therefore a stand-alone Planning Proposal is required in order to facilitate the redevelopment of this particular site and achieve a positive planning outcome in a timely manner.

It is proposed that the height and floorspace controls are amended by amending the relevant LEP maps for the site as shown in Figures 32 and 34.

An alternative approach is to include the site within clause '4.3A Exceptions to height of buildings' and '4.4A Exceptions to floorspace ratio', however this would create more provisions in the LEP and is considered more complicated than the mapping amendments.

5.2 Relationship to the Strategic Planning Framework

5.2.1 IS THE PLANNING PROPOSAL CONSISTENT WITH THE OBJECTIVES AND ACTIONS OF THE APPLICABLE REGIONAL OR SUB-REGIONAL STRATEGY?

Yes, the Planning Proposal is consistent with A Plan for Growing Sydney (2014) and the Draft North District Plan (2016).

A Plan for Growing Sydney (2014)

This Plan sets out the overarching land use strategy to accommodate population growth and infrastructure in the Sydney metropolitan area over the next 20 years while creating a high amenity and liveable city. The Plan forecasts that 624,000 additional dwellings will be required in Greater Sydney between 2014 - 2031.

The Plan sets out four key goals supported by directions and actions and the proposal to increase development potential near the train station fulfills numerous strategic objectives of the Plan:

Goal 2 relates to the provision of housing choice in well-serviced locations that meets the changing household needs, lifestyle choices, population growth and different household budgets.

Direction 2.2: Accelerate urban renewal across Sydney – providing homes closer to jobs.

A key action identified in the Plan is accelerating urban renewal and the provision of new housing across Sydney at train stations and transport corridors, providing homes closer to jobs. The Plan states the Government will prioritise the delivery of housing in or

near centres in the established urban areas to help more people to live where they want – close to jobs, services and transport.

The site is located within 400m/ 5 minute walk of the West Ryde Train Station and high frequency bus services and is within an identified Urban Renewal Corridor in the Plan (refer Figure 1). The site is within the core of the town centre with major supermarkets, library, commercial and civic amenities and employment all provided on the doorstep. Overall the site meets the criteria for prioritised delivery of housing as set out in the strategic objectives of the Plan.

Direction 2.3: Improve housing choice to suit different needs and lifestyles.

The Plan notes that currently the fastest growing households in Sydney are single person households and also that couple and single person households are forecast to have the greatest growth over the next 20 years. Despite this trend, the majority of Sydney's housing stock is detached dwellings. In addition, as the population ages, many people will choose to downsize their homes and most people will prefer to remain in their communities. This sets a strategic priority for the provision of greater housing diversity within existing communities. The Plan makes it clear of the Government's commitment to provide more housing in areas that take advantage of public transport, jobs and services and also to encourage innovative, well-designed, smaller homes to suit lifestyles and budgets.

The Planning Proposal will help to meet this strategic action by enabling the provision of 1, 2 and 3 bedroom apartments in a village town centre with walkable access to high frequency public transport, increasing the diversity of housing in an area that offers predominantly detached housing.

In addition to housing diversity, 4% of the new apartments will be designated as affordable housing to ensure the development contributes to Council's affordable housing targets and caters for those in need.

Goal 3 of the Plan is focussed on creating a great place to live with communities that are strong, healthy and well connected.

Direction 3.1: Revitalise existing suburbs.

The Plan recognises that revitalising suburbs can provide an opportunity to improve parks, civic squares and other public spaces such as the streets and states "Higher density development that is matched by local infrastructure improvements and good design enhances liveability. It allows more people to live close to work and to services, makes the best use of existing infrastructure and provides greater choice in transport around the city."

The proposal for increased housing density on the subject site is complemented with the provision of increased public space along Chatham Road for landscaping, seating and public art which, together with the new commercial frontages, will enhance the activity and amenity of this main street. It will also maximise the use of the existing transport and town centre infrastructure such as the train station, library and community centre by locating more residents within walking distance of these amenities.

Direction 3.3: Create healthy built environments.

Healthy built environments support active lifestyles through walkability and recreation opportunities. This is achieved with compact mixed-use centres; infrastructure such as footpaths and bike racks to make it easier to walk or cycle; active and safe streets that are inviting; and attractive public spaces with

landscaping and lighting.

The Planning Proposal will help to create a healthy built environment in West Ryde by enabling a development form that provides new footpaths and civic space; high amenity landscaping and lighting; active streets with continuous commercial frontages and awnings over the wide footpath to encourage walking; and new street tree planting to enhance the public domain. The combination of increased housing supply strategically positioned near public transport together with an improved public realm will set the foundations for a healthy, walkable community.

The Planning Proposal meets the criteria for the development of apartment housing in walkable distance of high frequency rail and bus services and the amenities West Ryde town centre; providing more housing choice in an area with predominantly detached dwellings; and revitalising the existing suburb with a well design built environment with quality buildings and high amenity public realm. Overall the development would have the effect of revitalising West Ryde and would deliver a net community benefit.

Draft North District Plan (2016)

The draft District Plan provides the 20-year vision, priorities and actions for the North District. Significantly, the Department of Planning and Environment has increased the 20 year forecast for additional dwellings in Greater Sydney by 9% to 725,000, with 97,000 dwellings targeted within the North District. The District Plan sets a shorter term target of 25,950 additional dwellings in the District over the next 5 years, with 7,600 planned within the City of Ryde

To date planning in Ryde has been based on the target set in the *Draft Inner North Subregional Strategy (2007)* for 12,000 additional dwellings in Ryde from 2011-2031. This represented an average of 600 new dwellings per year. In order to achieve the new 5 year target set in the District Plan, an average of 1,520 new dwellings per year will be required, representing a significant increase in development activity.

Furthermore the *Ryde Local Planning Study (2010)* concluded the current planning controls could potentially deliver 10,787 additional dwellings in Ryde between 2009 - 2031.

It could be assumed that by now, (after 7 years of development since the study was prepared) the capacity for new dwellings in Ryde is substantially less than this figure. This would mean that in order to deliver the 7,600 dwelling target set in the District Plan, all the potential housing growth identified to 2031 would need to be developed in the next 5 years (9 years earlier than expected), which is unlikely.

This would suggest additional provision will need to be made for the development of additional housing. This Planning Proposal helps to achieve this by increasing housing supply in a well located area identified as being suitable for growth.

The District Plan states in Action L3 that Ryde Council will "*investigate local opportunities to address demand and diversity in and around local centres and infill areas*". The Planning Proposal is consistent with this requirement.

In addition to housing targets, the District Plan sets out a range of criteria for urban renewal which this Planning Proposal is consistent with:

- *Alignment with investment in regional and district infrastructure.*

The site is within 400m/ 5 minute walk of the West Ryde Station. The increase of housing with convenient walking distance of the station will encourage greater use of public transport and maximise the government's investment in the T1 rail line, and reduce the dependency on vehicles for commuting.

- *Accessibility to jobs, particularly strategic and district centres.*

The site is within walking distance of West Ryde Station, which is only two stations from Rhodes, a strategic centre with significant employment, including office floorspace and large health precinct. It currently contains 15,700 jobs, which is forecast to increase to 22,000 jobs by 2031.

The site also has high frequency bus services to the major employment hubs and strategic centres of Parramatta and Macquarie Park, which collectively contain 155,000 jobs, and are forecast to increase to 229,000 jobs by 2031.

- *Within walking distance of centres with regional transport.*

The site is within the heart of West Ryde town centre which contains commercial and civic amenities, as well as bus and rail services to the Macquarie Park, Parramatta and Rhodes.

- *The feasibility of development, including financial viability across a range of housing configurations (one, two and three+ bedrooms) and consistency with market demand.*

The Planning Proposal will facilitate the development of a range of 1, 2 and 3 bedroom apartments which are all in demand from the market in West Ryde. The development will also provide 4% affordable housing to increase the options for all income levels.

- *Proximity to services including schools and health facilities.*

The site is located in the heart of West Ryde town centre which

offers supermarkets, library and other amenities. The site is within walking distance of a primary school and high school and is within 2 stations, or an 11 minute drive to the major health precinct in Rhodes, which contains three hospitals.

- *Consideration of heritage and cultural elements, visual impacts, natural elements such as flooding, special land uses and other environmental constraints.*

The site and surrounds are not affected by heritage or cultural items, or environmental values. The site falls within a low to medium flood risk precinct. A preliminary stormwater assessment has been undertaken (refer Appendix C) which indicates that stormwater and flood mitigation can be satisfactorily addressed at the development stage.

The Planning Proposal would allow for a building the same height as currently exists opposite the site, and the elevation of the surrounding residential area combined with the presence of mature tree canopies means there will be minimal visual impact associated with the proposed building height. The western setback is also proposed to be landscaped and used as communal open space for the apartments, adding landscape buffer to adjoining sites. The redevelopment of the site will also add significant visual amenity to the street.

- *Consideration of local features such as topography, lot sizes, strata ownership and the transition between the different built forms.*

The site is within a low point in the natural topography, helping to minimise any visual impact of development. The site is under single ownership enabling rapid redevelopment. The proposal will complement a key commercial street in West Ryde with an active street edge and apartments above, without causing

unreasonable overshadowing on adjoining properties.

- *Delivery considerations such as staging, enabling infrastructure, upgrades or expansions of social infrastructure such as local schools, open space and community facilities.*

The site is development ready and the Planning Proposal will not require upgrades to infrastructure or community facilities.

Overall, the Planning Proposal is consistent with the District Plan.

5.2.2 IS THE PLANNING PROPOSAL CONSISTENT WITH COUNCIL'S LOCAL STRATEGY OR OTHER LOCAL STRATEGIC PLAN?

Yes the proposal is consistent with the *Ryde 2025 Community Strategic Plan (2013)*, the *Ryde Local Planning Study (2010)* and the *Ryde Development Control Plan (2014)* which incorporates the key aspects of the *West Ryde Town Centre Masterplan (2010)*.

Ryde 2025 Community Strategic Plan (2013)

This Plan sets the overall direction and long-term planning for the economic, social and environmental growth of the Ryde LGA and outlines seven key outcomes with associated goals and strategies, many of which are supported by this Planning Proposal.

Outcome 1: City of Liveable Neighbourhoods - A range of well-planned clean and safe neighbourhoods, and public spaces, designed with a strong sense of identity and place.

The proposal will help to revitalise West Ryde Town Centre by redeveloping a prominent low amenity site with a high quality landmark development. The redevelopment will 'complete' a key

commercial street, Chatham Road, with continuous mixed use frontages and a widened public domain with landscaping and public art. This is envisaged to become an important new main street in the town centre, contributing to the identity and point of difference of this village centre. In parallel the proposal will protect the amenity of the adjoining residential area with a more appropriate land use transition.

Outcome 2: City of Wellbeing - A healthy community, with all supported throughout their life by services, facilities and people.

The creation of more housing diversity, a more active and attractive streetscape will contribute to the vitality of West Ryde. Combined with the proximity to transport and town centre services and facilities, including library and community centre, the proposal will improve the walkability of West Ryde will encourage more active and engaged lifestyles for the existing and new residents.

Outcome 3: City of Prosperity - Creating urban centres which champion business, innovation and technology to stimulate economic growth and local jobs.

The proposal for a quality mixed use development and revitalised commercial street will lift the profile of West Ryde and attract new businesses and jobs to the area. The significant investment associated with this proposal will also send a positive market signal that could stimulate development on other vacant and deteriorating sites in West Ryde.

Outcome 5: City of Connections - Access and connection to, from and within the City of Ryde. Providing safe, reliable and affordable public and private travel, transport and communication infrastructure.

The proposal to provide more housing and commercial development within 400m of the train station will enable more residents and workers to easily and safely travel on public transport to and from West Ryde.

The proposal will enhance walkability of Chatham Road with active commercial frontages; awnings over the footpaths; and a wider landscape public realm providing a new opportunity for meeting and connecting in West Ryde.

The restriction of vehicle entries to the laneway will ensure no increased traffic within the town centre and residential streets, and will create a safe environment for pedestrians with no driveways crossing footpaths, compared to the 3 crossovers that currently exist on the site.

Ryde Local Planning Study (2010)

The Planning Study was prepared to guide the future growth of Ryde to 2021 and inform the preparation of the comprehensive Ryde LEP.

With regards to housing, the Study noted that by 2031 the predominate household type in Ryde will be lone person households (29.1% of all households), highlighting that housing diversity will be an important issue for the future. The Study reaffirmed that most housing growth should be concentrated in centres where there is good access to public transport. In this regard the increase of housing density on the subject site would be consistent with the Study.

In terms of Centres, West Ryde is designated a village centre, sitting below Ryde and Macquarie Park in hierarchy. The Study notes West Ryde will be an important place on Victoria Road and

will provide opportunities for new buildings and public domain improvements. The proposal for a new mixed use development and revitalised main street is consistent with the vision set out in the Study.

The Study notes that all town centres need to work towards achieving good urban design such as safe, convenient access, good public transport, attractive, well-used public spaces, ecologically sustainable development, community facilities and diverse housing types. A common challenge is the need to improve urban design and promote quality buildings in Ryde, particularly interfaces with neighbours and public areas.

This Proposal to transform the low amenity service station and car wash into a high quality mixed use development will help to achieve these objectives - it will improve the quality of the public domain, provide new housing opportunities, improve the interface to the adjoining residential area, and improve the overall amenity of the town centre.

The Planning Proposal will also fulfill numerous Strategic Directions of the Study which reinforce the suitability of the site for high density development:

- continue to focus and support growth in centres through a mix of land uses and building types which provide for housing, jobs and services.
- strengthen the unique identity of individual centres.
- enhance the attractiveness of centres through improvements to the public domain.
- improve access to a range of housing types which meet the needs of present and future residents.
- encourage walking, cycling and public transport use by

focusing growth in centres, by improving public domain and by enhancing community safety.

Ryde Development Control Plan (2014)

Section 4.3 of the DCP sets out the objectives and controls for West Ryde Town Centre and incorporates the key aspects of the West Ryde Town Centre Masterplan 2010. The Planning Proposal is consistent with the general objectives that apply to the town centre as summarised below:

Objective 1. Facilitate the creation of a convenient community and retail centre; 2. Encourage higher density development surrounding the town centre and provide transition to surrounding suburban areas; 3. Encourage new development and re-use of existing buildings to contain a mix of land uses.

The proposal is for a medium density commercial and residential mixed-use development that complements the West Ryde town centre and provides active frontages at street level as well as public domain improvements. This form of development is a superior transition to the adjoining residential area compared with the existing service station and car wash which emits noise and odour and is a visual blight on the streetscape.

The proposed 7 storey height is an appropriate transition from the 8-10 storeys that exists in the centre, to the 5 storeys permitted on Victoria Road. The concept plan shows a large landscaped setback to the adjoining lower density residential area which varies from 9m to 18m, protecting the amenity of residents and providing a suitable transition while achieving the strategic density objectives in the town centre.

Objective 5. Improve pedestrian amenity and develop a sense of community place; 6. Create a pedestrian oriented town centre

including active street frontages; 13. Encourage a built form that integrates with the existing public domain and pedestrian network.

The proposed redevelopment is highly integrated with the adjoining public realm to create a pedestrian oriented street. The concept provides a high quality continuous active frontages to Chatham Road with commercial and retail uses. The building will also offer awnings the full length of the frontage, and provides a wider front setback than required (4m, whereas a zero setback is permitted) which will include landscaping, seating, lighting and public art to create an attractive and walkable street. The building massing, facade detail, and window and balcony placement has also been designed to integrate with public domain, create visual interest and maintain passive surveillance to further assist in creating a safe and active street.

Objective 7. Increase the number of people living within walking distance of high frequency public transport services.

The proposal will create in the order of 80 new apartments on a site that currently provides no housing. The site is located less than 400m/ 5 minute walk from the West Ryde train station, and is 50m from Victoria Road which has high frequency bus services, making it an ideal location for higher density development.

Objective 8. Provide for safe and convenient vehicle access and parking.

All vehicle access will be provided from Dickson Lane, which services the site conveniently without increasing traffic within the town centre or surrounding residential streets. This will also ensure the pedestrian network on Chatham Rd and Dickson Ave is not interrupted by vehicle crossovers, where currently there are 3 driveway interruptions. All parking is located underground for

land use efficiency and to minimise any impact on amenity in the town centre.

Objective 9. Minimise risk of flooding to the town centre.

The site falls within a low to medium flood risk precinct. Residential and commercial development is allowed subject to Council's development controls, including those for flooding.

Stormwater will be accommodated onsite with a minimum Onsite Stormwater Detention (OSD) volume of 50m³, ensuring no increased risk of flooding in the town centre. Enviropods and bio-retention areas and/or stormfilter cartridges will be used to achieve the required pollutant reduction targets set out in Council's Water Sensitive Urban Design Policy.

Objective 12. Enhance existing open space to offer increased amenity to surrounding residential development and provide for safe, inviting, well used and attractive public spaces.

The concept plan shows a 4m setback (0m is permitted) which will create a wider public space along Chatham Road with landscaping, seating, lighting and public art. This space will be framed with cafes and commercial uses at ground level and the windows and balconies of the new apartments above will overlook the street, providing passive surveillance. The proposal will enhance the amenity of the streetscape and will contribute to a safer and inviting public domain in West Ryde.

Character

The DCP establishes several character precincts in West Ryde with varying objectives and controls. The subject site is located within the Retail Core Precinct which is identified for high intensity mixed use development with retail, commercial and civic uses with residential units above. The edge of the precinct performs a

transitional role, with new development to be suitably designed to maintain the amenity of adjoining residential land uses.

The Planning Proposal is consistent with the objectives of the character precinct by facilitating new mixed-use development of a scale appropriate for the town centre, while providing a transition to the adjoining residential area with a wide landscaped setback. The amenity of the residential area will also benefit from the redevelopment of the existing service station site with odour and noise to a residential land use.

Controls

The DCP also sets out a range of detailed development controls for the Precinct relating to building design, access and parking, and the public domain. The conceptual design shown in this Planning Proposal is consistent with the controls, for example:

- ✓ Encourage a vibrant and active Town Centre and improve the visual image of West Ryde;
- ✓ Encourage a mix of residential, commercial and retail land uses within the Town Centre;
- ✓ Ensure buildings situated on corner allotments provide for visual interest and address the intersections which they front;
- ✓ Ensure car parking provisions and servicing do not compromise active street frontage and desirable urban form outcomes;
- ✓ New buildings are to have street frontages built predominantly to the street alignment for the first 2 storeys (a 4m setback has been requested by Council);
- ✓ Active frontages should be achieved using one or a combination of the following uses at street level: retail shop front; entrance to a retail arcade; frontage to open space;

glazed entry to a commercial or residential lobby; cafe or restaurant; outdoor dining areas; and active office uses;

- ✓ Define and activate edges to streets and parks to create a sense of character, legibility, convenience and safety for users of the public domain;
- ✓ Ensure new buildings address streets and parks, in order to provide definition and casual surveillance of these spaces;
- ✓ Provide awnings that shelter pedestrians along all public streets and retail areas;
- ✓ Provide private open spaces that also contribute to outlook and enliven the streetscape;
- ✓ Ensure balconies are integrated into the design of buildings, function and respond to the local context and environment;
- ✓ Provide opportunities for overlooking to streets and public open space;
- ✓ Vehicle access to properties should be provided from lower order roads or rear lanes where possible;

A comprehensive compliance assessment against the DCP controls will be undertaken at development stage when the detailed design is resolved.

5.2.3 IS THE PLANNING PROPOSAL CONSISTENT WITH APPLICABLE STATE ENVIRONMENTAL PLANNING POLICIES?

Yes. An assessment of consistency of the proposed rezoning with the relevant State Environmental Planning Policies is summarised below.

State Environmental Planning Policy No. 55 (Remediation of Land)

SEPP 55 aims to promote the remediation of contaminated land and specifies considerations for the assessment of rezoning and development proposals.

A Stage 1 contamination investigation has been undertaken and is attached at Appendix B. The investigation shows there are six Underground Storage Tanks (USTs) on the site. Severe oil staining and cracking in the slab was observed around the USTs and the petrol bowsers. Deposited/ dumped soil of unknown quality and quantity and leaking chemical drums were also observed on the site.

A preliminary conceptual site model (CSM) was derived for the site which identified potential contamination sources and concluded that there is the potential for contamination to be present on the site.

The investigation recommends that a Detailed Site Investigation (DSI) and Hazardous Materials Survey be undertaken to quantify any potential contamination and develop appropriate remedial and risk management measures.

Due to the restricted access while the service station remains in operation, this DSI is recommended as part of the demolition/ redevelopment as a condition of development approval.

State Environmental Planning Policy No. 65 (Design Quality of Residential Apartment Development)

A preliminary assessment of the concept plan has been undertaken against the principles of SEPP 65 and the key elements of the Apartment Design Guide (refer Table 1). While the building design will be refined at development stage, this initial assessment demonstrates that the Planning Proposal could enable the site to be developed in accordance with the principles and key requirements of SEPP 65.

A comprehensive SEPP 65 assessment will be undertaken at development stage when the detailed design is resolved.

Table 1. Preliminary SEPP 65 Assessment

Requirement	Comment	Compliant
Principle 1: Context and Neighbourhood Character	The concept plan reflects the existing form of the West Ryde town centre with zero setbacks, continuous active street frontages, continuous awnings and a commercial/retail ground level with residential above. The concept also integrates with the existing public domain character in terms of paving, street trees, furniture and materials and colour palettes. Overall the development will help to strengthen the existing qualities of the village centre.	Yes
Principle 2: Built Form and Scale	The concept design uses setbacks and vertical elements and articulation to create an appropriate form for the site that prioritises the public domain. The layout is aligned to maintain key view corridors into streets and lanes and also create views to the new civic space. The building frames the civic space to make it the focal point. The height has been designed to avoid overshadowing of the adjoining properties yet complement the scale of the town centre and achieve the strategic planning outcomes for the centre.	Yes
Principle 3: Density	The proposed density of housing will increase the number of residents within walking distance of the town centre and civic facilities such as the library, as well as high frequency bus and rail public transport services. At the same time, the apartment sizes and layouts will provide a high level of amenity for the residents.	Yes

Principle 4: Sustainability	The concept plan achieves good access to natural lighting with no south facing apartments, and the majority of apartments have cross ventilation, maximising energy efficiency and comfort. Building materials, systems, and fixtures will be resolved in the detail design stage to achieve a high environmental performance.	Yes
Principle 5: Landscape	The concept design integrates built form and open space to create a focal landscaped civic space with commercial frontages at ground level and apartment balconies overlooking the space from above. The building frames the space and enhances activity and safety while the apartments benefit from the landscape amenity, demonstrating a positive social and environmental outcome.	Yes
Principle 6: Amenity	The concept design shows simple, legible apartment layouts with good access to natural light and ventilation, views from balconies and high level of amenity from the commercial ground floor and landscaped public space.	Yes
Principle 7: Safety	The concept design frames the landscaped public space with commercial frontages to promote activity and social interaction, a natural form of safety. Passive surveillance is provided day and night from the apartments above. The layout also provides clear lines of sight to Dickson Lane and the vehicle access point for surveillance of this space.	Yes
Principle 8: Housing Diversity and Social Interaction	The concept provides a mix of 1, 2 and 3 bedroom apartments, providing housing choice for different demographics, living needs and household budgets. The Ryde Planning Study indicates that by 2031 the predominate household type in Ryde will be lone person households, and the apartments will serve the future population in an area with predominantly detached dwellings. In addition to housing diversity, 4% of the new apartments will be designated as affordable housing to ensure the development contributes to Council's affordable housing targets and caters for those in need.	Yes
Principle 9: Aesthetics	The concept design uses simple proportions, vertical elements and articulation with balconies to create a well balanced building form, 'complete' the commercial streetscape, and create and frame a new civic space. Further detailing will be undertaken at development stage.	Yes
Building Separation	The concept plan provides a 6m setback to the adjoining property boundary with habitable rooms above 4 storeys setback 9m, which complies with the ADG.	Yes

Street Setbacks	While 0m street setbacks are permitted and generally desired for the commercial ground floor, a 4m setback is proposed to Chatham Rd as requested by Council to provide additional civic space on the main street.	Yes
Open Space	The concept plan shows 34% of the site area at ground level as communal open space, of which 11% is deep soil zone (in the front setback area), meeting the ADG requirements for 25% and 7-15% respectively. The open space receives passive surveillance from apartments and includes landscaping and seating in line with ADG. The shadow diagrams (Figures 25-30) demonstrate that the open space would receive more than 50% direct sunlight for a minimum of 2 hours between 9am and 3pm on 21 June as required by the ADG.	Yes
Apartment Size and Mix	The concept plan provides the following apartment mix: - 32% 1 bedroom apartments average size 55m ² (exceeds the minimum 50m ²) - 64% 2 bedroom apartments average size 74m ² (exceeds the minimum 70m ²) - 4% 3 bedroom apartments average size 105m ² (exceeds the minimum 90m ²)	Yes
Daylight access	In the concept plan 76% of apartments receive a minimum of 2 hours direct sunlight between 9am and 3pm in mid winter and only 8% receive 0hrs, which exceeds the ADG requirements of 70% and 15% respectively. Further optimisation of solar access will be undertaken at development stage.	Yes
Ventilation	64% of apartments in the concept plan have natural cross ventilation, exceeding the minimum 60% required.	Yes
Entries	The concept plan locates the lobby entrance to the apartments on Chatham Road to provide pedestrian access that connects and addresses the public domain.	Yes
Access	The concept plan provides a single vehicle access point from Dickson Lane to provide safe access that avoids conflicts between pedestrians and vehicles and maintains a high quality streetscape in line with the ADG.	Yes
Parking	The concept plan has been calculated to generate a requirement for a total of 106 car bays all of which can be accommodated in basement parking. The parking associated with commercial uses would be provided separately to the residential parking.	Yes

5.2.4 IS THE PLANNING PROPOSAL CONSISTENT WITH APPLICABLE MINISTERIAL DIRECTIONS (S.117 DIRECTIONS)?

Yes. The Planning Proposal is consistent with the applicable Section 117 Ministerial Directions as summarised below..

Direction 1.1 Business and Industrial Zones

The objectives of this direction are to:

- (a) encourage employment growth in suitable locations,
- (b) protect employment land in business and industrial zones, and
- (c) support the viability of identified strategic centres.

This direction applies when a relevant planning authority prepares a planning proposal that will affect land within an existing or proposed business or industrial zone (including the alteration of any existing business or industrial zone boundary).

A planning proposal must:

- (a) give effect to the objectives of this direction,
- (b) retain the areas and locations of existing business and industrial zones,
- (c) not reduce the total potential floor space area for employment uses and related public services in business zones,
- (d) not reduce the total potential floor space area for industrial uses in industrial zones, and
- (e) ensure that proposed new employment areas are in accordance with a strategy that is approved by the Director-

General of the Department of Planning.

This Planning Proposal is consistent with Direction 1.1 for the following reasons:

- It encourages employment growth by enabling more intensive employment generating uses to be developed (eg. commercial, retail, cafe, restaurants) compared to the existing service station and carwash. The development could also act as a catalyst for further redevelopment in the town centre.
- The site is located in an appropriate location for employment, the West Ryde town centre, which is an established village centre within walking distance of the train station and high frequency bus services.
- It protects the existing employment land by retaining the existing B4 Mixed Use zoning.
- It will not affect other village or strategic centres, and will not impact the role or function of West Ryde town centre as the total amount of commercial floorspace is relatively small (approximately 750sqm) and will cater for several small businesses.
- It does not reduce any potential for employment uses.

Direction 3.1 Residential Zones

The objectives of this direction are:

- (a) to encourage a variety and choice of housing types to provide for existing and future housing needs,
- (b) to make efficient use of existing infrastructure and services and ensure that new housing has appropriate access to

infrastructure and services, and

(c) to minimise the impact of residential development on the environment and resource lands.

This direction applies when a relevant planning authority prepares a planning proposal that will affect land within:

(a) an existing or proposed residential zone (including the alteration of any existing residential zone boundary),

(b) any other zone in which significant residential development is permitted or proposed to be permitted.

A planning proposal must include provisions that encourage the provision of housing that will:

(a) broaden the choice of building types and locations available in the housing market, and

(b) make more efficient use of existing infrastructure and services, and

(c) reduce the consumption of land for housing and associated urban development on the urban fringe, and

(d) be of good design.

A planning proposal must, in relation to land to which this direction applies:

(a) contain a requirement that residential development is not permitted until land is adequately serviced (or arrangements satisfactory to the council, or other appropriate authority, have been made to service it), and

(b) not contain provisions which will reduce the permissible residential density of land.

This Planning Proposal is consistent with Direction 3.1 for the following reasons:

- It will enable a mix of 1, 2 and 3 bedroom apartments to be developed, providing housing choice for different demographics, living needs and household budgets. The Ryde Planning Study indicates that by 2031 the predominate household type in Ryde will be lone person households, and the apartments will serve the future population in an area with predominantly detached dwellings.
- In addition to housing diversity, 4% of the new apartments will be designated as affordable housing to ensure the development contributes to Council's affordable housing targets and caters for those in need.
- It makes efficient use of the infrastructure and services by providing an intensification of residential development within 400m/ 5 minute walk of West Ryde train station, high frequency bus services, West Ryde Library, West Ryde Community Centre and West Ryde Public School. The proposal utilises existing services as its located within an established town centre.
- It reduces the consumption of land by intensifying residential development within an existing developed and serviced area that is close to transport, employment and a range of commercial, retail and civic uses.
- It facilitates good design - the concept design is consistent with SEPP 65 and the Apartment Design Guide, and the concept also improves the streetscape and public domain with the provision of new civic space, landscaping, and continuous active street frontages.
- Residential development would only be permitted when the

appropriate infrastructure services are ensured, including water, power, waste and drainage.

- The Planning Proposal does not reduce the permissible residential density of the site.

Direction 3.4 Integrating Land Use and Transport

The objective of this direction is to ensure that urban structures, building forms, land use locations, development designs, subdivision and street layouts achieve the following planning objectives:

- (a) improving access to housing, jobs and services by walking, cycling and public transport, and
- (b) increasing the choice of available transport and reducing dependence on cars, and
- (c) reducing travel demand including the number of trips generated by development and the distances travelled, especially by car, and
- (d) supporting the efficient and viable operation of public transport services, and
- (e) providing for the efficient movement of freight.

This direction applies when a relevant planning authority prepares a planning proposal that will create, alter or remove a zone or a provision relating to urban land, including land zoned for residential, business, industrial, village or tourist purposes.

The Planning Proposal is consistent with Direction 3.4 for the following reasons:

- The site is within 400m/ 5 minute walk of West Ryde train station and high frequency bus services, and is located on a designated cycle route. The site is within a village centre with employment and services including commercial, retail, civic and residential uses. The Proposal will enable more housing and residents on the site, therefore will make walking, cycling and public transport a very convenient form of access.
- The convenience created for use of these transport modes will reduce the dependence on cars for access to services and employment.
- It will also reduce the travel demand in terms of distance, as many services are within close proximity, and number of trips, because the mixed use centre supports multiple activities.
- The increase in housing supply within walking distance of the train station and high frequency bus services will help to maximise the use of these services and maintain their efficiency and viability.

Direction 6.1 Approval and Referral Requirements

The objective of this direction is to ensure that LEP provisions encourage the efficient and appropriate assessment of development.

This Planning Proposal is consistent with Direction 6.1 because it is not designated development and does not require the concurrence of the Department of Planning and Infrastructure.

Direction 6.3 Site Specific Provisions

The objective of this direction is to discourage unnecessarily restrictive site specific planning controls.

A planning proposal that will amend another environmental planning instrument in order to allow a particular development proposal to be carried out must either:

- (a) allow that land use to be carried out in the zone the land is situated on, or
- (b) rezone the site to an existing zone already applying in the environmental planning instrument that allows that land use without imposing any development standards or requirements in addition to those already contained in that zone, or
- (c) allow that land use on the relevant land without imposing any development standards or requirements in addition to those already contained in the principal environmental planning instrument being amended.

The Planning Proposal is consistent with Direction 6.3 for the following reasons:

- It retains the existing B4 Mixed Use zoning. The concept is for a mixed-use development consistent with the intent and permitted uses in this zone.
- It will not create restrictive site specific planning controls - it aims to actually reduce the restrictions of the existing planning controls.
- No additional standards or requirements are proposed in addition to those already contained in the RLEP (ie. maximum height and floorspace ratio).

5.3 Environmental, Social and Economic Impacts

5.3.1 IS THERE ANY LIKELIHOOD THAT CRITICAL HABITAT OR THREATENED SPECIES WILL BE ADVERSELY AFFECTED AS A RESULT OF THE PROPOSAL?

The site is developed and is completely devoid of vegetation and habitat. There is no foreseeable impact on any critical habitat or threatened species, populations or ecological communities as a result of the Planning Proposal.

5.3.2 ARE THERE ANY OTHER LIKELY ENVIRONMENTAL EFFECTS AS A RESULT OF THE PLANNING PROPOSAL AND HOW ARE THEY PROPOSED TO BE MANAGED?

Traffic and Access

A traffic and parking assessment was undertaken by MRCagney and is attached at Appendix A. The assessment calculated that the development is expected to generate 22-36 peak hour vehicle trips (including commercial and residential uses). At 1 trip per 2-3 minutes, the assessment concluded that traffic impact is expected to be negligible and does not warrant further intersection analysis.

As a further measure, vehicle access to the site is proposed from Dickson Lane, diverting traffic associated with the development away from the town centre and residential streets thus mitigating any traffic impact.

Contamination

A Preliminary Site Investigation was undertaken by Environmental Investigations Australia and is provided at Attachment B. The investigation found the service station has been operating on the site since 1970 and previous to this the site was used for residential purposes. There are six Underground Storage Tanks (USTs) on the site plus an oil sump. Site observations found severe oil staining and cracking in the slab around the USTs and the petrol bowsers., fill and deposited/ dumped soil of unknown quality and quantity, and some leaking chemical drums.

A preliminary conceptual site model (CSM) was derived for the site which identified potential contamination sources and concluded that there is the potential for contamination to be present on the site.

The PSI recommended that a Detailed Site Investigation (DSI) and Hazardous Materials Survey be undertaken to quantify any potential contamination and develop appropriate remedial and risk management measures. Due to the restricted access while the service station remains in operation, the DSI is recommended as part of the demolition/ redevelopment as a condition of development approval.

Stormwater

Preliminary flooding and drainage advice has been provided by C&M Consulting Engineers which is provided at Appendix C.

Under Ryde Council's *Parramatta River – Ryde Sub-catchments Flood Study and Floodplain Risk Management Plan 2013*, the site falls within a low to medium flood risk precinct (refer Figure 12).

The 1%AEP (Annual Exceedance Probability) flood depth is approximately 0.25m (refer Figure 13). Allowing for Council's minimum freeboard requirement of +0.5m, it is recommended that initial planning be based on the minimum ground floor level being +600mm above the adjacent top of kerb level along the building frontage.

The Probable Maximum Flood (PMF) flood depth ranges from 1m to over 2m. The lowest PMF flood depth is in the southwest corner of the site (Dickson Lane). It is recommended that the entry to any basement car parking be planned for this location.

The site falls within Council's Onsite Stormwater Detention (OSD) Zone 2 which means that the minimum Site Storage Requirement (SSR) is 255m³/ha. A minimum OSD volume of 50m³ will be required for this site.

Enviropods and bio-retention areas and/or stormfilter cartridges will be used to achieve the required pollutant reduction targets set out in Council's Water Sensitive Urban Design Policy.

Overall the advice indicates that stormwater and flood mitigation can be satisfactorily addressed at the development stage.

5.3.3 HAS THE PLANNING PROPOSAL ADEQUATELY ADDRESSED ANY SOCIAL AND ECONOMIC EFFECTS?

There are a number of social effects arising from this proposal:

- The Ryde Planning Study indicates that by 2031 the predominate household type in Ryde will be lone person households, whereas West Ryde residential area offers predominantly detached houses. The proposal will provide a mix of 1, 2 and 3 bedroom apartments, providing housing

choice for different demographics, living needs and household budgets, supporting a more diverse community.

- In addition to housing diversity, 4% of the new apartments will be designated as affordable housing to ensure the development contributes to Council's affordable housing targets and caters for those in need.
- The proposal will create a new landscaped main street flanked by commercial frontages suitable for outdoor dining and socialising, which will add street life, amenity and a stronger community feel to the area.
- The proposal will provide a more compatible mixed-use commercial and residential land use transition between the town centre and residential areas compared with the existing service station and car wash which can emit noise and odour, having a positive social impact.

There are also a range of economic effects:

- The proposal will provide high quality commercial frontage to Chatham Rd, a key commercial street and entry point to the West Ryde town centre, supporting the development of new small businesses;
- The provision of new commercial/ retail floorspace offers a higher employment density compared with the service station and car wash, providing more jobs in the area with walkable access to public transport.
- The proposal will result in significant investment into a quality mixed use building and enhanced public domain, transforming the low amenity site. This will not only improve the amenity of the town centre, but will send a positive market signal that could stimulate further redevelopment and investment in West Ryde.

5.4 Public Benefit

The Planning Proposal and subsequent development would result in a number of public benefits. Items 1-5 would be valued and form part of a Voluntary Planning Agreement in order to secure the benefits.

1. Improved public domain.

The new development will provide a 4m setback to Chatham Road which, combined with the verge area, will create an 8m wide civic space, all of which will be landscaped to a high quality. The existing concrete footpath would be replaced with stone paving in accordance with the public domain guidelines and the kerb and gutter would also be upgraded. New public seating, art and bins will be installed and human-scale lighting will be integrated to create a safe and inviting evening experience. Additional street trees are proposed in the wide footpath area, adding to the colour and beauty of the streetscape.

This new civic space will be a defining feature in West Ryde on what is a main entry road into the town centre. The space will enhance the activity generated from the new commercial frontages and will be an attractive area for outdoor dining and socialising. Overall the development will 'complete' what is a key main street in the West Ryde town centre and enhance the public realm experience.

2. Provision of affordable housing.

The development will help to achieve Council's objectives for affordable housing as set out in the Affordable Housing Policy 2016-2031. In accordance with Council's adopted interim

position, 4% of the apartments will be affordable housing.

3. Additional parking.

The development will result in the provision of 6 additional on-street public parking bays through the removal of 3 driveways from Chatham Road and Dickson Avenue. In addition, public parking associated with the commercial uses will be provided in the basement parking area in accordance with the DCP requirements.

4. Contribution to roadworks.

As part of the development the applicant will contribute to roadworks, including street trees and kerbs, which will enhance an important entry road into the town centre, and benefit residents and visitors to West Ryde.

5. Remediation of contamination.

Any contamination identified in the detailed site investigation will be remediated in accordance with the expert advice and relevant guidelines, providing a significant benefit to the ecological and public health.

6. Improved land use transition to the adjoining residential area.

The mixed-use development will provide a residential interface on the western side that abuts the existing residential area, with a wide landscaped setback area at ground level. The service areas for the commercial tenants will be provided in the basement parking area, away from residential or public areas. This will offer a significant improvement over the existing service station and car wash which can emit odour and noise.

7. Provision of more housing diversity and affordability.

The Planning Proposal will enable a range of 1, 2 and

3 bedroom apartments to be developed in an area predominantly offering detached dwellings. The apartments will be a more affordable option than a stand alone house and are also within walking distance of shopping centres, civic facilities and high frequency public transport enabling much reduced transport costs. This form of housing will also cater for the changing demographics in the LGA towards smaller household sizes.

8. Increased business catchment.

The provision of increased housing on the site will increase the customer base for existing businesses, resulting in a positive economic impact.

9. Enhanced neighbourhood safety.

The development of the site will result in greater passive surveillance of the street from the apartments and will also result in more activity and pedestrians on the streets, both of which will improve the overall safety of the area. The provision of new lighting in the public domain will also improve safety.

10. Rejuvenation of a low amenity site.

The existing service station and car wash does not present a high quality frontage to the street and can emit noise and odour to the public domain and to neighbours. It does not contribute positively to the streetscape or town centre. This Planning Proposal will enable the costs of decommissioning the service station to be overcome through the redevelopment of the site for a high quality mixed-use development that contributes significantly to the public realm and the town centre experience.

5.5 State and Commonwealth Interests

5.5.1 ADEQUACY OF PUBLIC INFRASTRUCTURE AND TRANSPORT INFRASTRUCTURE

The site is well serviced with public infrastructure. The site is within 400m/ 5 minute walk of West Ryde Community Centre, West Ryde Library and West Ryde Public School. The site also has access to commercial, retail and employment land uses. Being within an existing centre the site also has access to all services such as power, water, sewer and gas.

The site has excellent access to a range of transport infrastructure which both reduces travel demand and enables sustainable modes of travel to be used. These include:

- West Ryde Train Station is within 400m/ 5 minute walk;
- High frequency bus services on Victoria Road are within 50m;
- Chatham Road is an identified bicycle route.
- Victoria Road, a main arterial road, is within 50m;
- Continuous footpath network throughout West Ryde town centre.

Importantly the site offers walkable access to all necessary commercial and civic uses in the village centre, reducing the overall travel demand for residents.

Overall the proposal will maximise the efficiency of existing transport and civic infrastructure by increasing the provision of employment and housing located within walking distance of these amenities.

5.5.2 WHAT ARE THE VIEWS OF STATE AND COMMONWEALTH PUBLIC AUTHORITIES CONSULTED IN ACCORDANCE WITH THE GATEWAY DETERMINATION?

No consultation with State or Commonwealth authorities has been carried out to date with regards to this Planning Proposal.

6. Community Consultation

No formal public community consultation has been undertaken to date in regards to this Planning Proposal. It is expected that formal exhibition and community consultation will occur in accordance with the Gateway determination directives. The exhibition would include:

- Notification letters to all owners/occupiers in the local surrounding area.
- Notification letters to relevant State Agencies and other authorities nominated by the Department of Planning and Environment.
- Advertising of the Planning Proposal on Council's website.
- Advertising of the Planning Proposal in the local newspapers.
- Exhibition notice of the Planning Proposal displayed at Council's administration building, where copies of the proposal will also be made available for viewing.

6.1.1 PRELIMINARY CONSULTATION WITH COUNCIL

A pre-lodgement meeting with Council officers was held with at the City of Ryde office on 7 October 2015 to discuss the opportunity to redevelop the subject site. There was also discussion about the inclusion of adjoining low density sites to create a precinct.

The advice from Council was that the existing low density area should be protected but that a Planning Proposal for the subject site could be considered as it forms part of the West Ryde town centre. The Council also advised that any significant amendment

to the height controls would require the following to be addressed:

- Have strategic justification.

The proposal meets the strategic objectives of metropolitan and local strategic planning as discussed in Section 5.2.2.

- Be contextually commensurate to surrounding densities and uses.

West Ryde comprises a range of building heights throughout the centre ranging from 2 - 10 storeys. The height variation has occurred on a site specific basis rather than a more uniform height transition from the centre outwards. To the north of the site is an 8 storey apartment building and to the east and south 5 storeys are permitted, with lower density to the west. The proposal for 7 storeys is consistent with this pattern of development form and provides an appropriate transition while also achieving strategic density objectives. In terms of use the proposed commercial/ residential uses are consistent with the town centre context.

- Take into account the centres hierarchy.

West Ryde is identified as a village centre, lower in the hierarchy than the strategic centres of Ryde and Macquarie Park, but above local centres. The proposal would enable the ground floor to be used for higher quality commercial and retail uses (and higher employment density) compared with the existing service station and car wash, in line with the strategic objectives for concentrating growth and jobs in compact mixed-use centres. However the total commercial/ retail floorspace (approximately 750sqm) is not a significant increase and will not impact the function or hierarchy of the village centre.

- Provides a transition to the low density residential area and retains residential amenity in this area.

The proposal will enable a high quality mixed-use residential and commercial development which provides an appropriate land use transition from the retail core to the low density residential area; improves the amenity of the streetscape and walkability of the centre; and creates a new landscaped civic space at the end of Dickson Ave (residential street). This is a much improved transition compared to the existing service station and car wash which emits noise, vapours, is vehicle-orientated and offers low aesthetic appeal to the area. The proposal does not cause overshadowing to the low density residential area and the elevation of the surrounding residential area combined with the presence of mature tree canopies means there is little visual impact associated with the proposed building height.

- Enable buildings on Victoria Road to achieve compliance with solar access requirements of SEPP 65 in the future.

The shadow diagrams prepared for the concept plan (refer Figures 25-30) demonstrate that little overshadowing would occur on the sites facing Victoria Road, where higher density development is permitted. This will ensure the buildings can achieve the solar access requirements of SEPP 65.

A draft Planning Proposal was submitted to Council in January 2016 and in February the Council requested further minor information/ alterations which was submitted in March.

During June-July several discussions took place with Council, from which it was agreed to limit height to 23.5m and also to provide a 4m setback to Chatham Rd. The current Planning Proposal reflects all consultation with Council to date.

7. Project Timeline

The following timeframes are expected for the Planning Proposal:

- August 2016
Planning Proposal is submitted to Ryde Council.
- September 2016
Council agrees to support Planning Proposal.
- October 2016
Planning Proposal is submitted to the Department of Planning and Environment.
- December 2016
Gateway Determination issued.
- February 2017
Further technical studies as required are completed.
- March 2017
Public exhibition of Planning Proposal.
- May 2017
All submissions are considered and report to Council for adoption.
- June 2017
Submission to the Department of Planning and Environment to finalise the LEP.

Appendix A

Traffic and Parking Assessment
(MRCagney, August 2016)

Appendix B

Preliminary Site Investigation
(Environmental Investigations Australia, December 2015)

Appendix C

Preliminary Flooding and Drainage Advice
(C&M Consulting Engineers, January 2016)

**EXTRACT FROM REPORT OF PLANNING & ENVIRONMENT COMMITTEE
NO. 10/16 AT ITS MEETING HELD ON 6 DECEMBER 2016**

4 PLANNING PROPOSAL - 2-6 CHATHAM ROAD, WEST RYDE

Note: Councillor Laxale was not present for consideration or voting on this Item.

Note: Jo Abboud (applicant) addressed the meeting in relation to this Item.

RESOLUTION: (Moved by Councillors Stott and Pendleton)

- (a) That Council submit the Planning Proposal relating to 2-6 Chatham Road, West Ryde (LOTS 24-26 DP 8092) for Gateway Determination, in accordance with Section 56 of the Environmental Planning and Assessment Act 1979 and that Council request delegation from the Minister to implement the Plan.
- (b) That Council, when the Gateway Determination is issued pursuant to Section 56 of the Environmental Planning and Assessment Act 1979, delegate authority to the Acting General Manager to publicly exhibit the Planning Proposal. A further report will be presented to Council following the completion of the exhibition period.
- (c) That Council endorse and exhibit the proposed amendments to Part 4.3 West Ryde Town Centre of the Ryde Development Control Plan 2014 concurrently with the Planning Proposal with the following amendment to Part 3.2.2 of the DCP inserting:
 - (b) *Pedestrian Refuges shall be provided where directed by Council and to Councils satisfaction. This includes at the intersection of Chatham Road and Dickson Avenue.*
- (d) That the Director of City Works and Infrastructure urgently review the current fencing at 8 Chatham Road, West Ryde and take action to provide additional measures to ensure safety at the fence line.

Record of the Voting:

For the Motion: Unanimous

Note: This is now a resolution of Council in accordance with the Committee's delegated powers.

Gateway Determination

Planning proposal (Department Ref: PP_2017_RYDEC_002_00): to increase maximum building height and floor space ratio controls at 2-6 Chatham Road, West Ryde.

I, the Director, Sydney Region East at the Department of Planning and Environment as delegate of the Greater Sydney Commission, have determined under section 56(2) of the *Environmental Planning and Assessment Act 1979* (the Act) that an amendment to the Ryde Local Environmental Plan (LEP) 2014 should proceed subject to the following conditions:

1. Prior to community consultation, the planning proposal is to be updated to demonstrate consistency with the draft North District Plan, released 21 November 2016.
2. Community consultation is required under sections 56(2)(c) and 57 of the Act as follows:
 - (a) the planning proposal must be made publicly available for a minimum of **28 days**; and
 - (b) the relevant planning authority must comply with the notice requirements for public exhibition of planning proposals and the specifications for material that must be made publicly available along with planning proposals as identified in section 5.5.2 of *A Guide to Preparing LEPs (Department of Planning and Environment 2016)*.
3. Consultation is required with the following public authorities under section 56(2)(d) of the Act and/or to comply with the requirements of relevant S117 Directions:
 - Roads and Maritime Services
 - Office of Environment and Heritage

Each public authority is to be provided with a copy of the planning proposal and any relevant supporting material, and given at least 21 days to comment on the proposal.

4. A public hearing is not required to be held into the matter by any person or body under section 56(2)(e) of the Act. This does not discharge Council from any obligation it may otherwise have to conduct a public hearing (for example, in response to a submission or if reclassifying land).



5. The timeframe for completing the LEP is to be **12 months** from the week following the date of the Gateway determination.

Dated *15TH* day of *FEBRUARY* 2017

A handwritten signature in black ink that reads "Karen Armstrong".

**Karen Armstrong
Director, Sydney Region East
Planning Services
Department of Planning and
Environment**

**Delegate of the Greater Sydney
Commission**

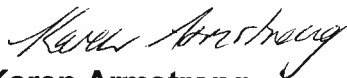
WRITTEN AUTHORISATION TO EXERCISE DELEGATION

Ryde Council is authorised to exercise the functions of the Greater Sydney Commission under section 59 of the *Environmental Planning and Assessment Act 1979* that are delegated to it by instrument of delegation dated 14 October 2012, in relation to the following planning proposal:

Number	Name
PP_2017_RYDEC_002_00	Planning proposal to increase the maximum building height and floor space ratio controls at 2-6 Chatham Road, West Ryde.

In exercising the Greater Sydney Commission's functions under section 59, the Council must comply with the Department of Planning and Environment's "*A guide to preparing local environmental plans*" and "*A guide to preparing planning proposals*".

Dated 15TH FEBRUARY 2017



Karen Armstrong
Director, Sydney Region East
Planning Services
Department of Planning and Environment

Delegate of the Greater Sydney Commission

Attachment 5 – Delegated plan making reporting template
Reporting template for delegated LEP amendments

Notes:

- Planning proposal number will be provided by the Department of Planning and Environment following receipt of the planning proposal
- The Department of Planning and Environment will fill in the details of Tables 1 and 3
- RPA is to fill in details for Table 2
- If the planning proposal is exhibited more than once, the RPA should add additional rows to **Table 2** to include this information
- The RPA must notify the relevant contact officer in the regional office in writing of the dates as they occur to ensure the publicly accessible LEP Tracking System is kept up to date
- A copy of this completed report must be provided to the Department of Planning and Environment with the RPA's request to have the LEP notified

Table 1 – To be completed by Department of Planning and Environment

Stage	Date/Details
Planning Proposal Number	PP_2017_RYDEC_002_00
Date Sent to DoP&E under s56	25/01/2017
Date considered at LEP Review Panel (if applicable)	
Gateway determination date	

Table 2 – To be completed by the RPA

Stage	Date/Details	Notified Reg Off
Dates draft LEP exhibited		
Date of public hearing (if held)		
Date sent to PCO seeking Opinion		
Date Opinion received		
Date Council Resolved to Adopt LEP		
Date LEP made by GM (or other) under delegation		
Date sent to DoP&E requesting notification		

Table 3 – To be completed by Department of Planning and Environment

Stage	Date/Details
Notification Date and details	

Additional relevant information:



IRF18/284

Mr George Dedes
Acting General Manager
Ryde City Council
Locked Bag 2069
NORTH RYDE NSW 1670

Dear Mr Dedes

Planning Proposal PP_2017_RYDEC_002_00 – Alteration of Gateway Determination

I refer to your letter seeking an extension of time to complete planning proposal PP_2017_RYDEC_002_00 to amend the development standards for land at 2-6 Chatham Road, West Ryde.

I have determined as the delegate of the Greater Sydney Commission, in accordance with section 56(7) of the *Environmental Planning and Assessment Act 1979*, to alter the Gateway determination dated 15 February 2017 for PP_2017_RYDEC_002_00. The Alteration of the Gateway Determination is enclosed.

Please be advised that the Department will continue to monitor the progress of the planning proposal with an expectation that the rezoning will be completed within the extended time frame.

The Department has established an Accelerated Rezoning Team to assist councils to finalise planning proposals. This assistance ranges from the preparation of Council reports through to liaising with government departments and agencies to assist in resolving outstanding issues. The ownership of the proposal remains with the Council. Should the Department be able assist you to finalising this proposal please contact Navdeep Hanjra on 8289 6772.

If you have any questions in relation to this matter, I have arranged for Ms Olivia Hirst to assist you. Ms Hirst can be contacted on 9274 6583.

Yours sincerely

Amanda Harvey
Director Regions, Sydney Region East
Planning Services

Encl: Alteration to Gateway Determination



Alteration of Gateway Determination

Planning Proposal (Department Ref: PP_2017_RYDEC_002_00)

I, Director, Sydney Region East at the Department of Planning and Environment as delegate of the Greater Sydney Commission, have determined under section 56(7) of the *Environmental Planning and Assessment Act 1979* (the Act) to alter the Gateway determination dated 15 February 2017 for the proposed amendment to the Ryde Local Environmental Plan 2014 as follows:

1. Delete:

“condition 5”

and replace with:

a new condition 5 “The timeframe for completing the LEP is by 30 June 2018”

Dated *1st* day of *February* 2018.

A handwritten signature in black ink, appearing to read "Amanda Harvey".

Amanda Harvey
Director Regions, Sydney Region East
Planning Services
Department of Planning and
Environment

Delegate of the Greater Sydney
Commission



Mr George Dedes
General Manager
City of Ryde Council
Locked Bag 2069
North Ryde NSW 1670



Dear Mr Dedes,

Re: 2-6 Chatham Road Ryde PP_2017_RYDEC_002_01

We write to you regarding the extension requested for the Planning Proposal at 2-6 Chatham Road Ryde.

After reviewing the merits of this proposal and the recent request for extension, it is considered that an extension of six (6) months is appropriate. Due to the ongoing delays with this proposal, it is requested that Council provide a detailed update to the Department after three (3) months to ensure that the Planning Proposal is progressing and will meet the extended timeframe granted.

The Accelerated Rezoning Team can assist Council staff with writing an updated Planning Proposal.

If there are other opportunities for the Accelerated Rezoning Team to assist Council progress this proposal, please contact Andrew Neil, Specialist Rezoning Officer on 8289 6785 or at Andrew.Neil@planning.nsw.gov.au.

Yours sincerely,

5/7/18

**Amanda Harvey,
Director Regions, Sydney Region East
Planning Services**

Alteration of Gateway Determination

Planning Proposal (Department Ref: PP_2017_RYDEC_002_00)

I, Director, Sydney Region East at the Department of Planning and Environment as delegate of the Greater Sydney Commission, have determined under section 3.34 of the *Environmental Planning and Assessment Act 1979* (the Act) to alter the Gateway determination dated 15 February 2017 for the proposed amendment to the Ryde Local Environmental Plan 2014 as follows:

1. Delete:

“condition 5”

and replace with:

a new condition 5 “The timeframe for completing the LEP is by 30 December 2018”

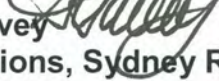
Dated

3rd

day of

July

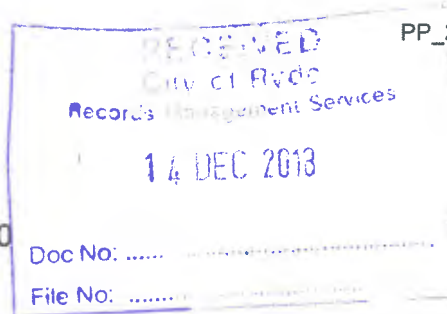
2018.


Amanda Harvey
Director Regions, Sydney Region East
Planning Services
Department of Planning and
Environment

Delegate of the Greater Sydney
Commission



Mr George Dedes
General Manager
Ryde City Council
Locked Bag 2069
NORTH RYDE NSW 1670



PP_2017_RYDEC_002_00/IRF 18/6756

Dear Mr Dedes

Planning Proposal PP_2017_RYDEC_002_00 – Alteration of Gateway determination

I refer to your correspondence seeking an extension of time to complete planning proposal PP_2017_RYDEC_002_00 to amend the development standards for land at 2-6 Chatham Road, West Ryde.

After reviewing the merits of this proposal and the recent request for extension, it is considered that an extension of six (6) months is appropriate. Due to the ongoing delays with this proposal, it is requested that Council provide a detailed update to the Department after three (3) months to ensure that the planning proposal is progressing and will meet the extended timeframe granted.

I have determined as the delegate of the Greater Sydney Commission, in accordance with section 3.34(7) of the *Environmental Planning and Assessment Act 1979*, to alter the Gateway determination dated 15 February 2017 for PP_2017_RYDEC_002_00 (as altered). The Alteration of Gateway Determination is enclosed.

If you have any questions in relation to this matter, I have arranged for Mr Dean Spanos to assist you. Mr Spanos can be contacted on 9860 1422.

Yours sincerely

Amanda Harvey 7/12/18
Director, Sydney Region East
Planning Services

Encl: Alteration of Gateway determination



Alteration of Gateway Determination

Planning proposal (Department Ref: PP_2017_RYDEC_002_00)

I, Director, Sydney Region East at the Department of Planning and Environment, as delegate of the Greater Sydney Commission, have determined under section 3.34(7) of the *Environmental Planning and Assessment Act 1979* to alter the Gateway determination dated 15 February 2017 (as since altered) for the proposed amendment to the Ryde Local Environmental Plan 2014 as follows:

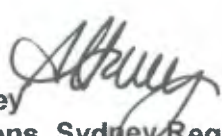
1. Delete:

"condition 5"

and replace with:

a new condition 5 "The time frame for completing the LEP is by 30 June 2019"

Dated *7th* day of *December* 2018.


Amanda Harvey
Director Regions, Sydney Region East
Planning Services
Department of Planning and
Environment

Delegate of the Greater Sydney
Commission

Community Submissions and Responses to Planning Proposal, 2-6 Chatham Road, West Ryde

No.	Trim No.	Comment	Response
1	D17/86485	<ul style="list-style-type: none"> • Need to relook at all of West Ryde e.g. Dickson Ave, Chatham Road and Fernvale Avenue • Traffic is getting busy need to consider some one way streets 	The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre and will include a review of traffic management, parking provision, and pedestrian safety.
2	D17/87773	<ul style="list-style-type: none"> • Proposal will bring shopping village to a standstill • Removal of old petrol tanks will cause issues with access to car parking under shops • Too much is being built 	The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site. Decontamination of the site will be a requirement at the development application stage to ensure that the use of the basement level car parking is safe.
3	D17/92096	<ul style="list-style-type: none"> • Adding further traffic to an already busy precinct will exacerbate the danger to pedestrians in the area • Pedestrian access from Anthony Road to Victoria Road is hazardous due to pedestrian crossings being located close to roundabouts and driveway access ways to shops. • A no right turn into Graf Ave from Chatham is ignored by vehicles resulting in pedestrian safety issues • Limited amount of on street parking now 	The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.
4	D17/92264	<ul style="list-style-type: none"> • Do not consider a new development without presenting to the community considered ways to address the area's crime, safety, traffic and parking issues. • Difficult to find parking in West Ryde particularly in the subject area and train station. 	The West Ryde Revitalisation Strategy will provide a holistic review of planning controls in the Town Centre. Community safety relating to individual developments will be addressed at the development application stage to ensure that crime prevention and safety is incorporated into the approved building design. Traffic management and parking provision in the West Ryde Town Centre will be analysed and reviewed during the preparation of the West Ryde Revitalisation Strategy, and site specific concerns will be addressed in the development application stage.

5	D17/100583	<ul style="list-style-type: none"> • No planning grounds to justify contravening existing development standards • Increased height and FSR provide excessive bulk and scale without transition to low density area • Overshadowing of 2 Dickson Street will occur • Privacy and amenity of low density residential properties will be adversely impacted • No Wind Impact Statement has been considered • PP fails Council's 5% affordable housing requirement • PP fails the requirement relating to studio and one bedroom dwellings not exceeding 25% of total dwellings • PP not in interests of West Ryde / Denistone residents • Issues of traffic congestion, vehicle parking and pedestrian crossing safety are not addressed 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and provide built form controls specifically targeting transitional sites such as this one, taking into account issues including overshadowing, privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls (including Affordable Housing requirements) will then influence the final development for this site at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area. Site specific design concerns and the details of public benefits and VPAs will be addressed at the development application stage.</p>
6	D17/100745	<p>Any change should be considered in the context of the redevelopment of the Shopping Centre as a whole PP does not consider:</p> <ul style="list-style-type: none"> • adjacent development standards • and appropriate transition of development standards to low density areas, • sites direct interface with adjacent hotel <p>Issues associated with operation of adjacent hotel and PP:</p> <ul style="list-style-type: none"> • Overshadowing al fresco area • Proximity of apartments to above area – separation of 6m unsatisfactory • Land use conflict between hotel operation and development 	<p>The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre and provide built form controls specifically targeting transitional sites such as this one, taking into account issues including interfaces and interactions with surrounding buildings and land uses. The completed Strategy, subsequent LEP updates and other relevant controls will influence the final development for this site at the development application stage. Issues regarding overshadowing, noise management, and the interface with the Hotel will be addressed amongst the building design considerations at the development application stage.</p>

7	D17/103355 D17/104088	<ul style="list-style-type: none"> • No planning grounds justify changes, over development of the site, • Not in interests of West Ryde/Denistone community, • Over development for the area. • Increased height and FSR will provide excessive bulk and scale which is not consistent with low density area • No appropriate transition to low density area • Privacy and amenity of low density area will be adversely affected. • Issues of traffic congestion, vehicle parking and pedestrian safety already exist, additional development will exacerbate • Traffic and pedestrian safety have not been addressed. • There should be no change to controls. 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will inform a holistic update to planning controls for the area. Developments will then be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area. Site specific design concerns will be addressed at the development application stage. Concerns regarding “overdevelopment” are noted.</p>
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8	D17/103370	<ul style="list-style-type: none"> • Proposed setbacks unable to compensate for loss of amenity, privacy to adjoining low density area. • Increase in height does not integrate with neighbourhood character due to excessive height, scale and built form adjacent to low density area • Traffic – At peak periods area is beyond saturation. • Pedestrian crossings are located in excessively dangerous positions • For any development in WR a detailed traffic assessment must be carried out in conjunction with Council and RMS. • Measures must be taken to improve the Chatham Road planning in order to promote proper and safe flow. • Parking – Insufficient parking for residents and workers already exists – vehicles are parking on street in untimed areas. • Availability of on street parking issue for residents in low density areas. • DCP should be amended to reflect only the higher ratio of parking in control 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including local character, privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will then inform an update to planning controls for the area. Developments will be subject to these updated controls. Specific building design concerns will be addressed at the development application stage.</p>
9	D17/103419	<ul style="list-style-type: none"> • Lack of planning grounds to justify amendments – overdevelopment of the area • Negative impact on the adjacent low density area e.g. privacy, amenity and parking • Potential devaluation of surrounding low density area • Current dangerous and unsafe levels of traffic congestion • Existing lack of parking causing dangerous conditions on roads 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and provide built form controls specifically targeting transitional sites such as this one, taking into account issues including privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls will then influence the final development for this site at the development application stage.</p>

10	D17/103933	<ul style="list-style-type: none"> • Overuse of infrastructure • Do you just accept the population targets given to you by the state government • Strongly object to more high rise in area • How significant a parameter is the 'building contribution ' to Council • Requests to be part of any discussion which emanates from the PP 	<p>The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre, and will take into account infrastructure provision and capacity, privacy and amenity. The development of the Strategy will include community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site at the development application stage.</p> <p>Local Governments are required under section 3.8 of the <i>Environmental Planning and Assessment Act 1979</i> to give effect to the strategic plans prepared by the State Government. For the City of Ryde, this means that our LEP must give effect to the North District Plan, including its five year housing targets.</p> <p>Ryde's Local Strategic Planning Statement (LSPS) sets out Council's intention to focus growth in existing centres, thereby protecting the character and amenity low density areas and ensuring growth occurs in places that are well supported by infrastructure. Council also has a strong focus on ensuring housing is provided in a variety of forms, offering greater diversity of choice than large freestanding houses or small apartments. (See chapter 3.1 of the LSPS)</p> <p>Developer contributions towards public infrastructure to meet the needs of the new residents resulting from the proposal will be determined at the development application stage.</p>
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11	D17/104087	<p>Object to PP</p> <ul style="list-style-type: none"> • No buffer between development and adjacent low density – existing height should be maintained • Dickson Lane not suitable for increased traffic flow • Traffic leaving development will be either drive through residential streets to make a right hand turn onto Victoria Road or a forced up Bellevue Avenue to left turn into Victoria Road which has poor approach visibility. • There will be increased traffic and more vehicles seeking on street parking. 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls will then influence the final development for this site at the development application stage.</p>
12	<p>Pro-forma letter D17/104150 D17/104307 D17/104471</p>	<p>Objects to PP</p> <ul style="list-style-type: none"> • No planning grounds justify changes, over development of the site, • Not in interests of West Ryde/Denistone community, • Over development for the area. • Increased height and FSR will provide excessive bulk and scale which is not consistent with low density area • No appropriate transition to low density area • Privacy and amenity of low density area will be adversely affected. • Issues of traffic congestion, vehicle parking and pedestrian safety already exist, additional development will exacerbate • Traffic and pedestrian safety have not been addressed. • There should be no change to controls 	<p>The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The development of the Strategy will include community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area. Site specific design concerns will be addressed at the development application stage.</p>

13	D17/104342	<ul style="list-style-type: none"> • PP adds greatly to density of area without commensurate improvements in traffic and pedestrian flows, security and waste services for residents. • Coles development has resulted in increased traffic congestion, pedestrian safety issues, increased litter, hazard parking, increased noise pollutions. • Do not let WR end up with the confusion and clutter as at Eastwood – commercial outcomes may erode the value of the quality of lifestyle of the area. 	<p>The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre, including amenity and infrastructure concerns. It will include a review of traffic management, parking provision, and pedestrian safety. Council will continue to provide and improve cleanliness and maintenance services in the public domain of the West Ryde Town Centre.</p>
14	D17/86271	<ul style="list-style-type: none"> • SAMA Design- this is a formal request from the proponent asking Council to consider an amendment to the PP. Proponent is seeking to increase height limit by 2 metres due to 'height of lift overrun'. 	<p>The matters raised by the proponent are discussed elsewhere in this report.</p>

17	D17/104561	<ul style="list-style-type: none"> • Oppose volume of units proposed • Proposed height is excessive • Creation of more traffic and parking problems • Dickson Ave has become dangerous in terms of traffic – traffic will be pushed up Dickon Lane 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre. It will also include a review of traffic management, parking provision, and pedestrian safety. The Strategy will be implemented by updating planning controls accordingly, which will influence development activities at the development application stage.
18	D17/104563	<ul style="list-style-type: none"> • PP allows for 106 cars that will funnel into Dickson Lane- service road cannot accommodate this level of traffic • Dickson Ave is over-congested and will be placed under further pressure • Chatham Rd and surrounding roads will suffer from further grid lock- creation of undue risks for pedestrians • Quality of life for West Ryde residents is at risk- the PP fails to nominate a public benefit to justify the increased height and FSR 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre, including a review of traffic management, parking provision, and pedestrian safety. The Strategy will be implemented by updating planning controls for the whole Centre accordingly, which will influence individual development activities at the development application stage. The specific public benefits provided via individual developments will be revisited at the development application stage.
19	D17/104756 D17/104588 D17/105279 D17/105467	<ul style="list-style-type: none"> • No planning grounds to justify a contravention of the current development controls • Proposed height and FSR will provide excessive bulk and scale without appropriate transition • PP not in the interests of residents • PP adds to existing traffic congestion, vehicle parking, pedestrian crossing safety 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The development of the Strategy will involve community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area.

20	D17/105086	<ul style="list-style-type: none"> • Proposed FSR and height does not show the transition role of the subject site to low density dwelling areas - likely to setup precedent for even higher density and height for un-developed sites closer to Town Centre. • PP has not demonstrated sufficient planning merits and community benefits in exchange of the increased density achievable on the site. → Widening of footpath and streetscape improvement are tokenistic and no value sharing on the proposed uplift for the community. • No VPA has been exhibited with PP- significant risk in approving PP and best negotiation stance for Council to secure community benefit is compromised • A more holistic traffic study for West Ryde Town Centre is urgently needed to fully understand the traffic implication of the Town Centre and entry/exit traffic to Victoria Rd, given the precedent to be set to other sites for potential uplifting. • Unclear in the proposal that whether the floor plate or floor space can meet the existing emerging need of local retail / commercial- need Economic Analysis • No strategic consideration has been given to noise from Victoria Road as the major traffic corridor within 50m to its south • DCP –clarification required to the definition of each pedestrian function area under the Pedestrian Circulation Framework. 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The development of the Strategy will include community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area. Site specific design concerns and the details of public benefits and VPA arrangements will be revisited at the development application stage.</p>
21	D17/105213	<ul style="list-style-type: none"> • Noise concerns relating to car use in surrounding streets • Noise concerns relating to air conditioners from development 	<p>Site specific design concerns and amenity impacts will be addressed at the development application stage.</p>
22	D17/105357	<ul style="list-style-type: none"> • Re-confirmation of previous submission 	<p>Response provided at submission number D17/92562.</p>

23	D17/105583	<ul style="list-style-type: none"> • Height concerns • Development will block natural light • Increased traffic congestion – particularly at 2 nearby roundabouts-up Chatham Rd and near side streets • Heavy street parking- will be further burdened • No continual pedestrian access down the eastern side of Chatham Road to Victoria Road 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and will take into account issues including overshadowing and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls will then influence the final development design requirements for this site at the development application stage.</p>
24	D17/105687	<ul style="list-style-type: none"> • There is already considerable traffic congestion around and near the intersection between Chatham Rd and Victoria Rd. • Parking is already a problem around the proposed site location-causing significant issues to local residents accessing the West Ryde commercial area. 	<p>The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.</p>
25	D17/105696	<ul style="list-style-type: none"> • Proposed height and FSR controls-result in excessive bulk and scale • No transition to low density residential areas • Privacy and amenity of low density properties adversely impacts • PP not in the interest of residents 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including privacy and amenity. The development of the Strategy will involve community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage.</p>

26	D17/106122	<ul style="list-style-type: none"> • Information available to public regarding PP/draft DCP amendment is inadequate and poorly displayed • Current height and FSR restrictions should remain-increase will result in sunlight and privacy of nearby residents • Traffic congestion, lack of parking and pedestrian safety as risk • Only access to residents carpark of proposed development will be along Dickson Lane- too narrow, one way and already over used –impact of traffic flow for Dickson Lane, Avenue and Bellevue Ave • Intersection of Dickson Ave and Bellevue Ave already dangerous – PP indicates increased risks will arise 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and will take into account issues including privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Site specific design concerns will be addressed at the development application stage. Information was made publically available in accordance with relevant guidelines and legislation.</p>
27	D17/105963	<ul style="list-style-type: none"> • Appendix A to the DA (MRCagney Traffic and Parking Assessment) fails to address the impacts of the commercial and/or retail uses of the site and the interaction with the immediate surrounds • Current parking arrangements on the east side of Chatham Road should be changed. Online news report dated 29 July 2016; a City of Ryde spokesperson stated almost 800 parking infringements had been issued in the previous 12 months. • Residents of Dickson Avenue and Chatham Road West Ryde impacted- by issues at the Dickson Avenue/Chatham Road roundabout- cars regularly backed up • Current parking challenges and proposed on-street parking from development poses risks to the community, road, pedestrian and commuter safety. 	<p>The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.</p>

28	D17/106178	<ul style="list-style-type: none"> • PP is over-development of apartments • The proximity to low density residential properties will have significant negative impacts • Will generate more traffic. • Removal of service station- important local services will be removed 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including amenity impacts. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls will then influence the final development for this site at the development application stage. It is noted that similar service station services are already available in the general local area.</p>
29	D17/106481	<ul style="list-style-type: none"> • Excessive height and no decent set backs • Traffic concerns- pedestrian safety issues 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.</p>
30	D17/106728	<ul style="list-style-type: none"> • No public benefit to be achieved- only developer profit • Traffic congestion concerns 	<p>The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre and will include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Site specific design concerns and the details of public benefits and VPA arrangements will be revisited at the development application stage.</p>

Late Submissions

1	D19/11413	<ul style="list-style-type: none"> • Cites concern that the proposal is in violation of the West Ryde Town Centre DCP (2010) and states this should be the source reference document for all developments • States proposed high density is not in keeping with the public interest • Issues with parking, congestion and risk of accidents • Puts further pressure on infrastructure • Compromises integrity of the Planning controls 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The development of the Strategy will involve community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area. Site specific design concerns and the details of public benefits and VPAs will be addressed at the development application stage.</p>
2	D19/11398	<ul style="list-style-type: none"> • Increased traffic / congestion • Pedestrian safety 	<p>The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.</p>
3	D19/11397	<ul style="list-style-type: none"> • Congestion • Increased on street parking pressure • Violates planning controls (LEP and DCP) and that the developer is requesting the increased heights and FSR is not in line with the current controls 	<p>The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre and will include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area. Site specific design concerns will be addressed at the development application stage.</p>

4	D19/11395	<ul style="list-style-type: none"> • Not a good outcome for the citizens of Ryde • DCP is not being adhered too. 	The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre. The development of the Strategy will involve community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area.
5	D19/11394	<ul style="list-style-type: none"> • Controls are being ignored for commercial gain 	See above
6	D19/11391	<ul style="list-style-type: none"> • Congestion • States there will be increased stress and no space for additional parking • Development of a 7 story building amongst low density housing will spoil amenity for both locals and visitors. • Affordable Housing concerns 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and provide built form controls specifically targeting transitional sites such as this one, taking into account amenity concerns. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls (including Affordable Housing requirements) will then influence the final development for this site at the development application stage.
7	D19/11362	<ul style="list-style-type: none"> • No reasonable explanation for increase in height • Council should be insisting on one affordable housing unit per building 	Council is committed to increasing housing supply in centres with good transport infrastructure and services such a West Ryde to meet population demand. The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre. The completed Strategy, subsequent LEP updates and other relevant controls (including Affordable Housing requirements) will then influence the final development for this site at the development application stage.

8	D19/11222	<ul style="list-style-type: none"> • Loss of Amenity • Opposes the 7 storey building • States traffic will increase 	The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre, including amenity considerations and a review of traffic management, parking provision and pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Site specific design concerns will be addressed at the development application stage.
9	D19/11021	<ul style="list-style-type: none"> • Increased traffic congestion • Parking concerns • Height / Bulk and visual pollution 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and provide built form controls specifically targeting transitional sites such as this one, taking into account amenity concerns. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls will then influence the final development for this site at the development application stage.
10	D19/11007	Objects to the Planning Proposal	No reasons given
11	D19/10885	<p>Objects to the Planning Proposal on the following grounds;</p> <ul style="list-style-type: none"> • Increased height is above the controls of the RLEP2014 • Increased FSR is above the controls of the RLEP2014 • Exacerbation of parking around Coles development • Safety risk to pedestrians • Persistent traffic jams 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and will include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area.

12	D19/10719	<ul style="list-style-type: none"> • Concerns of around loss of services (e.g. service station and car mechanic) • Traffic concerns and capacity of the existing round about • Concerns of how garbage would be collected 	<p>The West Ryde Revitalisation Strategy will include a holistic review of planning controls, the infrastructure situation in the West Ryde Town Centre, and of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site. Council will continue to take into account service delivery such as garbage collection in the development application approval process.</p>
13	D19/10586	<ul style="list-style-type: none"> • Concerns that West Ryde will turn into Meadowbank • States that the affordable housing is supposed to run by the State Government and that there is already enough public housing in Ryde • Concerns of congestion 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre, taking into account local character and including a review of traffic management, parking provision and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls (including Affordable Housing requirements) will then influence the final development for this site at the development application stage.</p>
14	D19/10557	<ul style="list-style-type: none"> • Sites concerns that this development breaches 15.5m height restriction • That the development proposal has no setbacks and doesn't provide for any open space, will create wind "turbulence" and overshadowing concerns • Traffic concerns. • Parking for the entire area should be reviewed • States that the MRCagney Traffic and Parking assessment fails to report the impacts of the commercial and retail uses on the site. • Traffic congestion issues on Dickson Ave • States that Councils attempt to 'promote' the parking spots in Ryde through the smart poles is unacceptable and is revenue raising. 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and will take into account issues including overshadowing, privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, without the need for spot-rezonings in the West Ryde Town Centre area. Site specific design concerns will be addressed at the development application stage. Concerns about Councils parking and smart poles advertising is noted.</p>

15	D19/10556	<ul style="list-style-type: none"> States that the West Ryde DCP controls are not being adhered to and that the increases in development will impact on West Ryde and create more overdevelopment 	The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre and will take into account issues including amenity. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area.
16	D19/10514	<ul style="list-style-type: none"> No consideration has been given to traffic congestion and states roads are not equipped for the extra traffic. Concerns of height 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and will include a review of traffic management, parking provision, and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.
17	D19/10419	<ul style="list-style-type: none"> States West Ryde needs a planning study to determine the future of West Ryde and what is best for the area Concerns of amenity loss Lack of pedestrian infrastructure 	The West Ryde Revitalisation Strategy will include strategic visioning for the West Ryde Town Centre and a holistic review of its planning controls. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.
18	D19/10417	<ul style="list-style-type: none"> Oppose the approval of the “Development Application” Parking and congestion 	The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site. Site specific design concerns will be addressed at the development application stage.
19	D19/10411	<ul style="list-style-type: none"> Traffic congestion Overdevelopment and excessive height Loss of transition between high density mixed use and low density residential 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage.

20	D19/10397	<ul style="list-style-type: none"> • Concerns of congestion • Concerns about height of the building 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Site specific design concerns will be addressed at the development application stage.
21	D19/10085	<ul style="list-style-type: none"> • Concerns around height controls on the development • Exceeds planning controls 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area.
22	D19/9840	<ul style="list-style-type: none"> • Inappropriate overdevelopment of the site • Over and above the current DCP and LEP requirements • Concerns that the FSR is enormously over the ratio normally allowed under the Affordable Housing SEPP 	Comments regarding “Inappropriate overdevelopment of the site” have been noted. The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre. The completed Strategy, subsequent LEP updates and other relevant controls (including Affordable Housing requirements) will then influence the final development for this site at the development application stage. Developments will be subject to these updated controls reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area.
23	D19/9570	<ul style="list-style-type: none"> • Concerns of height 	The West Ryde Revitalisation Strategy will include a review of height and FSR controls throughout the West Ryde Town Centre. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.

24	D19/9565	<ul style="list-style-type: none"> • Concerned of bulk and density of the development and the lasting impact it will have on the community • Concerns of overshadowing • Inadequate infrastructure • Goes against moratorium that Council placed on all new Planning Proposals • The development goes against communities wishes 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including overshadowing and amenity. The Strategy will also include a review of infrastructure, traffic management, parking provision, and pedestrian safety. The development of the Strategy will involve community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage.</p> <p>Regarding the Moratorium on Planning Proposals: It is noted that Council at its meeting on 26 June 2018 resolved to not accept any “new” planning proposals involving a residential component until the earlier of 1 July 2020. It should be noted that this Planning Proposal was submitted to Council prior to the resolution therefore Council is required to process the existing submitted Planning Proposal as per usual practice. The Planning Proposal for 2-6 Chatham Road was submitted to Council for assessment in February 2016.</p>
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25	D19/9030	<ul style="list-style-type: none"> • Affordable housing is already available at the new Ivanhoe redevelopment (states that it is providing 950 social housing units and 128 affordable housing units) • The finalisation of the Ivanhoe project means that there is no demonstrated need for affordable housing in West Ryde • Concerns of inadequate infrastructure • PP overdevelops the site • Goes against moratorium that Council placed on all new Planning Proposals 	<p>The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre. The development of the Strategy will involve community consultation. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage.</p> <p>Concerns regarding “overdevelopment” are noted.</p> <p>Regarding Ivanhoe Estate and the provision of Affordable Housing: While affordable housing provision is a part of the new Ivanhoe redevelopment, the provision of Affordable housing is required as per Councils Affordable housing policy. Council has an adopted Affordable Housing policy that seeks to achieve 5% of all new dwellings to be affordable housing. Council is also included in SEPP 70 (Affordable Housing) as an area identified with a need for affordable housing within the Greater Sydney Region. More information can be found here: https://www.ryde.nsw.gov.au/files/assets/public/policies/affordable-housing-policy.pdf</p> <p>Regarding the Moratorium on Planning Proposals: It is noted that Council at its meeting on 26 June 2018 resolved to not accept any “new” planning proposals involving a residential component until the earlier of 1 July 2020. It should be noted that this planning proposal was submitted to Council prior to the resolution therefore Council is required to process the existing submitted Planning Proposal as per usual practice. The Planning Proposal for 2-6 Chatham Road was submitted to Council for assessment in February 2016.</p>
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26	D19/8485	<ul style="list-style-type: none"> • Height concerns • Traffic congestions concerns • Unacceptable transition from high to low density 	The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre and include built form controls specifically targeting transitional sites such as this one, taking into account issues including privacy and amenity. The Strategy will also include a review of traffic management, parking provision, and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls will then influence the final development for this site at the development application stage.
27	D19/8431	<ul style="list-style-type: none"> • Concerns about traffic congestion 	The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.
28	D19/8421	<ul style="list-style-type: none"> • Concerned that Planning controls are being overridden • Concerns about traffic congestion and narrow setbacks and encroachment on neighbours. 	The West Ryde Revitalisation Strategy will include a holistic review of controls in the West Ryde Town Centre, including issues relating to amenity, traffic management, parking provision, pedestrian safety. The completed Strategy will then inform updates to the planning controls which influence development on this site, to be implemented at the development application stage. Developments will be subject to these updated controls, reducing the need for and likelihood of spot-rezonings in the West Ryde Town Centre area. Site specific design concerns will be addressed at the development application stage.
29	D19/8338	<ul style="list-style-type: none"> • Concerns of traffic and parking 	The West Ryde Revitalisation Strategy will include a review of traffic management, parking provision and pedestrian safety. It will be implemented through subsequent planning control updates which will influence the development design requirements for the site.

30	D19/8265	<ul style="list-style-type: none"> • Concerns of height and FSR • Concerns that Affordable housing should be permanent not for a period of 10 years. 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre. The completed Strategy, subsequent LEP updates and other relevant controls (including Affordable Housing requirements) will then influence the final development for this site at the development application stage.</p> <p>Regarding Affordable Housing Concerns: Council has an adopted Affordable Housing policy that seeks to achieve 5% of all new dwellings to be affordable housing. Council is also included in SEPP 70 (Affordable Housing) as an area identified with a need for affordable housing within the Greater Sydney Region. More information can be found here: https://www.ryde.nsw.gov.au/files/assets/public/policies/affordable-housing-policy.pdf</p>
31	D19/8130	<ul style="list-style-type: none"> • Overdevelopment • Concerns of traffic and pedestrian safety • Concerns West Ryde will eventually look like Meadowbank 	<p>The West Ryde Revitalisation Strategy will include a holistic review of height and FSR controls in the West Ryde Town Centre, taking into account local character and including a review of traffic management, parking provision and pedestrian safety. The completed Strategy, subsequent LEP updates and other relevant controls will then influence the final development for this site at the development application stage. Concerns regarding “overdevelopment” are noted.</p>

26 June 2017

City of Ryde
Locked Bag 2069
North Ryde NSW 1670

Attention: Lexie Macdonald

PLANNING PROPOSAL 2 – 6 CHATHAM ROAD, WEST RYDE

Dear Madam

I refer to the Planning Proposal for 2-6 Chatham Rd, West Ryde which is currently on exhibition. On behalf of the proponent Tesco Projects Pty Ltd, it is requested that Council consider an amendment to the Planning Proposal as part of the assessment process.

Background

Prior to lodging the Planning Proposal, the proponent consulted with Council through pre-lodgement meetings and developed several revisions of the concept design based on officer feedback. The iterative design process concluded with Council officers indicating support for seven storey height on the site, which is consistent with nearby building heights on Chatham Road, and this height was clearly depicted in the final pre-lodgement concept plans.

A height limit of 24m was suggested by Council as being appropriate to allow for the concept, and this would appear reasonable based on the assumption of 4m for the ground floor and 3.3m for the upper levels, which totals 23.8m.

The concept plans submitted with the Planning Proposal show the proposed seven storey building in the perspectives on p. 4, 28, 29 and 30, and also in the text on p.24, 25, 33, 41 and 55. Therefore it is clear that the proposal to increase the maximum permitted height of the site to 24m was intended to allow the development of a seven storey building.

Following consultation with Council regarding detailed design development, Council has recently raised concern that the lift overrun would exceed the proposed 24m height limit. Under the Ryde Local Environment Plan 2014 (RLEP) the definition of "building height" includes plant and lift overruns.

As shown in the elevations (Figure 1 and 2), while the proposed building is limited to 24m height, the two lift overruns would exceed the 24m height in an isolated central proportion of the building by 2m, reaching a height of 26m.

In order to construct the intended seven storey building either the lift overrun would need to be permitted to exceed the maximum height, or the maximum height would need to be amended to 26m to allow for the lift overrun. As the Planning Proposal is not yet finalised, we advocate the latter option to provide development certainty and to avoid the need for variations to planning controls at the DA stage.



Figure 1. Chatham Road Elevation



Figure 2. Dickson Avenue Elevation

Justification

As shown in Figure 3, the lift overruns would not be visible from street level near the site because they are set back sufficiently, however may be visible from a long distance for example at the top of Dickson Avenue (refer Figure 4). The lifts are considered to be a minor variation to the main building mass, have minimal visibility, and do not impact on views or the overall height and scale of the building.

Furthermore, the lifts will provide access to the roof garden which is an important component of the communal open space for the development. The roof garden will be landscaped to improve amenity for the residents and also the overall visual amenity of the building, and this landscaping can be designed to predominantly screen the lift overruns to further mitigate any visual impact.

As shown in Figure 5, the lift overruns will not cause any additional overshadowing to adjoining areas between 9am – 3pm on 21 June.

For the above reasons, it is considered that the lift overruns will not cause any impact or detriment to the proposed seven storey building in terms of visual impact or overshadowing.



Figure 3. Perspective from corner Chatham Rd and Dickson Ave.



Figure 4. Perspective from Dickson Ave.

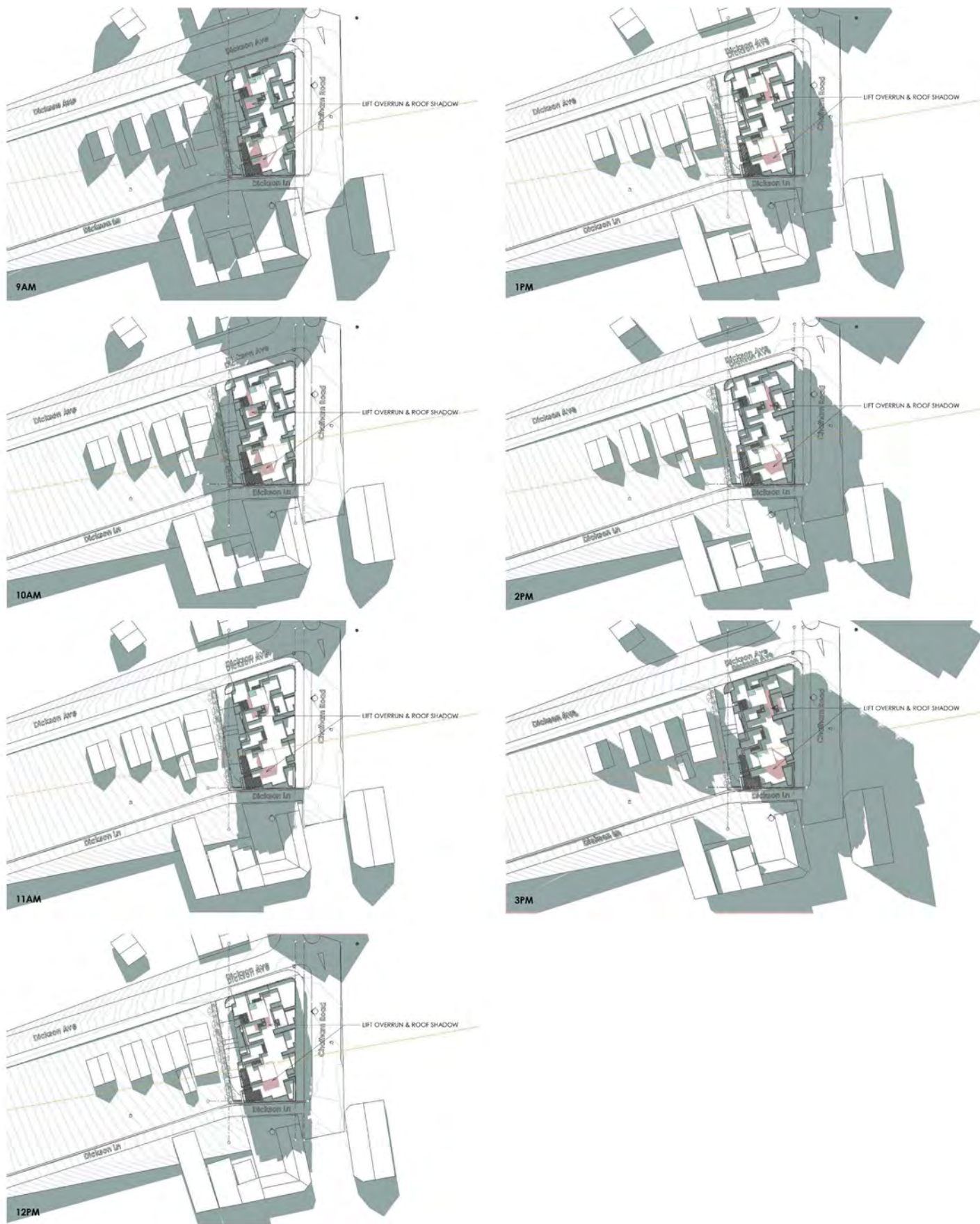


Figure 5. Shadow diagrams showing lift overrun shadow in red.

Increasing the maximum height of the site to 26m would not enable any additional building height or massing other than the lift overrun and the development would remain the same scale and height as shown in the Planning Proposal.

To provide further assurance to Council and the community about the development outcome, the following measures could be considered for inclusion in the amended Development Control Plan 2014 (DCP):

- A maximum of seven storeys is permitted on the site (in addition to the RLEP height limit);
- Lift overruns to be located away from the edges of the building to minimise their visibility;
- Roof landscaping to be used to predominantly screen the lift overruns;

These measures would ensure that the height, scale and overshadowing is unchanged from the current proposal and that any potential visual impact from the lift overruns is minimised.

Summary

We request that Council consider amending the maximum height limit controls to 26m as part of the assessment of the current Planning Proposal in order to facilitate the intended outcome of a seven storey building form. The amendment would not change the development potential and would enable the outcome intended for the site without any impact to scale, visual amenity or overshadowing.

As this amendment would not change the development outcome compared to what is currently depicted in the Planning Proposal, we do not believe a new Planning Proposal should be lodged and re-assessed nor would this amendment need to be re-advertised.

We are confident that the variation represents a minor technical amendment necessary to enable the proposed development outcome and could be addressed in Council's assessment and determination following the current public exhibition period.

Please do not hesitate to contact us should you require further information.

Yours Sincerely



Sam George
Director, SAMA Design

BUILT FORM STRATEGY

2-6 CHATHAM ROAD, WEST RYDE

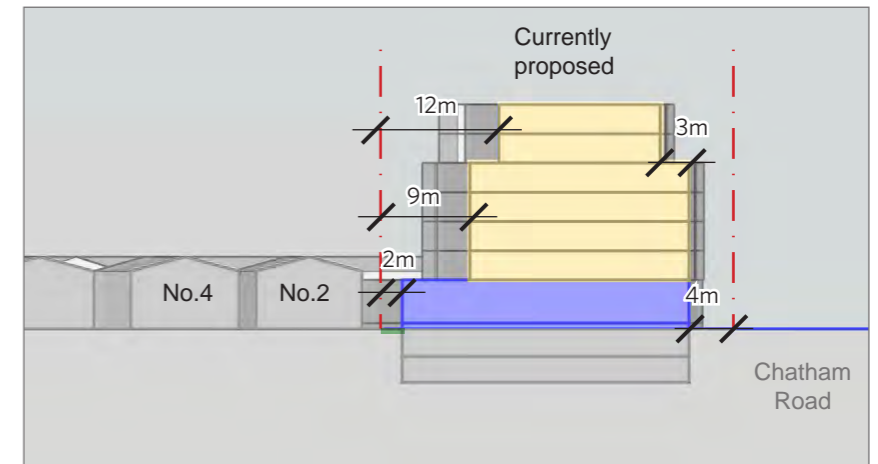
31 January 2019

Recommended Built Form Strategy



Figure 01. Built form concept plan

- KEY**
- Indicative built form context
 - Retail/Commercial (GF)
 - Residential
 - # Number of storey
 - ▶ Vehicle access
 - ▶ Building entry
 - 6m wide landscape buffer, including a 3m wide (minimum) deep soil zone
 - Site boundary
- Height of Building: 7 storey
Indicative FSR: 2.7:1



Section A-A - Currently proposed built form (Revision B)



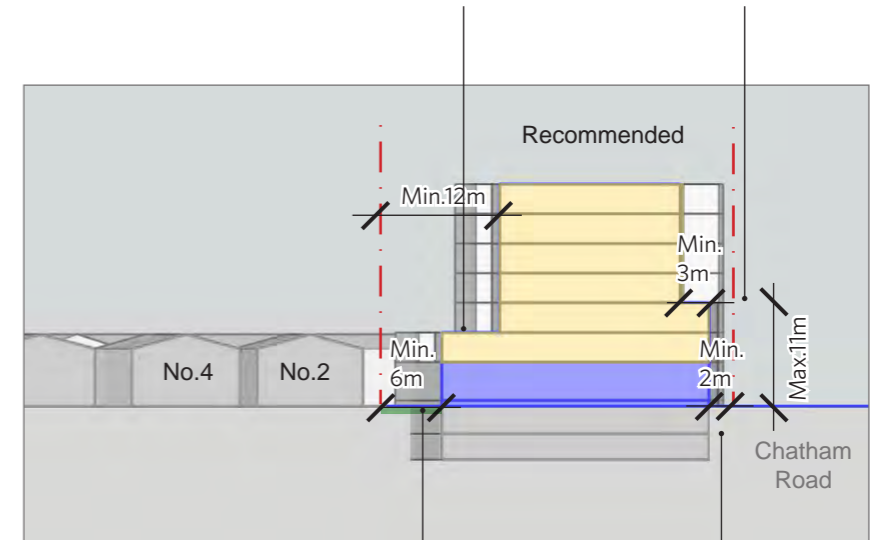
Step down built form to provide a sensitive response to the R2 zone. Maintain a sympathetic street wall height to the context.



Aerial view of the recommended built form, view due northwest



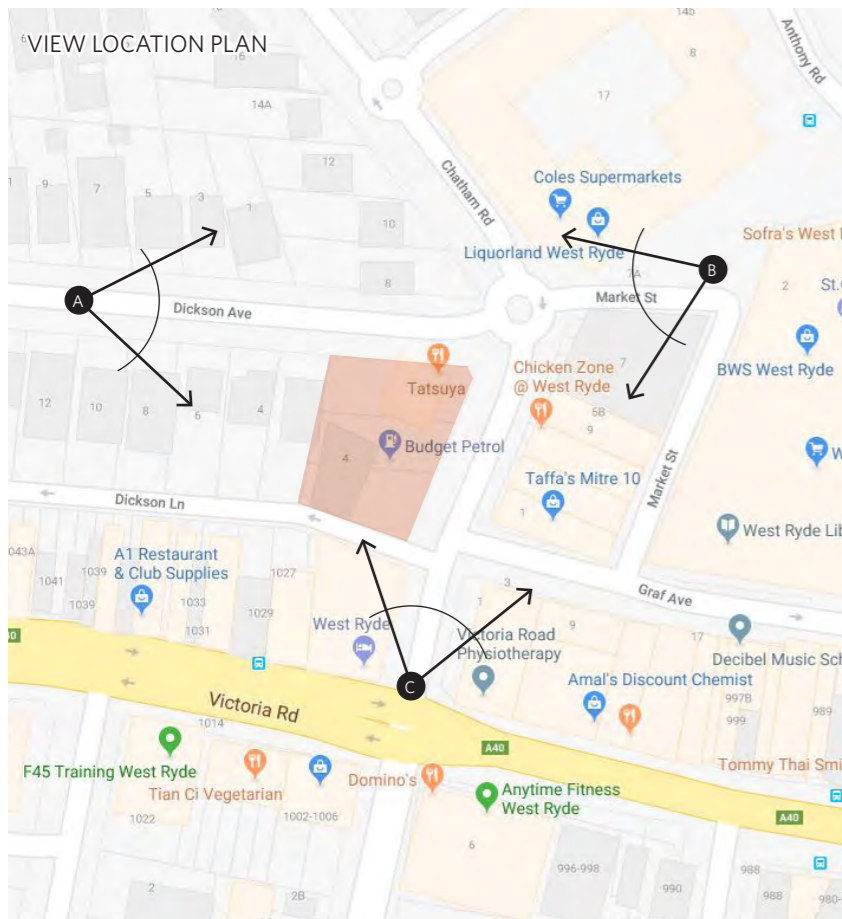
Aerial view of the recommended built form, view due southeast



Section A-A - Recommended built form

Increase separation to provide an adequate landscape buffer. Bring building forward to help alleviate potential impacts on the neighbouring property.

Street View Analysis

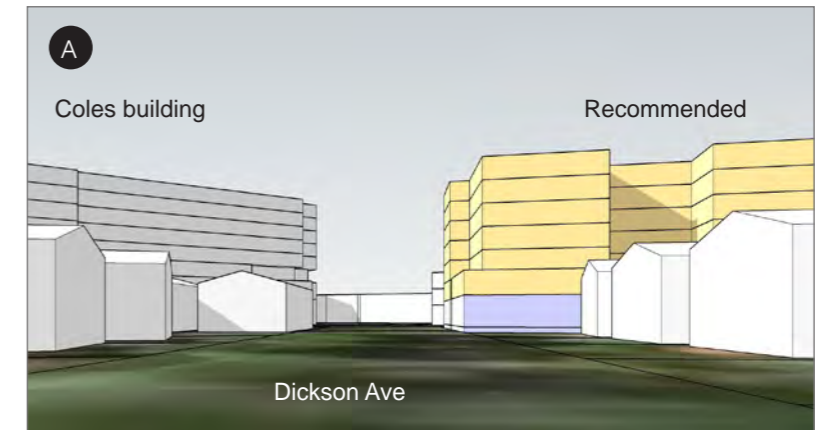


- KEY**
- Currently proposed building envelope
 - Recommended building envelope (GF retail/commercial)
 - Recommended building envelope (residential)
 - Existing adjacent building envelope

Figure 02. View due east on Dickson Avenue

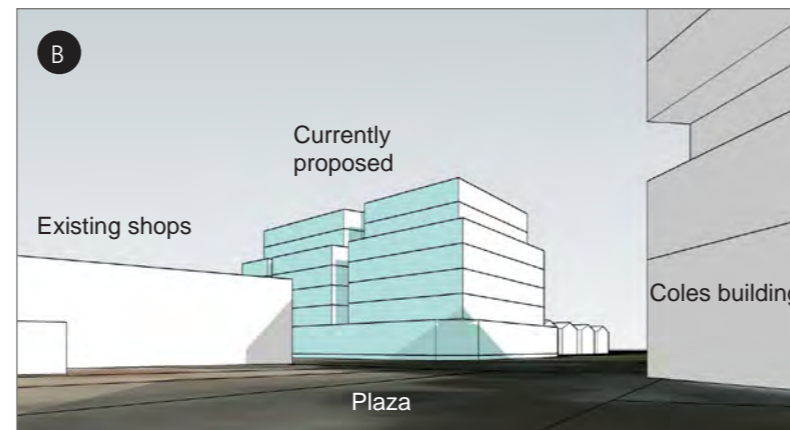


The proposal presents a long and continuous built form when viewed from the public domain of Dickson Avenue. The perceived bulk of the proposal is excessive, especially with the low-scale dwelling houses in the foreground.

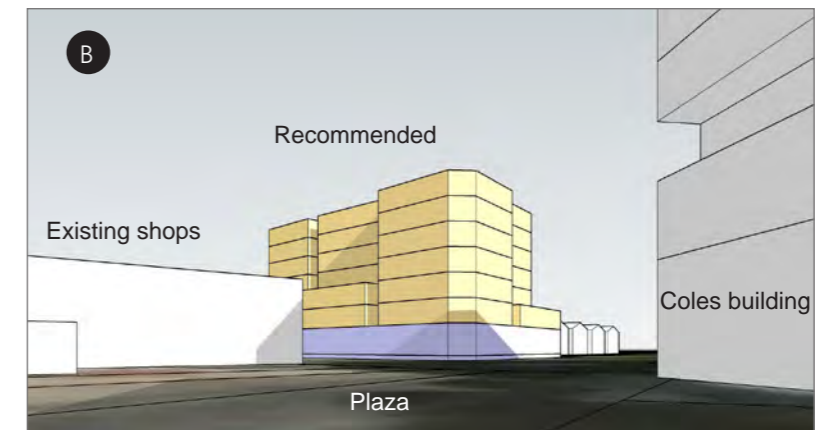


A U-shape configuration is more appropriate as it has a more sensitive built form to the R2 zone with greater building separation and a reduction in the perceived bulk.

Figure 03. View due southeast from the plaza

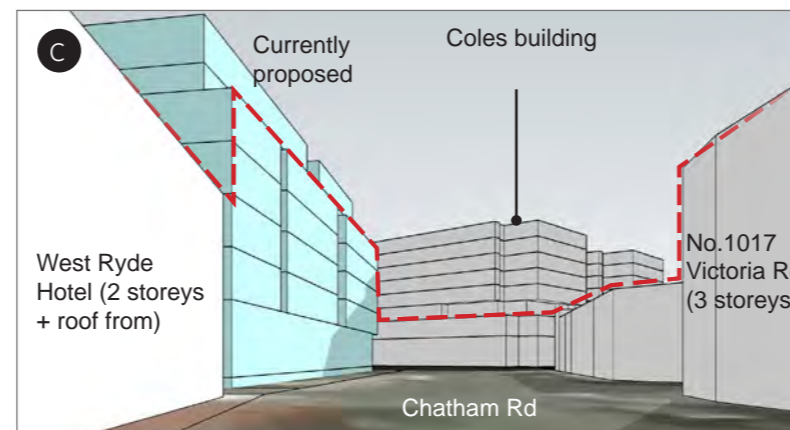


The form of having 5-storey street wall with 2 recessed storeys above does not help to achieve an elegant proportion for the 7 storey building.

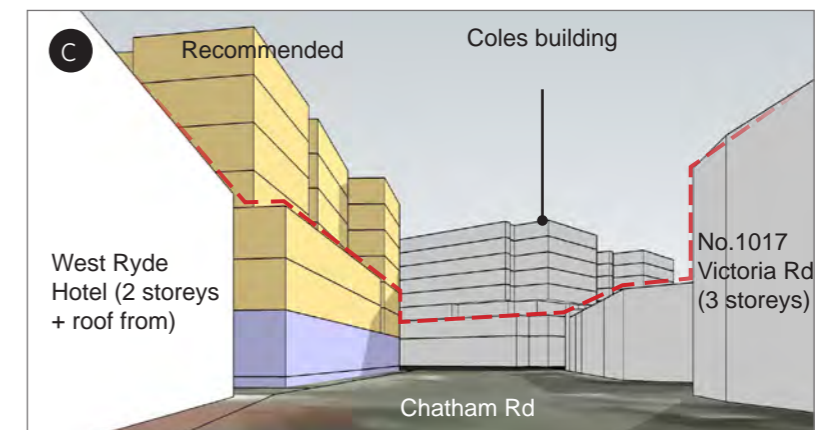


It is recommended to articulate and add visual interest to the development by creating a vertical built form to emphasise the Dickson Ave/Chatham Rd corner without additional building height.

Figure 04. View due north on Chatham Road



The proposed 5-storey high street wall is inconsistent with the existing streetscape character which predominantly has a street wall height of 2 to 3 storeys (red dotted line).



A 3-storey street wall height achieves a sympathetic streetscape relationship with the context (red dotted line).

Design Controls

Having considered the independent advice provided by the Design Review Panel, our analysis of the site and community submissions, the following site-specific design controls are recommended, in addition to the SEPP 65 and the Apartment Design Guide (ADG)'s criteria, to provide guidance in achieving an appropriate design outcome on the site. The following controls will be incorporated into the DCP.

Building height

- A maximum building height of 24m is allowed on the site.
- A minimum residential floor to ceiling height of 2.7m is to be provided in accordance with the ADG.
- A maximum street wall height of 11m is allowed to maintain an intimate street scale for the town centre, except for the Dickson Ave/Chatham Rd corner which is to be strengthened with greater verticality.
- Any corner building form expression or roof features should not rely on additional height above the limit.

Building setback and separation

- A minimum of 2m front setback is to be provided to Chatham Road to allow for the widening of the footpath.
- A minimum of 1.5m front setback is to be provided to Dickson Lane to allow for a new footpath.
- A minimum of 6m side setback is to be provided to the western boundary from the first two storeys (GF and L1).
- A minimum of 12m side setback is to be provided to the western boundary from the third storey (L2) upward.
- The above mentioned building separation/setback are minimum distances recommended; greater separation may be required subject to the compliance with the ADG.

Secondary setback

- A minimum of 3m secondary setback is to be provided above the third storey (L2) on Chatham Road and Dickson Lane as per Figure 01.
- A minimum of 3m secondary setback is to be provided above the second storey (L1) on Dickson Avenue as per Figure 01.
- No secondary setback is required at the corner of Chatham Road and Dickson Avenue in order to create a vertical built form to mark the corner.

Building articulation

- The corner built form component should have a minimum height to width ratio of 1.5:1.
- Substantial insets and slots are to be created on the facade to articulate the built form, break up the continuity of the mass and reduce the perception of bulk.
- If the insets and slots are to be used as an air source for any habitable rooms, they should have a width to depth ratio of 2:1 or 3:1 as per the ADG

to avoid trapping stale air.

Public domain interface

- A minimum depth of 10m should be provided to the commercial tenancies on the ground floor.
- Active frontage is to be maximised on Dickson Avenue, Chatham Road and the corner of Dickson Lane.
- Blank walls including air intake and vents are to be avoided on Chatham Road and minimised on Dickson Avenue and Dickson Lane.
- Any ramps and stairs (if required) are to be internalised and oriented perpendicular to the frontage to minimise their impact on the public domain.
- Continuous awnings are to be provided on Dickson Avenue and Chatham Road to provide weather protection for pedestrian.
- Building entry on Dickson Lane is to be considered to help activate the laneway.
- Substation and services such as hydrant boosters (if any) are to be encapsulated within the building.

Vehicle access

- Vehicle entry is to be provided on Dickson Lane and no more than one vehicle entry should be provided to the site.
- Vehicle entry is to be encapsulated within the built form, outside the side setback zone.
- Subject to swept path requirements for service vehicles entering/exiting the basement, the width of the car park entry is to be minimised to reduce visual impact.

Open space and landscape

- Communal open space may be provided on the roof to maximise solar access.
- Where appropriate, provide additional communal open space in the courtyard above the ground floor. Potential privacy impacts on the adjacent units and neighbouring sites are to be minimised.
- A landscaped area with screen planting is to be provided in the western setback zone.
- Deep soil zone with a minimum width of 3m is to be provided along the western boundary.
- Street trees are to be provided in the public domain on Chatham Road and Dickson Avenue as per Council's Public Domain Design Manual.

Visual and acoustic privacy

- Subject to achieving adequate solar access, the living area and balconies of the units should be towards surrounding streets or the internal courtyard and not to western boundary to minimise privacy impacts neighbouring residential properties.
- The applicant should assess the local noise levels and demonstrate that appropriate mitigation measures are provided to address the potential noise impacts from adjacent existing uses (e.g. the outdoor bar on Dickson Lane) and roads.

Waste collection

- Waste collection is to be carried out internally on site.
- Vehicle entry and basement layout are to be appropriately designed to enable an 11m truck entering and exiting the basement in a forward direction.
- The basement should have a minimum floor-to-ceiling clearance of 4.5m for truck access.

Indicative FSR calculation

The calculation of the FSR for the recommended built form on page 1 is based on the following assumptions:

- The commercial GFA on the ground floor was calculated including the commercial space and common corridors but excluding the driveway ramp, circulation cores, loading and servicing areas. The efficiency rate of the ground floor is estimated to be 50% of the GBA.
- The GFA of residential apartments was calculated including common corridors but excluding balconies and circulation cores. It was based on an efficiency rate of 75% of the GBA.

City of Ryde Development Control Plan 2014

Part: 4.3
West Ryde Town Centre
Draft (October 2016)

Translation

ENGLISH

If you do not understand this document please come to Ryde Civic Centre, 1 Devlin Street, Ryde Monday to Friday 8.30am to 4.30pm or telephone the Telephone and Interpreting Service on 131 450 and ask an interpreter to contact the City of Ryde for you on 9952 8222.

ARABIC

إننا نعتذر عليك فهم محتويات هذه الوثيقة، نرجو للحضور إلى مركز بلدية رايد Ryde Civic Centre على العنوان: 1 Devlin Street, Ryde 1 من الاثنين إلى الجمعة بين الساعة 8.30 صباحاً والساعة 4.30 بعد الظهر، أو الاتصال بمكتب خدمات للترجمة على الرقم 131 450 لكي تطلب من أحد المترجمين الاتصال بمجلس مدينة رايد، على الرقم 9952 8222، نيابة عنك.

ARMENIAN

Եթե այս գրույթինը չէք հասկնար, խնդրեմ եկե՛ք՝ Բայր Սիվիլ Կենտրոն, 1 Տելվին փողոց, Բայր, (Ryde Civic Centre, 1 Devlin Street, Ryde) Երկուշաբթիէն Ուրբաթ կ.ա. ժամը 8.30 – կ.ե. ժամը 4.30, կամ հեռաձայնեցե՛ք Հեռաձայնի եւ Թարգմանություն Սպասարկության՝ 131 450, եւ խնդրեցե՛ք որ թարգմանիչ մը Բայր Քաղաքապետարանին հետ կապ հաստատուի ձեզի համար, հեռաձայնելով՝ 9952 8222 թիվին:

CHINESE

如果您看不懂本文，請在周一至周五上午 8 時 30 分至下午 4 時 30 分前往 Ryde 市政中心詢問 (Ryde Civic Centre, 地址: 1 Devlin Street, Ryde)。你也可以打電話至電話傳譯服務中心，電話號碼是: 131 450。接通後你可以要求一位傳譯員為你打如下電話和 Ryde 市政廳聯繫，電話是: 9952 8222。

FARSI

اگر این مدرک را نمی فهمید لطفاً از 8.30 صبح تا 4.30 بعد از ظهر دوشنبه تا جمعه به مرکز شهرداری رايد، Ryde Civic Centre, 1 Devlin Street, Ryde مراجعه کنید یا به سرویس مترجم تلفنی، شماره 131 450 تلفن بزنید و از یک مترجم بخواید که از طرف شما با شهرداری رايد، شماره 9952 8222 تلفن بزند.

ITALIAN

Se non capite il presente documento, siete pregati di rivolgervi al Ryde Civic Centre al n. 1 di Devlin Street, Ryde, dalle 8.30 alle 16.30, dal lunedì al venerdì; oppure potete chiamare il Telephone Translating and Interpreting Service al 131 450 e chiedere all'interprete di contattare a vostro nome il Municipio di Ryde presso il 9952 8222.

KOREAN

이 문서가 무슨 의미인지 모르실 경우에는 1 Devlin Street, Ryde 에 있는 Ryde Civic Centre 로 오시거나 (월 – 금, 오전 8:30 – 오후 4:30), 전화 131 450 번으로 전화 통역 서비스에 연락하셔서 통역사에게 여러분 대신 Ryde 시청에 전화 9952 8222 번으로 연락을 부탁드립니다.

Amend. No.	Date approved	Effective date	Subject of amendment

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1.0 INTRODUCTION

This Part will facilitate in the revitalisation of West Ryde Town Centre and adjoining areas as a vibrant, attractive and safe urban environment with a diverse mix of retail, commercial, and residential opportunities.

1.1 The Purpose of this Part

The purpose of this Part is to provide policies relating to the future development of the West Ryde Town Centre and adjoining areas.

This Part of the DCP provides objectives, principles and development controls to guide future development within the West Ryde Town Centre and adjoining areas and achieve the intended future vision for the West Ryde Town Centre.

This Part should be read in conjunction with the following documents:

- Ryde Local Environmental Plan 2014
- The relevant State Environmental Planning Policies (SEPPs) including SEPP 65 - Design Quality for Residential **Apartment** Development.

West Ryde Centre Study and Master Plan 2010 should be the source of reference for developers and other individuals interested in the development of the Town Centre.

Ryde City Council commissioned a study into the West Ryde Town Centre to update the vision for the centre. The West Ryde Town Centre Master Plan was developed in consultation with owners, traders, residents and servicing authorities in West Ryde and was adopted on 7 December 2010. The adopted Master Plan forms an important strategic planning document describing the development vision which is to be pursued in the centre.

1.2 Land to which this Plan Applies

This Part supplements and gives guidance to the controls and objectives of Ryde Local Environmental Plan 2014. Specifically, this Part identifies objectives and controls that will shape the future development of the West Ryde Town Centre. The controls contained indicate how the objectives are to be implemented.

This Part applies to all land within the West Ryde Town Centre and adjoining areas as shown in the following map.

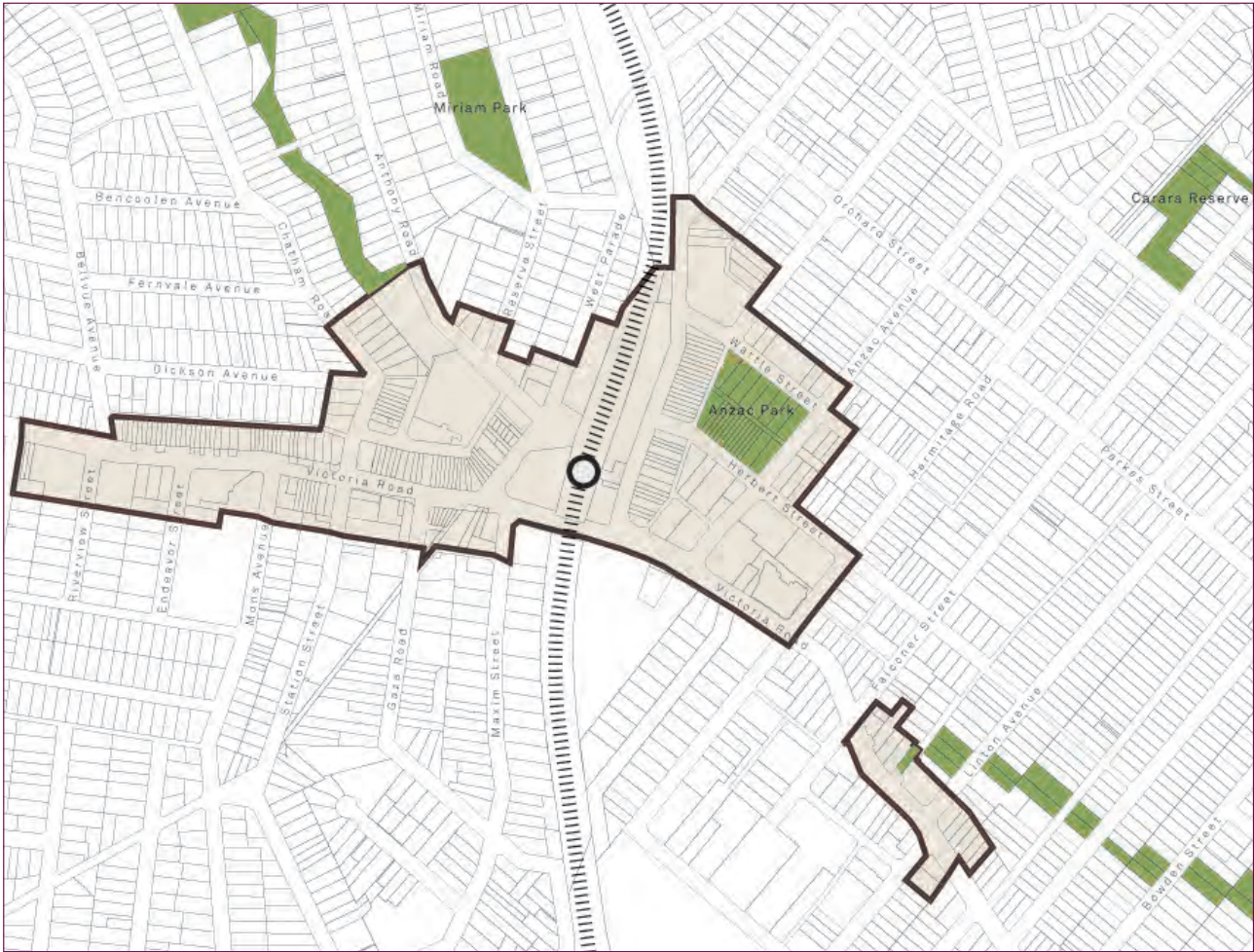


Figure 4.3.01 West Ryde Town Centre and adjoining areas

2.0 VISION

This Part is one of a number of planning initiatives undertaken to revitalise established urban centres within the City of Ryde. The vision for each centre is to create a unique character arising from its natural and built features, history and community expectations.

The objectives of this Part are to promote Council's vision for the West Ryde Town Centre.

2.1 Objectives

This Part aims to revitalise the West Ryde Town Centre through development provisions which:

1. facilitate the creation of a convenient community and retail centre within the core town centre;
2. encourage higher density development surrounding the town centre and provide transition to surrounding suburban areas;
3. encourage new development and re-use of existing buildings to contain a mix of land uses;
4. describe the desired maximum scale and bulk of new buildings;
5. improve pedestrian amenity and develop a sense of community place;
6. create a pedestrian oriented town centre including active street frontages;
7. increase the number of people living within walking distance of high frequency public transport services;
8. provide for safe and convenient vehicle access and parking;
9. minimise risk of flooding to the town centre;
10. facilitate development that is environmentally sustainable;
11. encourage the protection and enhancement of items and areas of environmental heritage;
12. enhance existing open space to offer increased amenity to surrounding residential development and provide for safe, inviting, well used and attractive public spaces; and
13. encourage a built form that integrates with the existing public domain and pedestrian network.

2.2 West Ryde Town Centre Master Plan

The Council adopted the West Ryde Master Plan on 7 December 2010. The Master Plan is supported by a number of planning studies, refining its underpinning objectives and desired outcomes, and informs the suite of controls contained in this section.

The Master Plan proposes a vision for the Town Centre and adjoining areas. It also provides an urban design framework to guide an increase in residential and employment opportunities, and promote the sustainable use of existing infrastructure and services.

2.3 West Ryde Town Centre Vision

The West Ryde Master Plan identifies a vision for West Ryde Town Centre that anticipates:

- the creation of a new identity for the Town Centre as an important place along Victoria Road. Improvements will see Victoria Road become a landscaped route, with the West Ryde Town Centre being defined by both new activity and built form;
- improvements to key public domain areas will see the creation of a series of 'green links', which extend both towards and through the Town Centre core. Improvements at the core will also be implemented highlighting this area as a pedestrian priority environment as well as increasing permeability and pedestrian accessibility;
- existing open spaces will be enhanced, offering increased amenity to surrounding residential development. New development and increased densities will be focused at the centre core in areas of pedestrian activity such as public transport nodes and close to open space areas; and
- achievement of a mix of development type introduces a transition between the core Town Centre and the surrounding residential areas of West Ryde. The retail core of the Town Centre will remain the primary retail and commercial centre for the surrounding locality.

2.4 Town Centre Precincts

The West Ryde Town Centre Master Plan provides a vision for the future urban form and function of the West Ryde Town Centre. Key precincts which are of importance to the future of the Town Centre have been identified for the purposes of this Part of the DCP. These key precincts are:

1. Victoria Road West;
2. Retail Core;
3. Ryedale Road;
4. Anzac Park;
5. Victoria Road Mixed Use; and
6. Victoria Road Enterprise Zone.

Character objectives and the desired future character for these Town Centre Precincts are outlined in Section 4.0. The built form controls outlined in this Part support these objectives, guiding the design of new development that will reinforce the characteristics of each area. Future development within or adjacent to these identified Town Centre Precincts is to be consistent with relevant built form controls, and is to demonstrate that the intended future character is protected and enhanced.

The six Town Centre Precincts are identified in the following map (Figure 4.3.02).

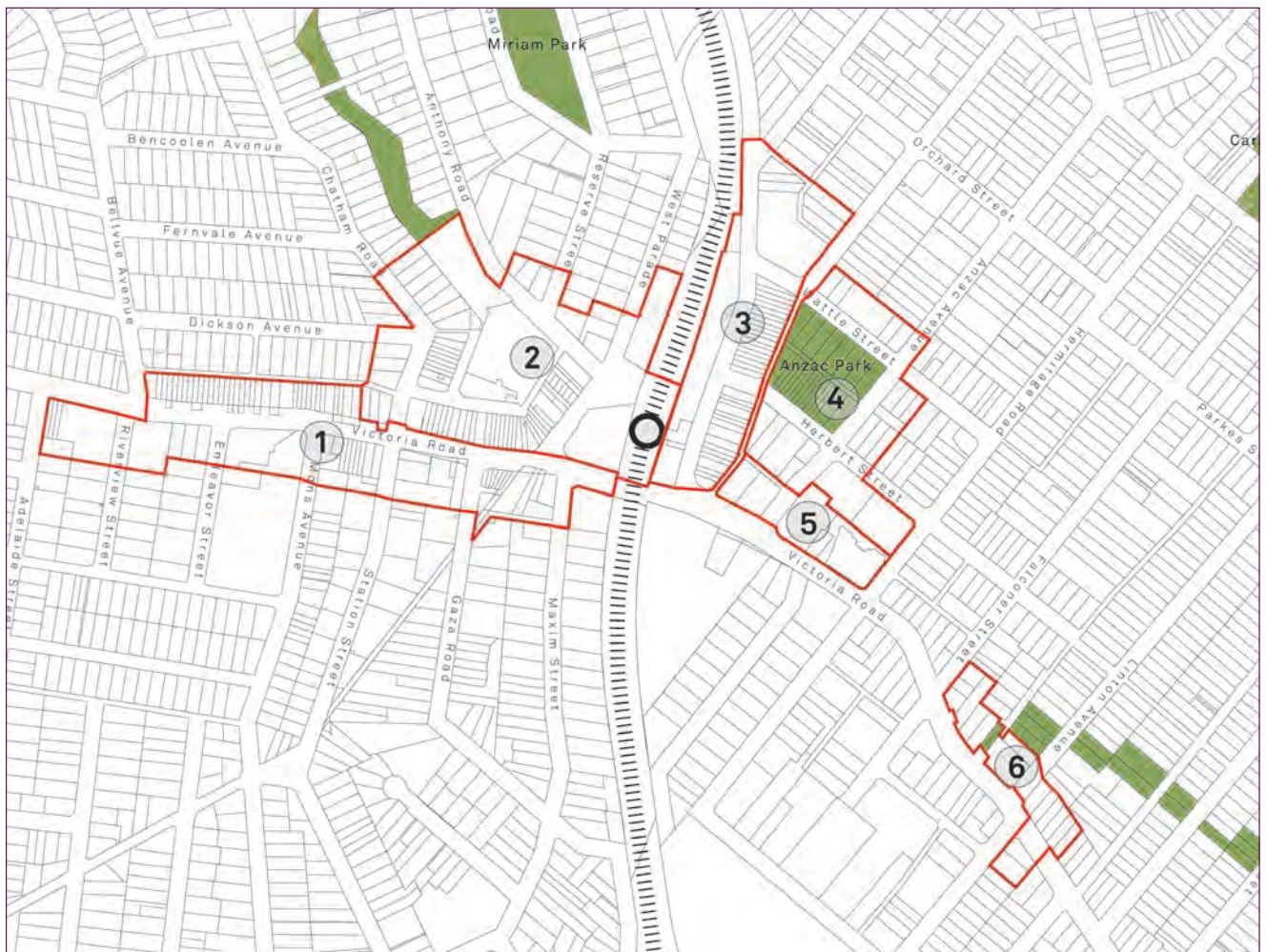


Figure 4.3.02 West Ryde Town Centre DCP Precincts

2.5 Key Town Centre Principles

The West Ryde Master Plan provides principles to influence the future built form and public domain of the West Ryde Town Centre and adjoining areas. There are five key factors which are important to the future development of the town centre and will influence future development. Each of these is supported by principles that achieve to the intended future vision of the West Ryde Town Centre and adjoining areas.

2.5.1 Green Strategy & Water Management

A series of 'Green Links' can be developed to connect the edges of the centre and existing green space areas to the central retail core precincts, and beyond. Through improving existing open spaces and integrating these with existing and new street tree planting and public/domain and streetscape improvements, these corridors will offer amenity to surrounding residential development and critical pedestrian linkages to, and throughout, the town centre. These links will also provide critical public transport.

Principles

1. To improve existing public amenity and build on existing natural assets to ensure the future sustainability and amenity of West Ryde.
2. To improve public domain through a series of connecting 'green links' extending towards and through the Town Centre Core.
3. To create a 'green strategy' that integrates residential amenity with water management.

2.5.2 Pedestrian Circulation

An integrated approach to pedestrian and vehicular circulation is needed to allow safe and efficient access to and within the town centre, with priority given to pedestrian movement, amenity and safety in the core.

The network of streets, lanes and pedestrian/shared spaces are a vital component of a town centre that need to be reinforced and maintained. The role and function of each of these spaces is determined by the scale, character, and purpose of the surrounding built form, as well as the activities that take place within the public domain.

Principles

1. To maximise pedestrian safety (primarily through traffic calming measures) within active precincts, integrated public transport corridors and bus interchange.
2. To provide a legible access and parking arrangement within the Town Centre through a clear hierarchy of pedestrian prioritisation with parking opportunities adjacent.
3. To define and activate edges to streets and parks to create a sense of character, legibility, convenience and safety for users of the public domain.
4. To improve the pedestrian environment and amenity including quality pavement design, street trees and furniture.
5. To ensure new buildings address streets and parks, in order to provide definition and casual surveillance of these spaces.
6. To ensure safe and convenient movement of cyclists within the town centre, including greater connectivity to surrounding cycle networks.

2.5.3 Safety & Accessibility

Future development within the town centre should encourage pedestrian movement, providing safe access for the whole community to and from the main transport hubs. Future consideration should be given to ensuring clear movement lines through the Town Centre and between primary transport nodes. Public domain upgrades will also assist in providing a safe and secure Town Centre.

Principles

1. To ensure development provides legible pathways for pedestrians and contributes to the provision of safe access.
2. To provide active frontages at street level to contribute to the safety and surveillance of the street.
3. To encourage public domain upgrades including street tree planting and street lighting.
4. To introduce safe road crossings to enhance north-south and east-west connectivity.

2.5.4 Retail and Employment Areas

The important economic and employment role of West Ryde is recognised and should be enhanced by future development of the Town Centre and adjoining areas. The Town Centre comprises several distinct retail and commercial areas. These areas should be further developed as an integrated series of retail and commercial precincts in order to enhance the economic viability of West Ryde.

Principles

1. To encourage future development of distinct, but connected, retailing precincts.
2. To activate and reinvigorate the Victoria Road corridor to improve amenity and viability of future uses.

2.5.5 Residential Precincts

Residential uses are considered an important land use in the Town Centre. Over time, the development of the centre will encourage a mixture of housing densities throughout, with higher density residential developments concentrated around the central core and within close proximity to the transport interchange. Low density residential developments will be encouraged on the outer edge of the Town Centre to create a clear transition between the town centre and the surrounding zoned residential land.

Principles

1. To focus residential density around the Town Centre core and improve the public domain to increase 'livability'.
2. To focus density around open space areas to encourage casual surveillance and provide additional amenity to dwelling occupants.
3. To maintain a transition in the scale of residential development at the edge of the centre.

3.0 GENERAL DEVELOPMENT CONTROLS

This section of the Plan sets out the objectives and development requirements that address

- Site Planning;
- Building Form Character Principles;
- Built Form Controls;
- Public Domain/Public Amenity;
- Accessibility;
- Environmental Management;
- Social Considerations; and
- Housing Choice.

The following controls apply to all types of development found within the Town Centre and adjoining areas. Precinct base controls are specified in Section 4.0 Precinct Development Controls.

3.1 Built Form

This section outlines specific built form controls for new development in the West Ryde Town Centre. New development is to address each of the built form controls, ensuring that the overall scale and form responds appropriately to its context.

3.1.1 Building Height and Bulk

The building height controls aim to deliver a range of building heights across the Town Centre, responding to sunlight access requirements, achieving an acceptable pedestrian scale and function of the public domain and promoting flexibility and adaptability into the future.

Objectives

1. To ensure an appropriate bulk and scale of development which is consistent with the character of the West Ryde Town Centre.

Controls

- a. The maximum height of any building in the town centre will be in accordance with the height shown on Ryde Local Environmental Plan 2014 Height of Building Map.
- b. Scale and bulk of development will primarily be determined by the maximum Floor Space Ratio applying to the land. Floor Space Ratio of buildings is to be in accordance with the Ryde Local Environmental Plan 2014 Floor Space Ratio Map.

Floor to Ceiling Heights

- c. The following controls provide the minimum floor to ceiling heights, as illustrated in Figure 4.3.03:
 - i. Non residential uses:
 - Ground floor retail/commercial uses require 3.6 metres floor to ceiling height; and
 - Any non residential level above require a minimum 3.3 metres floor to ceiling height; and
 - ii. All residential uses:
 - Minimum 2.7 metres floor to ceiling height.

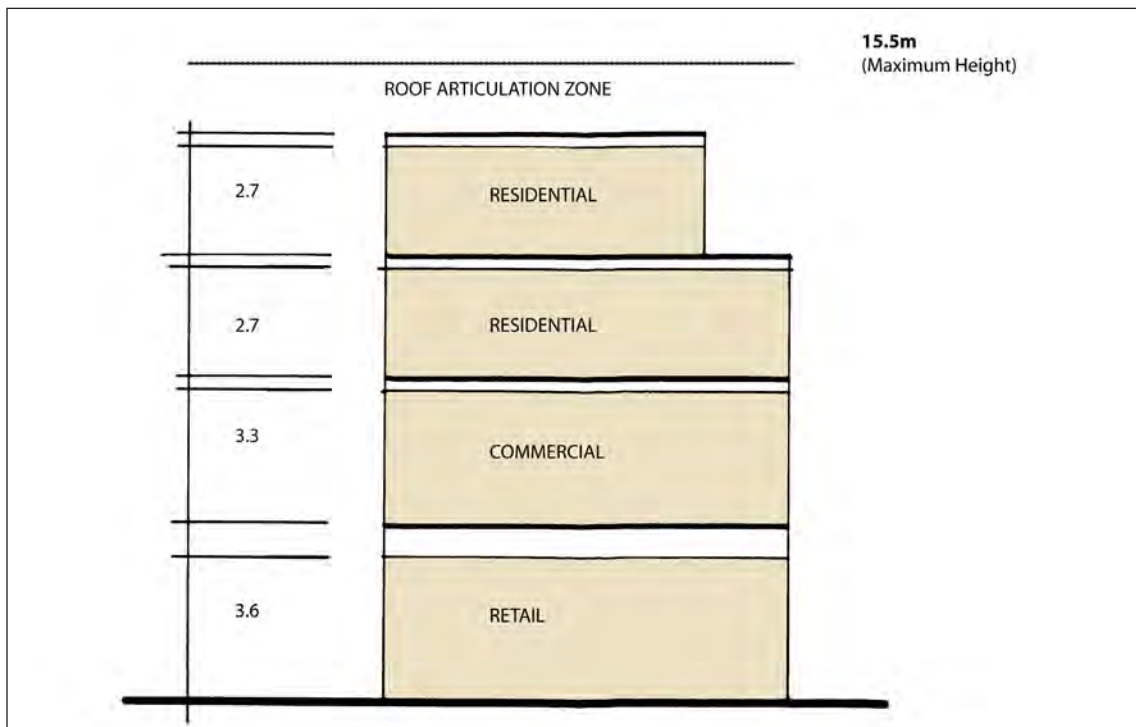


Figure 4.3.03 Minimum Floor to Ceiling Heights (Indicative)

Building Articulation and Features

- d. In multi-storey and mixed use buildings, roof articulation should be provided to add visual interest to buildings. Any elements within the roof articulation zone are not to extend above the maximum height in metres specified by the Ryde LEP 2010.
- e. Building articulation is to respond to the local context and environmental conditions by considering roof shape, pitch and overhangs, entries and verandahs, balconies, terraces, materials, finishes, fixtures, patterns, colours and detailing.
- f. Lift overruns, plant equipment and communication devices are to be integrated into the design of the building.

3.1.2 Mixed-Use Development

Objectives

1. To encourage a vibrant and active Town Centre and improve the visual image of West Ryde.
2. To encourage a mix of residential, commercial and retail land uses within the Town Centre.
3. To ensure buildings situated on corner allotments provide for visual interest and address the intersections which they front.
4. To ensure car parking provisions and servicing do not compromise active street frontage and desirable urban form outcomes.
5. To encourage safety through built form design and mixed-use developments.

Controls

- a. Uses which promote pedestrian activity (i.e retail shops, cafes etc) are to be provided at ground floor level to promote vibrancy and allow passive and active surveillance opportunities.
- b. New development is to encourage a mix of retail and commercial activities to be located on a single level or at ground level. Residential uses should be provided within upper levels of the building.
- c. Where new residential development is proposed as part of a mixed use development the following issues are to be considered:
 - i. the proposal should be consistent with the requirements of State Environmental Planning Policy No. 65 - Design Quality of Residential **Apartment** Development and the **Apartment Design Guide**, where appropriate; and
 - ii. Development must comply with noise and sound insulation requirements under BCA and AS3671-1987: Acoustics - Recommended Design Sound Levels and Reverberation Times for Building Interiors. Design features may be used to achieve primary acoustic privacy. In addition, developments are to comply with State Environmental Planning Policy (Infrastructure) and RailCorp where appropriate.

3.1.3 Street Setbacks and Alignment

The way a building addresses the street is critical in delivering a high quality and vibrant public domain. Setback requirements have been determined having regard to the function of the street, laneway or public domain, which the development addresses.

Objectives

1. To establish build to street lines which respond to the function and character of the street.
2. To promote the definition and activation of the streetscape through built form.
3. To allow for access and circulation throughout the town centre.
4. To ensure a transition to the neighbouring low density residential areas.
5. To allow an outlook to and passive surveillance of the street.
6. To maintain reasonable sunlight access to the public domain, open space and to adjoining sites.
7. To clearly identify corner sites through prominent built form.

Controls

- a. New buildings are to have street frontages built predominantly to the street alignment for the first 2 storeys, except for land to which this Part of the DCP applies and are zoned residential, industrial or enterprise corridor.
- b. The first two storeys of all buildings along a build to street (hard) setback line as indicated in Figure 4.3.04, are generally to maintain a hard alignment with the street. Setbacks are to be minimised.
- c. New buildings which are built along a street frontage with no build to street setback line indicated in Figure 4.3.04 are to provide setbacks as required for their development type.
- d. Building design is to minimise any adverse wind effects on public spaces. The orientation, height and form of development are to be designed to promote public safety and comfort at ground level. Awnings are to be provided, if necessary, for pedestrian comfort.
- e. All applications for buildings over 5 storeys shall be accompanied with a Wind Impact Statement from a qualified person. For buildings over 9 storeys a detailed wind impact

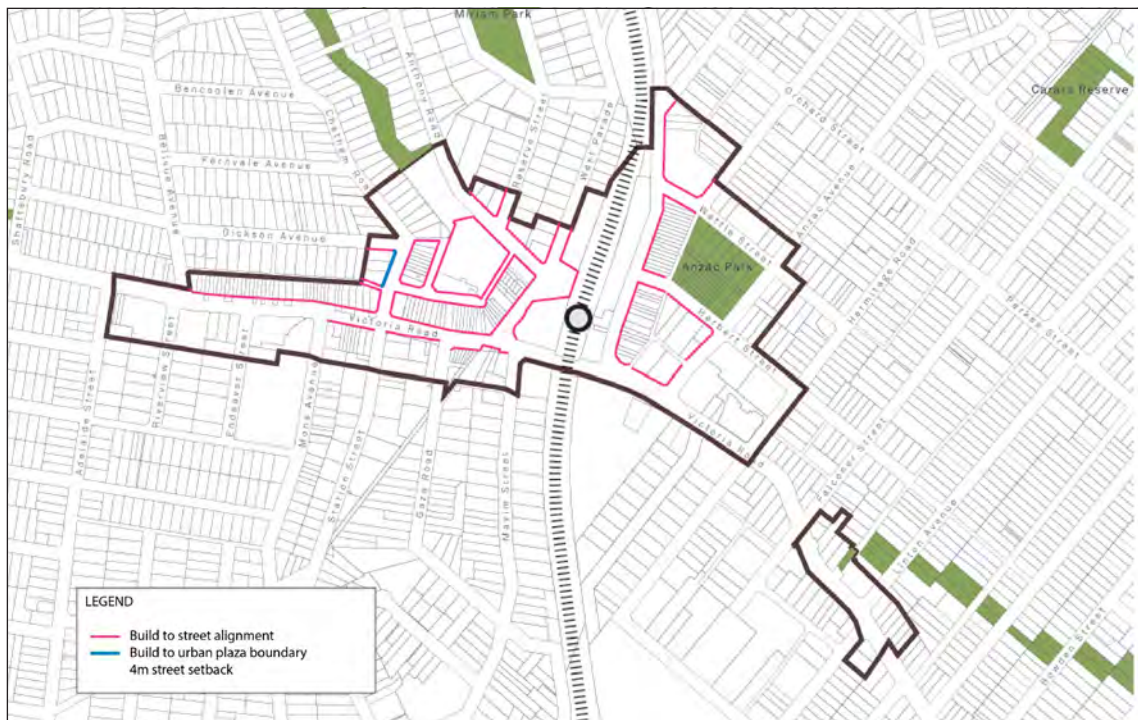


Figure 4.3.04 Build to Street Setback

3.1.4 Urban Design

In addition to built form controls, appropriate urban design is important to encourage the creation of a high quality urban environment and to enhance the sense of place. Within the West Ryde Town Centre, it is also important to maintain an appropriate transition to surrounding low density residential areas.

Objectives

1. To encourage commercial/retail facilities along Victoria Road reinforcing and supporting activities within the West Ryde Town Centre.
2. To encourage mixed use development activities.
3. To provide opportunities for a range of commercial/retail uses at ground level.
4. To ensure future scale and mix of development recognises the residential/ commercial interface and encourages a transition between high density development and lower density residential land.
5. To activate laneways and 'back-of-house' areas to create improved pedestrian environments and linkages and build upon the sustainability and accessibility corridors between the adjoining residential areas and West Ryde Town Centre.
6. To encourage clearer connections between the western and eastern sides of West Ryde Train Station.
7. To encourage a variety of built form in new development and to assist in defining street blocks.
8. To encourage high quality urban design of new development.
9. To ensure new and refurbished development responds to the urban context.

Controls

- a. Built form is to follow and reinforce the established street alignment, providing a continuous building line to define the public domain.
- b. Pedestrian corridors and linkages such as arcades, lanes and streets, should be provided, maintained and enhanced.
- c. Built form design should respect the existing character or contribute to a preferred character of the town centre.
- d. Built form design of mixed use development should provide a transition to surrounding lower density residential development by providing building articulation, separation, and setbacks in accordance with Parts 2 and 3 of the *Apartment Design Guide*.
- e. Mixed use development should be designed to positively contribute to the public domain and ensure that the interface between residential and other uses within the development is adequately addressed in accordance with *Part 45 Mixed Use of the Apartment Design Guide*.
- f. Open Space and public domain is to be provided, maintained and enhanced to Council's satisfaction in accordance with the City of Ryde Public Domain Technical Manual.
- g. Off-street parking should be provided behind the front building line to limit impact to the streetscape and must be consistent with 3.1.6 Active Street Frontages and Street Address. Basement parking should be provided where possible.
- h. Where residential development is proposed, pedestrian entry should be separated from the entry to other land uses in buildings.
- i. Car parking and servicing must not impact adversely upon desirable built form outcomes and must be consistent with active street frontages objectives. Car parking should be located behind the building or at basement level.

3.1.5 Building Entrances and Lobbies

New development involving multi-storey and mixed use development, should provide safe and accessible building entry from the street. The controls for building entrances and lobbies are provided to ensure appropriate design of building entry.

Objectives

1. To ensure entrances establish a distinguishable address and outlook to the public domain.
2. To provide safe, high quality building entry points and lobby areas that contribute to the street frontage.
3. To provide all weather protection to all building entry and lobby areas.
4. To ensure compliance with Crime Prevention Through Environmental Design principles.

Controls

- a. All entrances are to be clearly visible and identifiable from the street and public areas. Use of colour, contrasting materials and articulation in the building design can assist in entrance visibility. Figure 3.4 provides preferred lobby layout principles.
- b. Building lobbies must be accessible from a continuous path of travel.
- c. The lobby area is to have a separate and identifiable street entry, at ground floor level from the footpath.

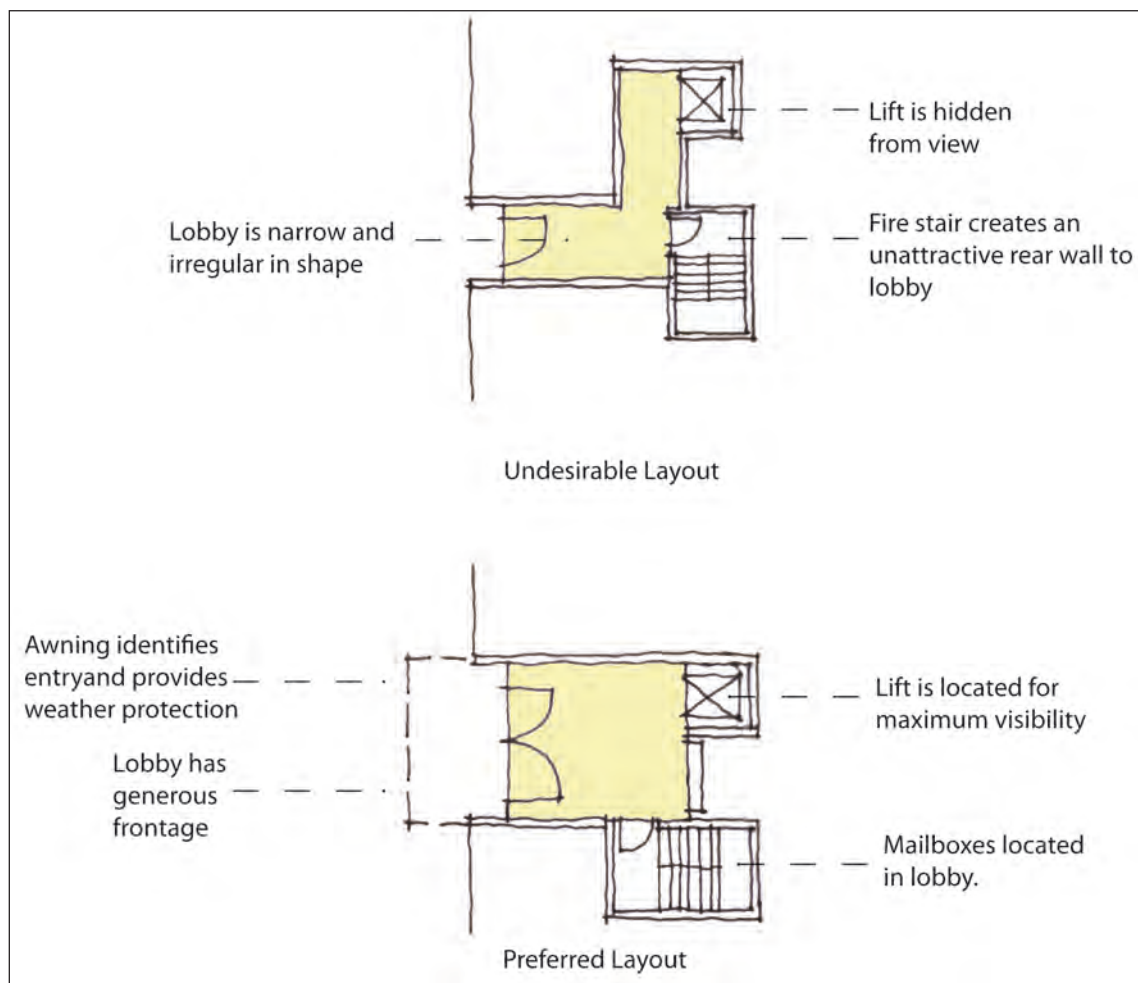


Figure 4.3.05 Lobby Design Principles

3.1.6 Active Street Frontages and Street Address

Active street frontages provide for interesting and safe pedestrian environments. Busy pedestrian areas and non-residential uses such as shops, studios, offices, cafes recreation and promenade opportunities promote the most active street frontages.

Active street frontages and street addresses are critical to the viability and vitality of the West Ryde Town Centre as direct, easy access from the footpath draws people from the street into premises. Active street frontages also add to the safety and security of a street by enabling casual surveillance.

Objectives

1. To maximise active street frontages and street address.
2. To retain and reinforce the continuity of activities along the street.
3. To clearly define corner sites and contribute to the street address through design and facade features.
4. To ensure buildings positively relate to surrounding development and enhance the quality and character of streetscape.
5. To ensure buildings situated on corner allotments address the intersections which they front.
6. To provide building facades that are of high architectural value and of visual interest to contribute to the character of the street and public domain.

Controls

- a. Active street frontages are required along those property frontages identified in Figure 4.3.06. Active frontages should be achieved using one or a combination of the following uses at street level:
 - retail shop front;
 - entrance to a retail arcade;
 - frontage to open space;
 - glazed entry to a commercial or residential lobby;
 - cafe or restaurant;
 - outdoor dining areas; and
 - active office uses (including community uses).
- b. Active ground floor uses are to be at the same general level as the footpath and be accessible directly from the street.
- c. Building facades are to be designed to maximise activation, movement and lighting within the public domain.
- d. Developments on corner allotments should incorporate a significant architectural feature to address the corner such as a wrap around verandah, upper storey balcony, bay window, corner entry or roof feature.



Figure 4.3.06 Active Street Frontages

3.1.7 Awnings

Awnings encourage pedestrian activity along streets as they provide shelter and shade, enhance amenity, protection, comfort and usability of footpaths. In addition, awnings provide streetscape continuity and reduce the perceived bulk of development.

Objectives

1. To provide awnings that shelter pedestrians along all public streets and retail areas.
2. To provide continuous awnings that define the street frontage and encourage pedestrian activity.
3. To enhance the quality of the streetscape through a consistent approach to awning design.

Controls

- a. Awnings should be provided along street frontages as shown in Figure 4.3.06 to contribute to active street frontages.
- b. New awnings are to be designed to:
 - i. be continuous for the entire length of the site frontage;
 - ii. be set back from the face of the kerb by 0.6 m;
 - iii. be weather sealed to the face of the building to which they are attached and to the adjoining awning
 - iv. have a height clearance above the footpath level of at least 3 m or a height consistent with adjacent awnings; and
 - v. maintain sufficient clearances from any overhead electricity or telecommunications installations.
- c. Awnings are to step in response to changes in street level, and may highlight building entrances. Otherwise awnings should be relatively level and should continue the alignment of other adjacent awnings.
- d. All awnings to provide under awning lighting to enhance public safety and to facilitate night use of the Town Centre.

3.1.8 Balconies

Balconies provide elementary architectural features that contribute to the form, character and style of buildings and streets. They provide articulation, visual interest as well as an important source of private open space.

Objectives

1. To provide private open spaces that also contribute to outlook and enliven the streetscape.
2. To ensure balconies are integrated into the design of buildings, function and respond to the local context and environment.
3. To provide opportunities for overlooking to streets and public open space.

Controls

- a. In mixed use and residential apartment buildings involving more than 20 dwellings, at least one balcony or courtyard per apartment is to be provided off the living area.
- b. In larger development, balconies should provide different styles and designs to provide visual interest to the facade.

3.1.9 Visual Privacy and Acoustic Amenity

Design measures are incorporated to protect the privacy and amenity of occupants of residential apartments or serviced apartments. Acoustic privacy is a measure of sound insulation between residential apartments and between external and internal spaces. It is important in a mixed use environment to ensure that the noise levels between neighbouring properties are respected.

Note: Future development is to take into account the provisions outlined in SEPP 65 - Design Quality of Residential Apartment Development and Development Near Rail Corridors and Busy Roads - Interim Guideline.

Objectives

1. To ensure adequate visual and acoustic privacy of residential development in the Town Centre and to associated private open space.
2. To ensure that the siting and design of residential buildings minimises noise transmission from abutting railway lines, major roads or other major noise-generating land uses.
3. To minimise the risk of noise and vibration impacts on noise sensitive developments located near the rail corridor and major arterial roads.
4. To encourage building design to provide for public safety and security, while maintaining the quality of the streetscape.
5. To reduce opportunities for crime through crime prevention and environmental design principles.
6. To reduce the impact of road and rail related noise on surrounding retail and residential developments.
7. To meet RailCorp and State Environmental Planning Policy (Infrastructure) 2007 requirements to maintain safety and operation of the rail network.

Controls

Visual Amenity

- h. Orientate the main living spaces within apartments to the street and/or communal open space (in designing the layouts this will need to be balanced against other criteria such as solar access).
- i. Proposed development should address the design principles outlined in the NSW Police Service's Crime Prevention through Environmental Design (CPTED).
- j. Development design should incorporate the following techniques to increase public safety and security:
 - i. Provide active uses wherever possible at ground level;
 - ii. Avoid blank walls onto streets, or large building setbacks with no visual supervision;
 - iii. Maintain strong view corridors along streets, laneways and pedestrian linkages;
 - iv. Provide high levels of lighting in carparks;
 - v. Provide passive surveillance by locating entrances and living areas where surveillance is limited;
 - vi. Locate entrances and living areas to provide surveillance of the public domain;
 - vii. Provide well lit entrances and main walkways, with appropriate landscaping;
 - viii. Use physical barriers or other methods to deter people from entering unsafe spaces; and

- ix. Design lighting to ensure it does not produce glare or dark shadows. This can be achieved by the following:
- use diffused lights and/or movement sensitive lights;
 - direct these lights towards access/egress routes to illuminate potential offenders, rather than towards buildings or resident observation points;
 - lighting should have a wide beam of illumination, which reaches to the beam of the next light, or the perimeter of the site or area being traversed;
 - as a guide areas should be lit to enable users to identify a face 15 metres away; and
 - illuminate possible places for intruders to hide.

Acoustic Amenity

- k. Where residential development is proposed in proximity to a major road, railway lines or major noise generating activity, appropriate materials with acoustic properties should be incorporated in the design of the dwellings.
- l. Council may require a noise and vibration assessment to be undertaken for development applications for noise generating developments or for residential developments on sites adjacent to noise generating sources such as rail corridors.
- m. Development must comply with noise and sound insulation requirements under BCA & AS3671-1987: Acoustics - Recommended Design Sound Levels and Reverberation Times for Building Interiors. Design features may be used to achieve primary acoustic privacy.

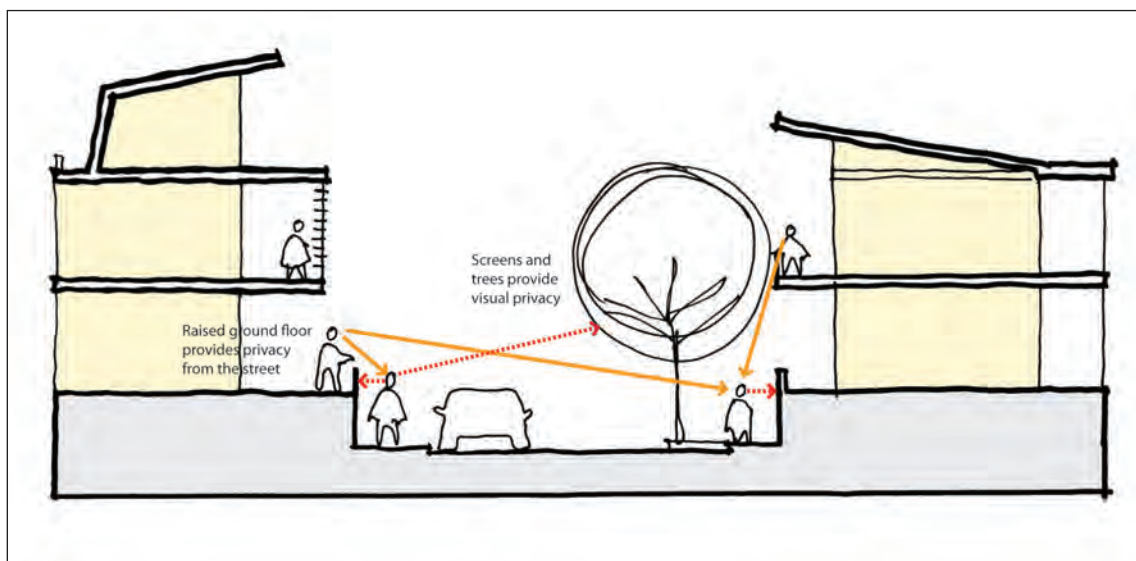


Figure 4.3.07 Casual Surveillance and Privacy Design Principles

3.1.10 Housing Choice and Mix

A choice of apartment types and mix of sizes in the town centre caters for a variety of household types. A range of dwelling sizes and types create a housing mix that will cater for a diverse population, as well as provide for changing use over time.

Objectives

1. To ensure that residential development provides a mix of dwelling types and sizes to cater for a range of household types, including families
2. To ensure that dwelling layout meets the needs of the occupants and is sufficiently flexible to allow for changing needs and activities over time.

3. To ensure the provision of housing that will, in its adaptable features, meet the access and mobility needs of any occupant.
4. To encourage a range of new housing with good access to the city centre..
5. To ensure Residential Flat Buildings respond to the **Apartment Design Guide** and SEPP 65.

Controls

- a. Developments comprising residential uses must provide a variety of residential units mix, sizes and layouts within each residential development.
- b. The total number of studio units and one-bedroom apartments/dwellings within any development must not exceed 25% of the total number of apartments/dwellings.

*The **Apartment Design Guide** provides examples of appropriate unit sizes which will be used to assess the appropriateness of unit size proposed in any development.*

3.2 Traffic, Access and Pedestrian Amenity

This section outlines controls for access and pedestrian amenity for new development.

3.2.1 Vehicular Access

The location, type and design of vehicle access points to a development can have significant impacts on streetscape, site layout, building facade design and pedestrian activity.

The design and location of vehicle access to development should minimise conflicts between pedestrians and vehicles on footpaths, and reduce visual intrusion and disruption of streetscape continuity.

Objectives

1. To ensure safe and efficient access to properties and limited impact on existing traffic movement.
2. To minimise the impact of vehicle access points and driveway crossovers on streetscape amenity, pedestrian safety and the quality of the public domain.
3. To reduce impact on traffic flow and movement along the major transport corridor of Victoria Road.
4. To minimise conflict between vehicles and pedestrians within the town centre.

Controls

- a. Vehicle access is to be designed to:
 - i. Minimise the impact on the street, site layout and the building facade; and
 - ii. Be integrated into the building design, if located off a primary street frontage.
- b. Vehicle access to properties should be provided from lower order roads or rear lanes where possible.
- c. New property access to Victoria Road will be permitted only where it is determined that access from a lower order road or laneway is not possible or would result in a detrimental impact to the surrounding traffic network. Access point must be RMS compliant.
- d. Vehicle access points are to be minimised as much as possible, particularly within mixed

use developments and residential flat buildings. Where practicable, buildings should share, amalgamate, or provide a rear lane for vehicle access points.

- e. For large scale development, all vehicles must be able to enter and leave the site in a forward direction without the need for complicated turns.
- f. Vehicle access points should inflict the least amount of impact on pedestrian movement, especially movement corridors surrounding the railway station.

3.2.2 Pedestrian Access

Pedestrian links throughout the town centre will enhance the public domain and legibility of the centre. Direct through-site links will improve access between transport nodes, retail areas and civic uses. The improvements in the public domain network and ease of access to key features will improve the town centre's desirability.

Within the West Ryde Town Centre Core and Ryedale Road Precinct, the following hierarchy of pedestrian circulation should be established:

- i. Pedestrian Town Centre;
- ii. Pedestrian Priority Area;
- iii. Key Intersection; and
- iv. Major Vehicular Intersection.

Objectives

1. To improve pedestrian comfort in the town centre.
2. To create a safe environment for pedestrians in the town centre,
3. To create attractive, convenient and safe pedestrian linkages that allow easy movement throughout the West Ryde Town Centre.

Controls

- a. Pedestrian links are to be provided in accordance with the Pedestrian Circulation Framework (refer Figure 4.3.08) and the City of Ryde Public Domain Technical Manual.
- b. Pedestrian Refuges shall be provided where directed by Council and to Council satisfaction. This includes at the intersection of Chatham Road and Dickson Avenue.**
- c. Where circulation is provided through a site or within a building serving to connect two points, the thoroughfare should function as a shortcut, be continuous and level with public pedestrian areas and incorporate an active edge of retail or commercial uses.
- d. Through-site links can be provided by plazas, arcades, colonnades or tree lined passages or a combination of these.

Note: Consideration will be given to the provision of pedestrian links additional to those outlined in the Pedestrian Circulation Framework, where development has frontages to two streets or provide an opportunity to extend the existing network.

- e. All pedestrian access areas and footpaths adjacent to new development will be required to be reconstructed using paving treatment in accordance with Council's requirements. The design, finish and element of any new through site links and access ways to be in accordance with Council's Public Domain Technical Manual.
- f. Council encourages the provision of through-site pedestrian links throughout the Town Centre Core.

- g. Buildings should be designed to limit overshadowing of major pedestrian spaces such as the proposed village square/civic space and the southern portion of Graf Avenue, Ryedale Road and areas fronting Anzac Park.
- h. Internal pedestrian links should, where practical and feasible, make provision for natural light.
- i. Distinctive paving treatment, bollards and other street furniture should be created in the retail core where pedestrians and vehicles mix. The following streets should be considered for such treatment:
 - Graf Avenue;
 - Market Street;
 - Anthony Lane;
 - Ryedale Road;
 - Chatham Road; and
 - Anzac Lane.



Figure 4.3.08 Core Town Centre Pedestrian Circulation Framework

3.2.3 Bicycle Facilities

In recognition of a more sustainable place and the creation of extended pedestrian and bicycle networks, the following objectives and controls seek to better accommodate bicycle facilities.

Objectives

1. To encourage the use of bicycles for trips that might otherwise involve the private motor vehicle.
2. To ensure the facilities required to support bicycle users are provided.
3. To ensure that appropriate facilities are available to permit safe and convenient storage of bicycles.

Controls

- a. Bicycle storage racks are to be provided to accommodate a minimum of:
 - 1 bicycle space for every 200 square metres of office floor space;
 - 1 bicycle space per 300 square metres of retail; and
 - 1 bicycle space for every 3 residential units.
- b. Bicycle racks must be easily accessible from the public domain, and within areas that are well lit with adequate levels of natural surveillance.
- c. The bicycle parking area must be capable of being made secure to protect the security of cyclists and their belongings. Communal showers, changing facilities and lockers for storing cycle attire and equipment may be required.
- d. Notwithstanding (b) and (c) above, bicycle storage facilities for residential uses can be provided within private garage areas, where it is demonstrated that:
 - there is sufficient storage within the garage for a bicycle and the required number of vehicles; and
 - there is a safe path for cyclists to leave the garage area.
- e. Bicycle facilities are to be in keeping with the City of Ryde Public Domain Technical Manual.

3.3 Environmental Controls

This section outlines the environmental controls that are to be met by all new developments.

3.3.1 Solar Access

Solar access is a major contributor to environmental comfort and amenity in homes, retail and commercial office space and the public domain. Good passive solar design solutions offer a resource and financial benefit by reducing the need for artificial lighting, heating and cooling.

Objectives

1. To encourage the use of renewable energy sources in the centre.
2. To maximise the amount of natural light in pedestrian areas, public open spaces and residential dwellings during the winter months.
3. To maximise the use of natural light to reduce energy consumption.
4. To minimise the need for artificial lighting during daylight hours.

Controls

- a. All developments must provide shadow diagrams that accurately describe the overshadowing impact to adjacent buildings and public domain areas.
- b. Demonstrate access to sunlight is to be substantially maintained so that existing private and public open spaces, footpaths and existing windows to habitable rooms in adjoining buildings receive at least 3 hours of sunlight between 9 am and 3 pm on 21 June (winter solstice).
- c. Major public open spaces are to be designed to receive a minimum of 50% sunlight on the ground plane for at least 2 hours between 10 am and 2 pm on 21 June.

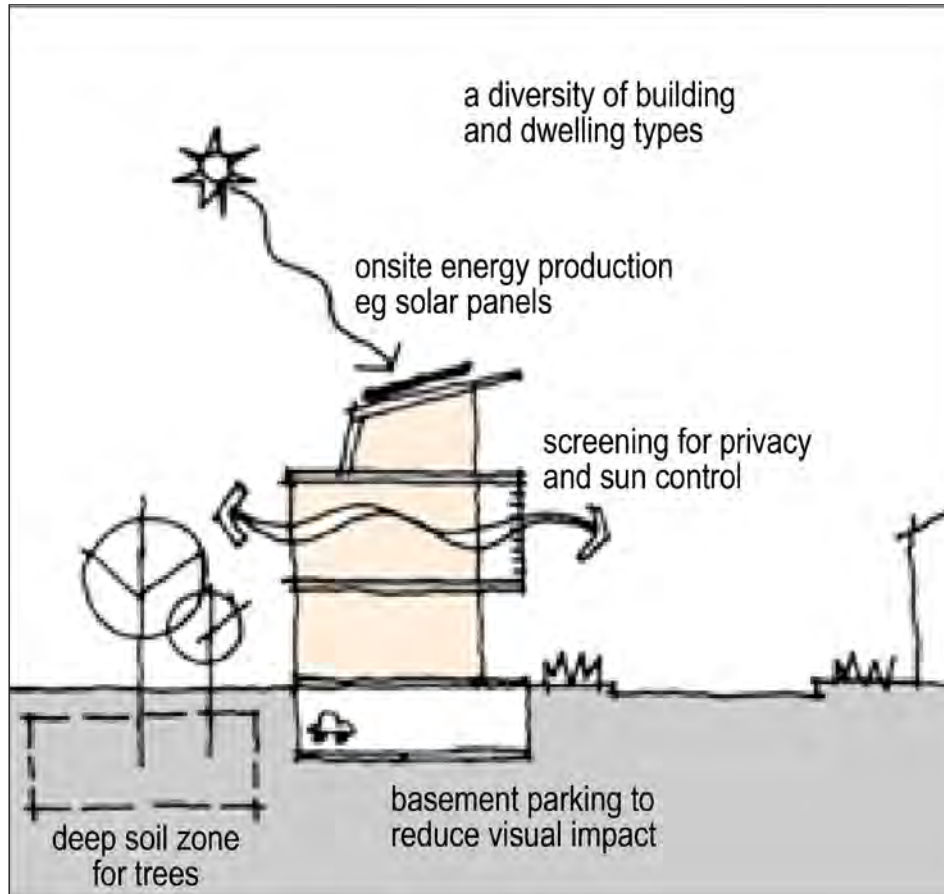


Figure 4.3.09 Site Design Principles for New Development

3.3.2 Natural Ventilation

Natural Ventilation contributes to environmental amenity and comfort, as well as reducing the need for artificial heating and cooling. Incorporating opportunities for natural ventilation in new development is important in ensuring adequate thermal comfort and reducing demand for mechanical heating and cooling.

Objectives

1. To reduce the use of mechanical means of heating and cooling to minimise energy consumption.
2. To ensure natural ventilation is available to all habitable rooms of a dwelling.
3. To allow the opportunity for mixed modes of ventilation in commercial buildings.

Controls

- a. In locations where wide footpaths already exist, or are proposed, ensure ground floor shops can satisfy natural ventilation requirements for operating a restaurant.
- b. Design of commercial developments must incorporate mixed modes of ventilation.

3.3.3 Stormwater Management

Objectives

1. To appropriately manage stormwater and runoff from sites.
2. To minimise the likelihood for flooding and the potential effects of flood events.
3. To ensure any future development does not exacerbate flooding on other properties or increase stormwater run-off.
4. To ensure public open space operates efficiently and without disruption during high levels of rainfall and the potential impacts of flood events.
5. To encourage development with high sensitivity to flood damages or danger to life to be sited and designed so that it is subject to minimal flood hazard, and the unnecessary impediment of stormwater.
6. To ensure that the design and siting controls, and built form outcomes required to address the flood hazard do not result in unreasonable impacts on the:
 - amenity and character of an area;
 - streetscape and the relationship of the building to the street;
 - social and economic outcomes; and the
 - environment and ecology
7. To minimise the amount of run-off generated by new development and inconvenience to other properties.
8. To retain stormwater on site, where possible, and minimise downstream runoff from industrial sites.
9. To ensure future built form is water efficient, reducing water usage and incorporating water recycling in day to day operations.
10. To encourage Water Sensitive Urban Design (WSUD) principles to be incorporated into the design of stormwater drainage and in the orientation of development.

Controls

- a. All stormwater drainage is to be designed in accordance with Part 8.2 Stormwater Management of this DCP.
- b. A Stormwater Management Plan is required to be submitted with all development applications.
- c. Access to underground parking is to be designed with consideration to flood levels and impact on the street frontage.
- d. All new development should meet best practice targets for stormwater management set out in Managing Urban Stormwater (The Blue Book) by Landcom.
- e. Runoff which enters a property from upstream properties must not be obstructed or impeded from flowing onto the site and must not be redirected so as to increase the quantity or concentration of surface runoff entering adjoining properties.
- f. Where appropriate enable the installation of grey water collection treatment on site, so that waste water can be re-used for non-potable purposes, such as toilet flushing and irrigation of gardens and landscape.

3.4 Public Domain

The public domain is the public space in West Ryde Town Centre, the public face and setting for buildings and structures. It is the parts of the town centre not privately owned. The goal of public domain design is to create an integrated space that is legible, comfortable, safe and engaging; which encourages pedestrian use and increases the amount and quality of public leisure spaces.

3.4.1 Street Furniture, Paving & Street Lighting

Street furniture includes lighting, seats, bus shelters, benches, litter bins, telephone booths, drinking fountains, street signs, etc. and are to be used to establish an identity for West Ryde and define roads, paths and gateways.

Street lighting is essential in promoting a safe public domain, in order to clearly define entry points to buildings and public spaces, and to promote activity at night.

Objectives

1. To create visual unity in the design and appearance of public spaces in the centre.
2. To provide comfort and convenience for pedestrians in the centre.
3. To ensure clear separation between roadway and parking areas for legibility and safety on streets with a continuous flow of traffic, particularly for vision impaired people.
4. To provide consistent paving in order to unify the town centre.
5. To promote a well-lit, safe and vibrant public domain at all times.

Controls

- a. Developments which entail the provision of new public spaces (i.e. streets, footpaths, walk ways and the like) will need to incorporate new street furniture and paving and in some cases underground power lines and new light poles in the public space.
- b. Street furniture, paving, underground power lines, and lighting should be designed and installed in accordance with the City of Ryde Public Domain Technical Manual.
- c. Provide a pavement surface which is consistently graded both along and across the pedestrian route.
- d. Use tactile indicators in paving with discretion, considering the needs for all pedestrians.
- e. Council encourages lighting, located approximately 2 to 2.5 metres apart, above ground level located on building walls, awnings or other appropriate structures to minimise shadow from built form and structures.
- f. The multifunction pole lighting system is to be used in the West Ryde Town Centre in accordance with council requirements. The multifunction pole will incorporate lighting, street signage and banners into one element.

3.4.2 Street Tree Planting and Landscaping

Street trees can improve legibility in the urban environment by reinforcing the hierarchy of streets and enhancing sense of place. Placement of trees affects light and shadow, colour and views, which contributes to the quality of pedestrian experience. Trees also contribute to environmental quality in many ways.

Objectives

1. To create attractive public spaces and walkways.
2. To soften the appearance of buildings and improve the visual quality of the retail core.

Controls

- a. All development proposals are to be accompanied by a landscape plan prepared by a qualified and suitably experienced landscape architect.
- b. Where appropriate, developments should incorporate landscaping in the form of planter boxes on the upper levels of buildings to soften the building form (i.e. roof gardens, planting on structures).
- c. Ground level entry areas to upper level dwellings shall be well lit and not obstructed by planting in a way that reduces the actual or perceived personal safety and security of building occupants or pedestrians.
- d. Street trees shall be provided in accordance with the City of Ryde Public Domain Technical Manual and shall be provided at the developers' cost in conjunction with any new building work involving additional floor space.
- e. Street tree species must be selected for their hardiness under adverse and polluted conditions, to provide screening to pedestrians and residents from traffic, and to improve the visual quality of the area.
- f. Street trees at the time of planting shall have a minimum container size of 200 litres, and a minimum height of 3.5 m, subject to species availability.

3.4.3 Public Art

Public art in the urban environment can provide an essential reference point to a place's civic image and positioning. It can make urban spaces attractive and welcoming, promote local identity, link private and public domains, increase pedestrian activity and connectivity, evoke business confidence and attract investment. Good public art can be a destination in itself.

The strong heritage associated with West Ryde provides a spectrum of ideas that can be explored and expressed through public art. Artworks can be used to create emblems or symbols that depict and promote the identity of West Ryde, to distinguish particular developments, enable new businesses to develop signatures, identify entry points and generally stimulate the interaction of ideas that are central to the vision of the area.

Public art, while permanent in its appearance and structure, is also an installation which may change over time.

Strategy

The following principles should support the development of public art in West Ryde:

- Public art reflects local character and cultural identity, creating distinctive urban environments and a sense of place;
- Public art can strengthen and connect neighbourhoods by engaging communities in creative processes;
- Public art is original, creative and innovative in its design and use of form, technique and materials, and at the forefront of new ideas and sustainable practice;
- Public art is inclusive, non-partisan and secular in its subject matter;
- Public art shall be funded, commissioned and attended in a way that encourages artistic excellence and upholds the design intent of the artworks;
- Public art shall comply with all measures and standards in regard to health and safety, maintenance, longevity and durability.

Objectives

1. To include site-specific integrated public artworks in new developments in West Ryde
2. To develop iconic points of reference or focal points that promote identity and add to the enjoyment and experience of West Ryde.
3. To contribute positively to site and surrounds, and respond to the natural and built environment.

Controls

- a. Public art is to be in keeping with the City of Ryde Public Domain Technical Manual and the City of Ryde Public Art Policy.
- b. Public art must be included in all new mixed use development with an estimated construction value of more than \$20 Million.
- c. A site specific Arts Plan is to be submitted together with a development application.
- d. Requirements for the provision of public art and the format of an Arts Plan are to be confirmed with Council prior to lodging a Development Application.

4.0 PRECINCT DEVELOPMENT CONTROLS

The area of the West Ryde Town Centre and adjoining areas have been divided into a number of precincts which reflect the differing urban character of West Ryde. These urban precincts include:

1. Victoria Road West
2. Retail Core
3. Ryedale Road
4. Anzac Park
5. Victoria Road Mixed Use
6. Victoria Road Enterprise Corridor

Each of these precincts reflect different urban character and functions, varied natural and physical settings and serve different purposes. As such, it necessary to provide specific details for each precinct to supplement the general controls specified in Section 3.0



Figure 4.3.10 DCP Precinct Boundaries

4.1 Victoria Road West

4.1.1 Character Statement

The Victoria Road West Precinct will be a vibrant, lively area, providing the primary traffic and transit corridor to West Ryde, and serving as a prominent feature of the Town Centre. The precinct will continue to provide a diverse range of retail and business uses for the community. New development will draw on the existing pedestrian environment. Built form is to follow and reinforce the established street alignment of the Victoria Road West Precinct. Opportunities for larger format retail and commercial premises are located on the southern side of Victoria Road due to the availability of larger sites. The existing small retail shops on the northern side of Victoria Road provide opportunities for small start up businesses.

Active street frontages along Victoria Road West must be retained with any new development, promoting a safe and active environment. Retention of the existing character for the retail sector of Victoria Road West should be prioritised by allowing for small to medium retail units with narrow frontages presenting to the street at ground level. Revitalisation of the precinct is encouraged through the enhancement of the public domain.

Future development will assist in establishing Victoria Road West as a 'landscaped route', defined by new activity and built form.



Figure 4.3.11 Victoria Road West Precinct Boundary

Objectives

1. To enhance the role of Victoria Road West as a commercial/retail corridor of West Ryde Town Centre.
2. To create opportunities for new mixed use developments on the northern side of Victoria Road.
3. To support the activities of the primary West Ryde commercial/retail core adjoining.
4. To encourage maximum development potential through the consolidation of allotments.
5. To ensure future development reflects the surrounding residential development.
6. To ensure the built form of Victoria Road West caters for small and medium scale business opportunities, providing opportunities for small and start up businesses.

Controls

- a. All future development should recognise and address the residential/commercial interface, with a clear transition between high density and lower scale residential density development.

4.2 Retail Core

4.2.1 Character Statement

The Retail Core is the primary retail centre for West Ryde. This precinct provides an important retail and commercial centre for the surrounding West Ryde locality and adjoining residential areas.

The Retail core is encouraged as a main shopping centre as up to approximately 14,000 m² gross leasable floor area focuses on the provision of food items, basic goods and community services in a central location.

Future development in the retail core is encouraged to include a mix of ground level commercial and retail, combined with residential units above. This typology provides an enhanced pedestrian environment and helps to enliven and activate the street environment.

The Retail Core Precinct will provide the focus for high intensity and large footprint mixed use development with a diversity of retail, commercial, residential and civic services with a focus for large scale employment uses.

The edge of the Retail Core Precinct performs a transitional role, with new development to be suitably designed to maintain the amenity of adjoining residential land uses.

New development is to be designed and oriented to maximise and improve visual and physical connections across the Precinct and to ensure community focal points in the private domain interact with the surrounding network of civic spaces and linkages.

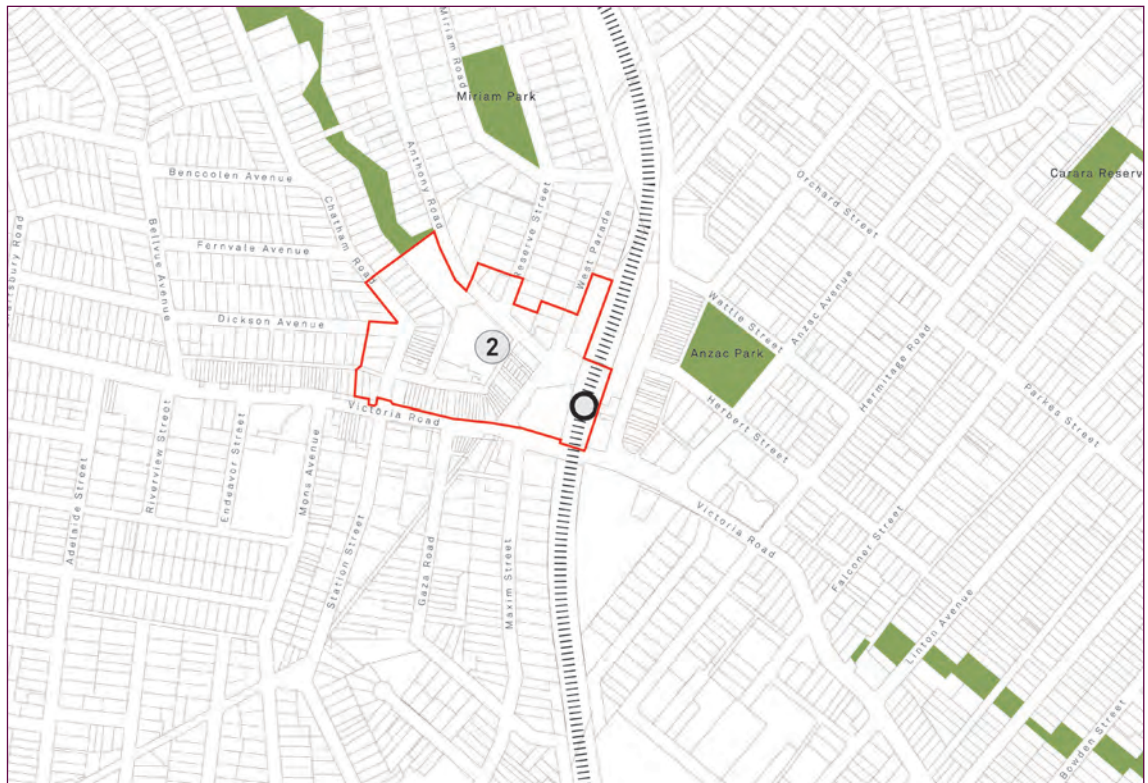


Figure 4.3.12 Retail Core Precinct Boundary

4.2.2 Urban Design

Objectives

1. To encourage mixed use development within the Town Centre where appropriate.
2. To encourage a safe and secure pedestrian-oriented environment.
3. To provide opportunities for a range of commercial/retail uses at ground level.
4. To establish a retail core which services, and is compatible with the surrounding residential areas.
5. To provide the primary commercial and retail centre of West Ryde.
6. To increase diversity in housing opportunities within the West Ryde Town Centre through new mixed use developments.
7. To maintain a diversity of access opportunities and formalise the functions and use of thoroughfares.
8. To maintain and enhance the linkages between the retail core and railway station.

Controls

- a. All development should provide flexible building layouts which facilitate variable tenancies or uses on the first floor of a building above the ground floor.
- b. New development should include retail activities at ground level to maximise activity at street level.
- c. Built form of new development must follow and reinforce the established street alignment to provide a continuous building line for the town centre.

- d. Laneways and arcades must be enhanced and maintained in order to provide clear and accessible pedestrian environment.
- e. Development within the retail core is to ensure a transition to surrounding low density residential areas is achieved.



Images 1 (a), (b) & (c) Precedent Images

4.2.3 Public Domain

Objectives

1. To build on the character of the retail core and the amenity of the public domain.
2. To promote pedestrian activity and safety in the public domain.
3. To provide visual interest and richness in architectural detail.
4. To encourage an address to the street outside of areas where active or street frontages are required.

Controls

- a. Public areas should have direct access from the public domain.
- b. Provide active ground floor uses including well articulated ground floor entrances to buildings.
- c. Building facades are to be of high architectural value and of visual interest to contribute to the character of the street and public domain.
- d. Buildings are to be articulated and are not to present long unrelieved structures that dominate the landscape. All street frontages are to be activated by light, activity, glazing, building articulation or materials to create visual interest.

- e. Provide a linear urban plaza incorporating the 4-metre active street frontage setback along the Chatham Road Key Pedestrian Area (as identified in Figure 4.3.04). The urban plaza is to be designed in accordance with the Public Domain Technical Manual and to Council's satisfaction (refer to Figure 4.3.12a below).

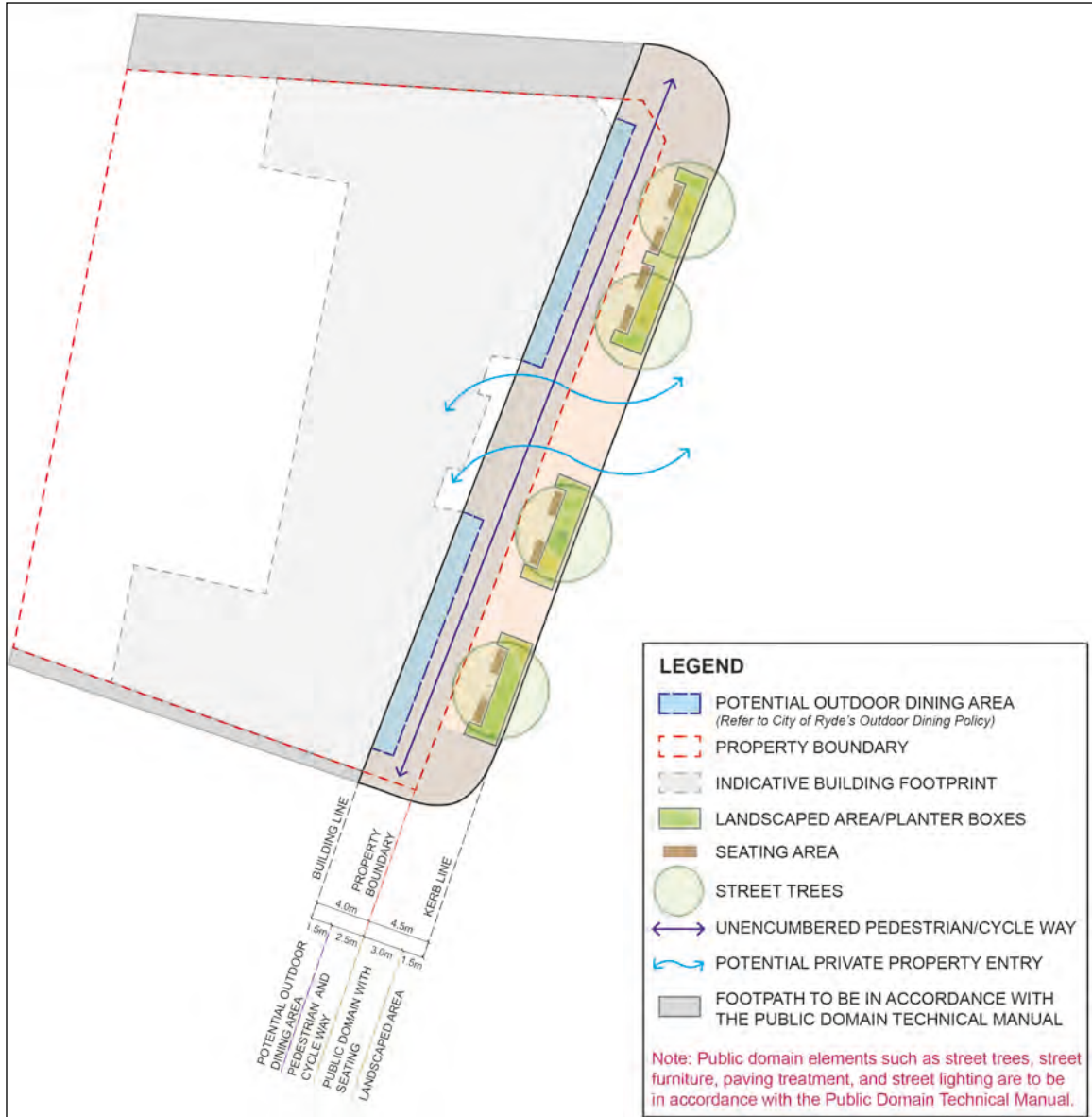


Figure 4.3.12a Indicative Concept - Linear Urban Plaza

4.2.4 Public Car Parking

Objectives

1. To provide a reasonable amount of convenient car parking for general public use in the retail core.

Controls

- a. New car parking within the Retail Core Precinct Centre should be provided in a basement. Where this cannot be achieved, parking areas should be provided at ground level and be adequately enclosed and screened from street frontages.
- b. Car Parking associated with residential and retail and commercial land uses should be separate.

4.3 Ryedale Road

4.3.1 Character Statement

The Ryedale Road precinct is envisaged to be a future mixed use precinct benefiting from a variety of residential, commercial and retail development, and its close proximity to the rail corridor, the retail core of West Ryde, and Anzac Park.

Development on the east side of the railway will consist of a mix of ground level commercial and retail combined with residential units above. Increased heights are permitted in this precinct to capitalise on the proximity to rail. The open space of Anzac Park will balance the scale of the development, and makes the location an appropriate precinct for this type of built form.

The West Ryde Railway Station will activate surrounding businesses and create opportunities for new linkages, connecting Ryedale Road Precinct with the Retail Core. Active uses are to be promoted at the ground and lower levels of development to promote vibrancy and passive and active surveillance of the public domain.

New development adjacent to Anzac Park should enhance the interface with this open space location, ensuring opportunity for views to Anzac Park from the surrounding area, whilst minimising any adverse impacts on the open space. The precinct will also need to recognise the interface between the higher density residential development and the adjacent retail development.

Much of the Ryedale Road Precinct is a Heritage Conservation Area. Future development will also ensure an enhanced recognition and interpretation of the Heritage Conservation Area, as any future development should recognise the significance of the area and incorporate it into the design.

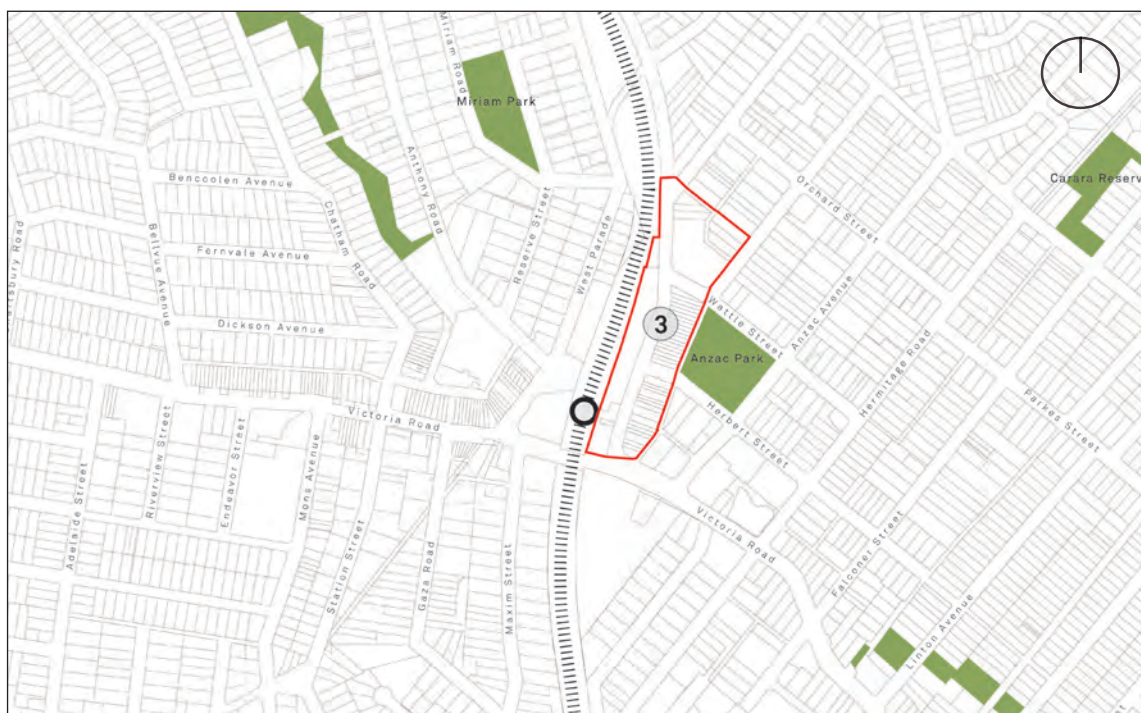


Figure 4.3.13 Ryedale Road Precinct Boundary

Objectives

1. To maintain and preserve the Ryedale Road heritage conservation area.
2. To ensure future development is sympathetic to the existing character of the precinct.
3. To enhance redevelopment opportunities for sites within the Ryedale Road precinct to achieve the maximum building envelope;
4. To recognise the historic fine grain pattern at street level.
5. To establish a neighbourhood level mixed use commercial and retail precinct which capitalises on accessibility to the retail core and West Ryde railway station.
6. To incorporate active uses at ground level, including cafes and restaurants, with residential dwellings positioned above.
7. To improve the accessibility and interface of the West Ryde Railway Station.
8. To help foster a distinctive community identity that reflects the history and enhances the heritage qualities of the local area.
9. To respond to the physical, cultural and urban heritage of the site.
10. To facilitate the provision of a diversity of residential, retail, and commercial uses.

Controls

- a. A Heritage Impact Statement is to be prepared as part of the redevelopment for a heritage item or site in the Ryedale Precinct heritage conservation area.
- b. Development along the residential interface boundary is to be sympathetic in scale and activity to the surrounding residential land uses, protecting the residential amenity.
- c. Future development adjoining the train station should incorporate improved pedestrian access, in order to provide a connection between Ryedale Road and the retail core.
- d. Development is to maintain the character and heritage significance of the heritage conservation area and ensure the infill development responds positively to the heritage character of the area.
- e. Where lot consolidation is proposed, applicants are to provide building envelopes that demonstrate the new development would not significantly impact upon the amenity, streetscape and desired future character, including:
 - adjoining sites are not isolated and retain access;
 - the objectives and principles of this section can be achieved;
 - adequate on site parking can be achieved to meet the parking demands of the development; and
 - the design and function of the development above podium level will achieve a high level of amenity to both its future occupants and to adjoining properties.



Images 2 (a), (b) & (c) Precedent Images

4.4 Anzac Park

4.4.1 Character Statement

The Anzac Park Precinct comprises a residential precinct surrounding the large open space area known as Anzac Park. Anzac Park is an important open space within the West Ryde Town Centre.

The precinct is encouraged for residential development with potential opportunities for ground floor mixed use and live/work adaptability, where the land is zoned for mixed use.

Future development should integrate with the important open space setting created by Anzac Park. New development should provide an attractive streetscape surrounding the park and provide opportunities to overlook the park.

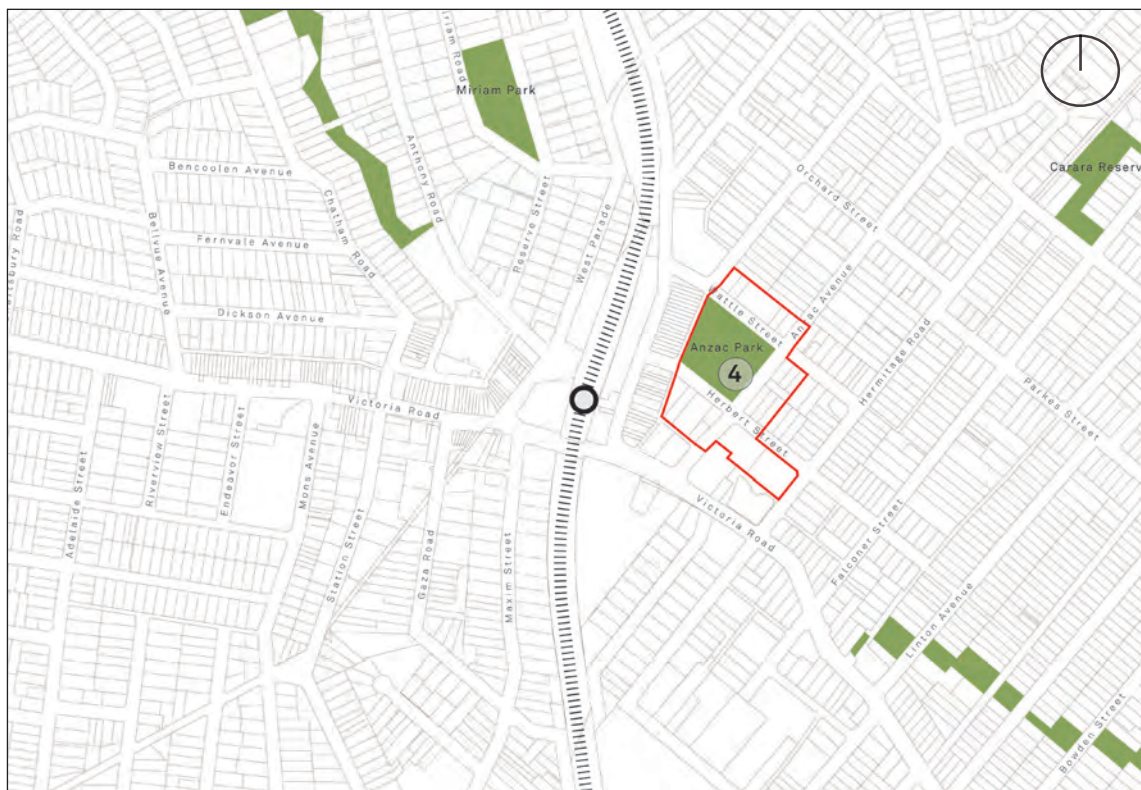


Figure 4.3.14 Anzac Park Precinct Boundary

Objectives

1. To encourage increased residential density around Anzac Park.
2. To reinforce the role and setting of Anzac Park as an important open space and passive recreation space within West Ryde.
3. To recognise the heritage values of the adjacent Ryedale Road Precinct.
4. To provide opportunities to increase pedestrian linkages with the retail core and Ryedale Road Precinct.
5. To provide a consistent built form edge that addresses Anzac Park.

Controls

- a. New buildings are to be designed to activate facades which interface with Anzac Park to promote casual surveillance and interaction (eg. design to include upper level balconies and low front walls).
- b. The bulk and scale of new development is to complement the surrounding residential location.
- c. New buildings within the Anzac Park Precinct which adjoin established residential areas are to provide a transition to these existing areas to maintain the amenity of adjoining residential land uses.



Images 3 (a), (b) & (c) Precedent Images

4.5 Victoria Road Mixed Use

4.5.1 Character Statement

Land to east of the Town Centre comprises a variety of uses along the Victoria Road corridor. The Victoria Road Mixed Use precinct includes several larger allotments on the northern side of Victoria Road providing commercial, retail and residential uses.

Sites within the precinct have been redeveloped recently, adopting a character of mixed use development, comprising of a variety of retail, commercial and residential land uses. This precinct provides a transition to higher density mixed use development to the west and low density residential land to the east, providing an important interface with Victoria Road.

Active ground level uses such as retail activities are encouraged along Victoria Road.

The precinct is slightly elevated above Victoria Road and presents as a prominent location when viewed from the eastern approach to the town centre. New development will assist in establishing a distinctive gateway to the West Ryde Town Centre.



Figure 4.3.15 Victoria Road Mixed Use Precinct Boundary

Objectives

1. To provide a transition from the high density commercial mixed use precinct of the town centre.
2. To assist in establishing a prominent visual gateway to the West Ryde town centre.
3. To provide opportunities for a variety of commercial, retail and residential activities within mixed use developments.
4. To recognise and respond to the elevated location of the precinct along Victoria Road through creation of a visually attractive setting.
5. To respond to the existing built form of adjacent heritage properties.

Controls

- a. New development should provide a primary interface to Victoria Road.
- b. The intention is to develop a mix of uses. This will be achieved by the following measures:
 - i. the precinct will encourage retail uses at ground level fronting onto Victoria Road and existing and proposed land uses;
 - ii. generally commercial uses will be provided on the second levels; and
 - iii. residential uses should be positioned on and above the third level.
- c. Balconies and other facade elements should be provided to the upper levels of buildings which front Victoria Road to increase visual interest to the street.

4.6 Victoria Road Enterprise Corridor

4.6.1 Character Statement

Land along Victoria Road, west of the rail line, provides an important gateway to the West Ryde Town Centre and supports predominantly commercial and residential activities. The precinct is opposite the West Ryde Industrial Area, an important employment and economic precinct for the West Ryde Area.

The land zoned as B6 Enterprise Zone along Victoria Road serves as an important commercial precinct which supports the industrial zone and protects the primacy of core commercial activities within the Town Centre. New development in this precinct will assist in strengthening the visual quality of the Victoria Road Enterprise Corridor.

Development in this precinct will comprise larger footprint commercial activities which are not appropriate for core Town Centre locations and which supplement the activities of the industrial zone. New development should exhibit high quality design which responds to the Victoria Road frontage, provides an important interface with low density residential and improves the visual quality of the corridor.

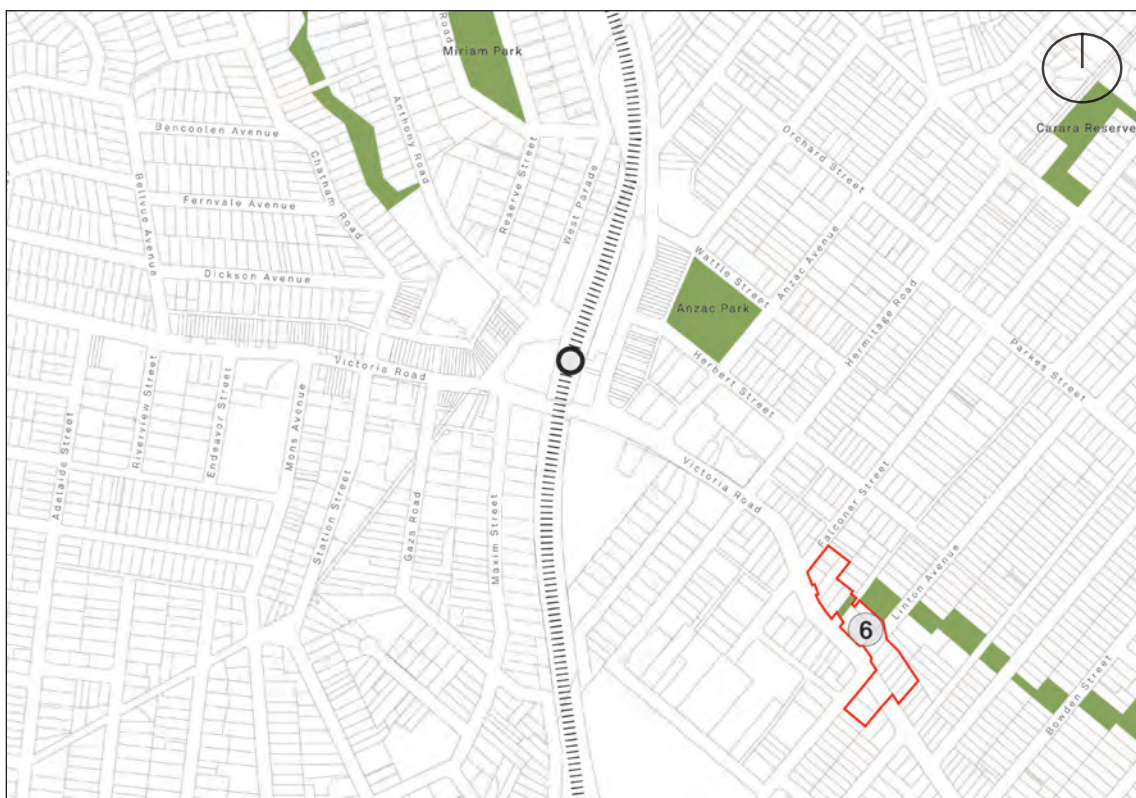


Figure 4.3.16 Victoria Road Enterprise Corridor Precinct Boundary

Objectives

1. To establish a transport corridor related business corridor along Victoria Road adjacent to industrial land uses.
2. To protect the primacy of core commercial and retail activities within the West Ryde Town Centre
3. To encourage establishment of activities which will support the industrial activities to the south west.
4. To encourage a built form and design which is of a high quality and defines the Victoria Road corridor.
5. To assist in defining the eastern gateway to West Ryde.
6. To support and supplement the light industrial activities of West Ryde
7. To ensure that any future extension of the existing use of the land is compatible with adjacent development.

Controls

- a. New development should address Victoria Road and provide use of glazing and contrasting material to provide visual interest to the street. Development should not provide blank walls to the street.
- b. All future development should recognise the presence of the industrial precinct.
- c. Buildings and public domain will delineate entry to the Town Centre through innovative design.
- d. New buildings within the Victoria Road Enterprise Corridor Precinct are to provide a transition to adjoining low density residential areas to maintain the amenity of those adjoining residential land uses.

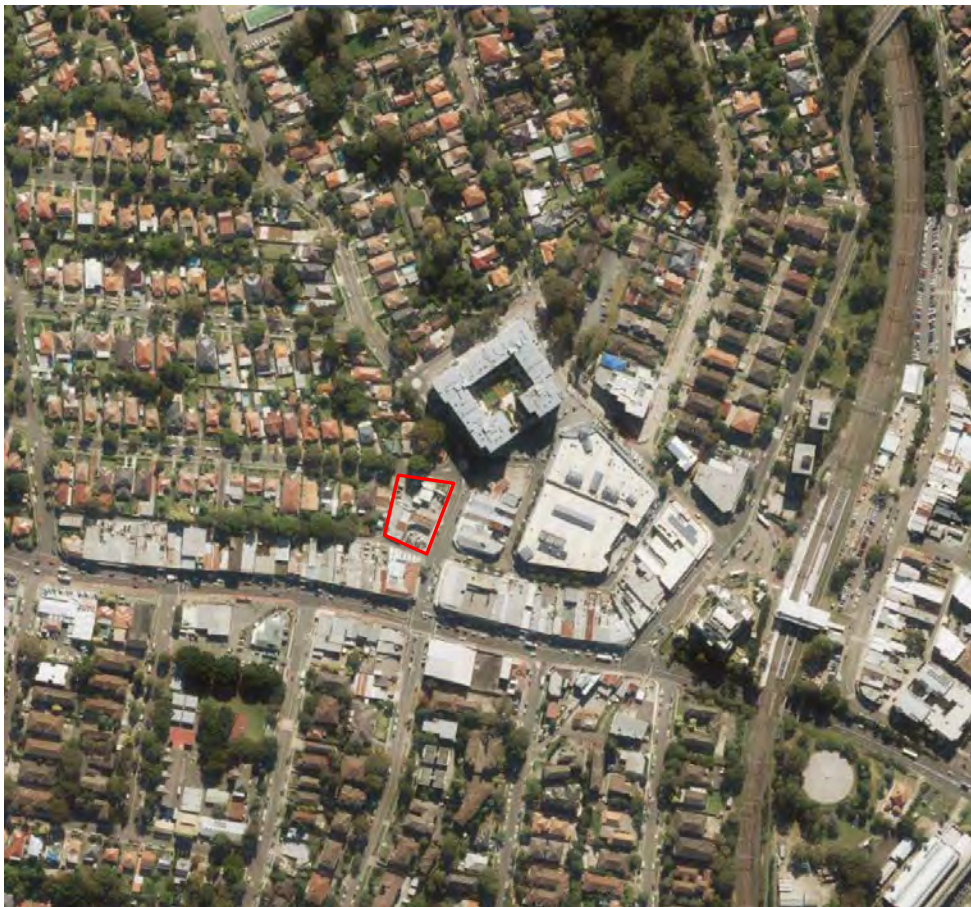
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City of Ryde
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www.ryde.nsw.gov.au

Urban Design Assessment Report



Mixed-use development 2-6 Chatham Road, West Ryde

Date: 4 June 2018

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1. Introduction

GMU have been engaged by Ryde Council to undertake an urban design review of the proposed Planning Proposal and draft Development Control Plan (DCP) amendments relating to the site located at 2-6 Chatham Road, West Ryde.

The subject site is located on the western end of West Ryde Town Centre and is zoned B4 - Mixed-use development, with a maximum applicable height of 15.5m and an FSR of 1.25:1. It comprises the following lots with a total land area of 1,967 square metres:

- 2 Chatham Road (LOT 24 DP 8092)
- 4 Chatham Road (LOT 25 DP 8092)
- 6 Chatham Road (LOT 26 DP 8092)

The site has three street frontages:

- Chatham Road – commercial/active street frontage;
- Dickson Avenue – residential street frontage; and
- Dickson Lane – a one-way (westbound) service laneway.

The subject site currently contains a service station and car wash addressing Chatham Road and a restaurant addressing the corner of Chatham Road and Dickson Avenue. The site has predominantly single-storey structures with little or no vegetation (five medium-sized palm trees).

In arriving at the opinions expressed in this report, GMU has undertaken review of the state and Council's strategies and applicable controls, a site visit including consideration of the context and an independent analysis of the high-level urban design impacts.

The comments within this report deal with issues of concern in relation to the proposed amendments to height and FSR demonstrated by the indicative concept plan and suggested amendments or inclusions for the proposed DCP amendments to address potential issues and ensure a quality outcome is achieved for the site and centre.

2. The Planning Proposal

The proposed planning proposal is to amend the height control for the site from 15.5m to 24m and the FSR control from 1.25:1 to 3:1.

An indicative concept plan is included illustrating a potential development outcome for this site for a 7-storey mixed-use development with commercial/retail ground floor and apartments above with 75 apartments, 750sqm of retail/commercial space on the ground level and 106 basement car spaces with the vehicle access ramp along the rear boundary adjacent to the low-density dwellings. The proposal also includes a 4m street frontage setback to Chatham Road intended as a linear plaza space.

3. Comments in relation to Planning Proposal and the indicative concept scheme

GMU has reviewed the concept scheme and the intended changes to the height and FSR controls in relation to this site. To assist Council and the applicant in understanding likely issues if this indicative concept was lodged formally, the following comments are provided. Review of the concept scheme also informs whether the proposed height and FSR in the Planning Proposal are reasonable given the context and interrelationships with adjoining lots and the public domain.

Bulk and scale:

The proposal seeks to significantly vary the current controls for West Ryde Town Centre. The proposal is to increase height from 15.5m up to 24m. This is accompanied by an FSR increase from 1.25:1 to 3:1. In determining whether this is appropriate we note that the location of the site is sensitive. It is at the western edge of the town centre, some distance from the rail station itself. It has a direct interface with the lower scale R2 zone to its western boundary as well as to the north across Dickson Ave. The location of the site does not play a prominent role within the centre itself as it does not mark a prominent access point. Currently the height strategy for the centre has this site as part of the height transition from the greater height towards the station to the low scale of the neighbouring residential dwellings. This site is also not overly large which affects its ability to deliver a scale transition and ensure appropriate amenity whilst also achieving the maximum height sought by the planning proposal.

The height strategy in the LEP for the town centre currently has heights concentrated at the railway station where 33.5m is possible as a maximum. This scale then reduces to 15.5m immediately to the western side and this height is constant across the remainder of the western centre. To the eastern side the height reduces to 23m and then to 15.5m as it approaches the low scale R2 zone at 9.5m.

On the northwestern side of the centre there is evidence of buildings that seem to exceed the height limit ie the Coles development at some 8 storeys. However this site is not immediately next to the R2 zone (it is separated by a roadway) and it does form part of the arrival to the town centre from the north for 2 streets. Therefore whilst this development appears to exceed the controls there is some logic based on the site location within the block and centre. The subject site however shares a direct boundary with the R2 zone so the treatment of scale and setback at this edge is very important.

The site is also relatively separate from the station. If greater height is appropriate within the centre it is more logical from an urban design viewpoint for this to occur close to the station itself (as happens on the other side of the station). Heights in the order of 23m are seen as the next height gradation and therefore a height of this order might have some basis for lots with frontage to West Parade. The site is 2 blocks from this location however.

The proposal also seeks a height greater than 23m – the proposal is for 24m. Given the position of the site on the town edge and its distance from the station there seems little justification or logic in such a significant increase away from the station and in a location where a reduction of height would be expected.

Such an increase in height should really not be considered without a holistic consideration of the western side of the town centre, if not the entire town centre as approval of a height of this magnitude will change the outcome for the centre from a urban form emphasising the station to one where the western side of the centre would adopt heights close to those at the centre with no transition in scale from this greater height to the low height of 9.5m to the west. Such a change in strategy would ideally be accompanied by an extension of the town centre boundary to move the transitional height location to west of the subject site.

Therefore we have concerns that translation of the height around the station to the town edges (outside a strategic review) is not appropriate and is likely to have adverse impacts in terms of scale and visual bulk to the R2 area, particularly adjacent sites and that it will create a height node in an inappropriate location..

It is relevant to consider the response to the R2 boundary provided in the concept plan to see if an appropriate design solution is offered to deal with this interface and the need for scale transition. Study of the concept plans shows that the scheme has a setback of only 6 and 9m proposed for the full 7 storeys (apart from a portion of a possible central courtyard) adjacent to Dickson Ave and the boundary with the R2 zone.

This relationship does not achieve a reasonable transition and the depth of the site makes it problematic to achieve an appropriate response. In such circumstances it is reasonable to expect that a concept would allow for a 4 storey height or some 12.5m as it approaches the western boundary as well as along the western part of the site addressing Dickson Ave. Otherwise under the guidance of the Apartment Design Guide the setback should at the very least be 50% of the separation distance plus 3m ie 9m and 12m (not 6 and 9m) as advocated next to lower scale zones to the significantly greater height. These changes would of course result in a reduction in the FSR sought by the applicant for the site.

Within the centre of the site the built form assumes a U shape which assists in providing some separation to the greater scale and at the lower levels provides this extra setback. However, the additional separation does not translate into the higher storeys. Care needs to be taken to ensure the width of the central courtyard is sufficient to ensure amenity and outlook to the units and that privacy can be assured. Therefore, the proposal at the FSR indicated is unlikely to be able to moderate its interface to the adjacent R2 zone successfully resulting in an inappropriate bulk for the height proposed.

The proposed increase in height also results in a tension between the other sites forming part of the town centre to the south and east relative to the subject site. To ensure a reasonable contextual response a concept proposal should allow for a street wall height and setback that acknowledges and respects the 15.5m height limit that will affect the rest of the centre in proximity to the site. However the concept plan offers no street wall at that height and rises to the new height for the entirety of the building. The result is a lack of human scale in the façade modelling and greater dominance in the building form than is appropriate.

A street wall height with a nil boundary setback (considering 4m front setback for the linear park along Chatham Road) should really be limited to the first 4-5 storeys (subject to the height of the ground floor) to ensure a reasonable streetscape response and then any taller form should be setback at least 3m behind to reinforce this scale. This should occur to all street and laneway frontages (apart from the western edge of Dickson St where the street wall and overall height should reduce to 3- 4 storeys.

The concept plan and FSR proposed appears to be resulting in a potential built form that is overly bulky and offers little articulation or visual interest particularly to the eastern façade. The concept continues unrelieved for the total 50m length of the building, ignoring the finer grain lot pattern seen in many areas of the town centre and to the west of the site which is likely to see narrow lot development solutions. The concept plan should allow for greater articulation and variation in the built form to avoid creating an unrelieved development facing the public domain.

Landscape, public domain and deep soil area

The concept plan indicates a street setback of some 4m to provide a linear park area along the Chatham Rd frontage. It is not clear why a linear park is considered necessary or desirable in this location however if Council considers this is strategically appropriate for the desired future character of the town centre then this could be a positive outcome if the concept plan responds to it well. From that viewpoint useable and well-considered retail uses to any future ground floor would be important. Sufficient deep soil below this setback area is also important to support significant street tree and landscape outcomes.

A landscape area is shown in the concept plan as a buffer along the western boundary but the indicative plan shows a layout where the basement extends almost to the boundary and most of this landscaped area is occupied by an exposed vehicle ramp next to this boundary. This would not be acceptable as it would undermine any landscape buffer and would result in direct impacts in terms of noise and visual amenity. Further it appears that this ramp is external to the building above which also would be a very poor outcome.

A minimum depth of 6m should be provided for the deep soil planting along the rear (western boundary) to accommodate mature trees and planting as a landscape buffer to the adjacent neighbouring sites and to ensure vehicle ramps are not provided within this setback area.

The rear setback to the R2 zone should only be occupied by landscaped areas. The concept plan indicates communal open space in part of this area. This would be acceptable subject to an appropriate interface to any retail and an overshadowing analysis.

Ground level retail/commercial uses

Achieving a successful outcome for the proposed linear park is dependent on quality retail tenancies within the ground level combined with high-quality landscape solutions. The concept plan indicates retail and commercial uses anchoring only the corners of the site with a very small tenancy is shown between the two residential cores. It is appropriate that a minimum extent of frontage should be required for this site facing Chatham Road and that the depth of any retail tenancy should be sufficient to support the majority of retail uses.

Any awning should also be continuous over building entry points for continuous pedestrian protection.

Built form appearance

The indicative façade/ building appearance is very repetitive without sufficient modulation, depth and visual interest to moderate the bulk or respond to the finer grain lot pattern in the area. Greater variation in materials with more natural materials and less reliance on what appears to be painted render in combination with additional articulation/ modulation would be needed to moderate scale.

Amenity impacts

The indicative floor plan layout for upper floors indicates some potential privacy issues which would need to be addressed however the dual core solution is considered appropriate for the site.

Conclusion on the Planning Proposal and concept scheme –

GMU concludes from this analysis that there is no apparent urban design justification evident for why this site should achieve greater height and density. The reasoning of the applicant appears to hinge on very broad parameters in State Government strategies rather than a detailed and considered analysis of how much of any new housing requirement should occur within the West Ryde Centre and then why it should be located on this site as opposed to other sites within the centre where the existing LEP strategy for height would be supported rather than diluted.

There appears to be little consideration of the need for transition to the adjacent low scale R2 zone, the existing or future streetscape scale or serious intent to activate and support the proposed linear park. No strategy is presented for how the rest of the centre should adjust to account for the increased height in this position.

The indicative concept plan provided as part of the proposal demonstrates in our opinion that the FSR sought in combination with the increased height is excessive and as such will be likely to result in an overdevelopment of the site. We consider that greater setbacks or a scale reduction would be required to achieve an appropriate response to the R2 zone and that a street frontage height that respects the current and likely future heights of the rest of this part of the centre should be provided in such a scheme. Neither of these outcomes are considered within the concept plan and both would have an impact on the final likely FSR.

We suggest that any consideration for increased height and density on the periphery of the town centre should really be considered as part of a holistic vision for the centre so that the height is appropriate to the boundary and future character of the centre.

4. Comments in relation to the DCP amendments

In the event that Council supports the increased height and FSR in the planning proposal we have reviewed the intended amendments in the West Ryde Town Centre DCP to ensure that a quality outcome is possible and that the controls address the sort of issues evident in the concept scheme.

We are concerned that the DCP amendments do not appear to address the specifics of this site and any additional clauses (other than the 4m setback requirement) tend to be absorbed into the general provisions. Given this site seeks a significantly greater height and different outcome to much of the town centre we recommend that Council includes a site-specific envelope for the site that encapsulates 3D envelope provisions to control the final outcome on the site. This would assist in ensuring reasonable setbacks and transition in scale to the R2 zone as well as street frontage heights and percentage of retail frontage required.

Specific comments in relation to the current proposed amendments are provided below.

General Development Controls section -

CI 3.1.1 Building Height and Bulk –

GMU recommends that an objective is added as:

- Objective 2 “to ensure that where a new development is proposed of greater height sharing a boundary with the R2 zone then development is to provide a scale transition with greater setbacks to moderate the scale, overshadowing and visual impact of the new development”
- Objective 3 “to ensure that where a new development is proposed that shares a boundary with the R2 zone the setback area provides an appropriate landscape buffer to any development.”

This is to ensure that any future scheme provides an appropriate transition and minimises impacts on neighbouring properties.

We suggest that an additional control is added at b) that states that achievement of the maximum FSR is dependent on satisfying the objectives and controls within this DCP and SEPP 65.

Floor to ceiling heights

GMU recommends that an additional control is provided at control d) which requires that a minimum floor to floor height of 3.1m is required for residential uses to ensure sufficient height is provided for the ceiling void to accommodate construction tolerances, services and lighting whilst achieving a minimum 2.7m floor to ceiling height.

This is to ensure that future development does not try to achieve an additional floor beyond the 7 floors shown in the concept plan by compromising on floor to floor heights and then arguing for the roof terrace and its access to be above the height plane.

Architectural appearance/ Building articulation

Additional controls are recommended to be added to Building Articulation and Features on page 13 of the draft. The concept is a very long building with little articulation. It is important to ensure that articulation includes more than entries, balconies and terrace but also includes deep insets and slots within the building form to both the street and rear elevations in particular given the building length. Therefore, we suggest that control e) be expanded to include reference to ‘deep insets and slots within the façade’ in addition to the other items listed.

Roof character

We also suggest that control f) is expanded to include reference to the plant and overruns being designed into the roof form of the building.

CI 3.1.2 Mixed Use Development

This section includes many positive objectives but lacks reference to the size of any retail tenancy. We have experienced numerous examples of DA's which are designated for active frontages in centres but provide quite token depths to the tenancies which means that the range of tenants is very limited and spaces may not rent, undermining the intent of the control.

We recommend an additional objective be added along the lines of 'to ensure mixed-use development maximises the extent of retail/commercial frontages and the functionality of these uses on the ground floor. We suggest that a minimum depth of 10m is required for individual speciality tenancies.

Under the controls for this section we recommended that control c)i) is amended to state that the proposal is consistent with SEPP 65 and the ADG where 'it does not, conflict with the specific requirements of the DCP (other than those exclusions mentioned in SEPP 65).

CI 3.1.3 Street Setbacks and Alignment

Expand this section in terms of objectives and controls to require that new development provide a street wall height that respects and acknowledges the scale of existing development and where a greater height is proposed relative to the general height of new development in the town centre the street wall height should respond to that height with a setback for additional levels above this height. A controls should be added requiring that additional floors above the street frontage height be setback a minimum of 3m to all street frontages.

CI 3.1.4 Urban Design

GMU suggests some further controls be added. The new control d) should require development to provide a transition, deep soil and landscape buffers to lower density development.

We also suggest that the ADG is not the reference for the appropriate setback but rather that Council includes specific setback controls are added to ensure a positive outcome. We would suggest that a controls could take one of two approaches.

The first would be to assume a 24m height is sought close to the boundary in which case the DCP should require a minimum of:

- 9m as a setback for the first 4 levels; and
- 12m for levels 5-8

These setbacks should be irrespective of the room arrangement to provide sufficient separation and mitigate the height differential. This could be relaxed to 6m for the first 10m (depth) of the building from the street boundary for the first 4 levels.

Ideally a setback map should be provided with these interface locations, or this could be provided within the relevant precinct controls later in the DCP.

Alternatively, the controls should require any building form adjacent to the boundary with an R2 zone to have a maximum height equivalent to the height of the adjacent zone or no more than one storey above it. The built form should be setback a minimum of 6m in such circumstances with levels above this height to have a setback equivalent to 50% of the separation distances in the ADG plus an additional 3m to mitigate the massing. This setback applies to both blank walls and habitable windows to minimise visual impact from the height and overshadowing.

We would also suggest that:

- control g) be amended to delete reference to basement parking only occurring 'where possible' and that
- control i) should be amended to delete the reference to parking behind 'behind the building'. Buildings of the scale contemplated here and elsewhere in the centre should provide all parking in basements.

It is suggested that additional controls also be added requiring vehicle and service ramps for sites adjoining R2 zones to be located a minimum of 6m away from the relevant boundary and that they be encapsulated within a building form and not located as open ramps within side or rear boundaries setbacks.

We would also suggest controls are added around the location of building services such as substations, fire services, etc. so that they are encapsulated within the building.

CI 3.1.6 Active Street frontages and Street Address

GMU recommends adding an objective 7:

- 'To ensure retail tenancies of sufficient depth are provided to accommodate a range of retail uses'.

We recommend that another control is added as Controls e) and f) stipulating:

- All retail tenancies at the ground level are to be provide with a minimum depth of 10m to ensure the majority of retail uses are able to be accommodated; and
- Where active uses are required for street frontages with nil setbacks they are to occupy a minimum of 80% of the building frontage.

We also question control d) in this section which appears to advocate for wrap around verandas and upper storey balconies or bay windows.

These elements tend to be very domestic in character and are unlikely to enhance the appearance or character of town centre development of this scale. Such verandas or balconies would need to over sail the footpath which does not appear to be contemplated elsewhere in the document and bay windows can come under articulation rather than specifically requiring their inclusion.

It is suggested that the clause is sufficient if it requires expression of the corner form, corner entries and roof features. We also recommend that consideration is given to a further control that states expression of the corner is not to rely upon additional height to achieve emphasis.

CI 3.1.9 Visual Amenity and Acoustic Amenity

This section should be expanded under control Visual Amenity h) to add a statement that:

- "main living areas and balconies are to be oriented towards the street, rear boundaries or courtyards within the development and not to side boundaries'.

This will minimise the risk of major privacy impacts within the centre and especially at the edges with low scale development.

Control j)i) should also remove the words 'wherever possible' as for sites with active use requirements such frontages are essential.

We also suggest that Control j)ii) is amended to require blank walls to be minimised rather than 'avoided' as all buildings such as this will require services and also fire doors etc on the street.

CI 3.2.1 Vehicle Access

The controls do not really address the risk of vehicle ramps and servicing being located immediately adjacent to the boundaries with low scale residential zones. We suggest that additional controls are added as well as an objective seeking to minimise the impact of vehicle access and servicing areas on neighbouring residential low scale developments. This could occur as:

- Objective 5 – The location of Vehicle ramps and services must minimise impacts on adjoining lots and particularly adjacent to any R2 zones.
- Control g) – Vehicle access ramps are not to be located to occupy side and rear setback areas adjoining to the R2 zone. Ramps are to be located a minimum of 6m from the boundary.

The existing controls 3.2.1 a) ii) should then be amended delete the words '*if located off a primary street frontage*' as access should be integrated into the building design in any circumstance.

CI 3.3.1 Solar Access

This suggestion is not specifically related to this PP but the amendment of the DCP provides a good opportunity to amend the requirement for solar access on page 25 control 3.3.1 b) to 2 hours rather than 3 hours to be consistent with the ADG.

CI 4.2 Retail Core

The proposal is within the Retail Core and additional requirements are contained in CI 4.2 Retail Core. We suggest that CI 4.2.2 is reinforced with similar suggestions to the ones above including requiring new development at ground level to provide tenancies of sufficient depth and the setback and scale controls suggested for the earlier part of the DCP.

We also suggest that reference to parking at grade is also deleted from CI 4.2.4 Car Parking.

We also note that the current DCP does not appear to include any objectives or controls about the street frontage height or human scale to the street. As mentioned earlier these types of controls could be provided within the General controls or specifically within this precinct to deal with this site specifically.

We recommend that Council consider stipulating (on maps) the street frontage height that would be appropriate. If the controls are retained at a height of 15.5m for all other sites around the subject site then this would be an appropriate street frontage height that should apply to the site with any additional floors setback a minimum of 3m.

We recommend this outcome as a scheme that for examples, proposes 7 storey buildings rising direct from the street with no setback on in the town centre would result in a very dominant and aggressive form.

We would recommend, subject to Council considering any heritage or buildings unlikely to redevelop in the centre, that street walls in the order of 4-5 storeys be considered.

However, this does need be coordinated with height across the centre so that the resulting massing might be managed based on the proportion of the building base to the upper floors.

Where more than 2 upper levels are setback it can result in a poor urban form unless a tower is proposed. We would also recommend that minimum setback distances are stipulated for upper floors. This dimension should vary depending on how much massing results above the street wall height.

Conclusion on the proposed amendments to the DCP –

GMU recommends that site-specific controls be developed for this site including envelope, setback and street frontage height controls that ensure a moderation of scale to the R2 boundary and an appropriate streetscape scale. The proposed amendments relate to controls that would apply quite broadly across the centre. In their present form, the amendments are not sufficient to ensure the issues identified in the concept scheme would be properly managed.

Alternatively, GMU recommends that the further objectives and controls discussed above are inserted into the draft DCP amendment.

5. General conclusion and recommendations

The planning proposal is at odds with a strong holistic vision or framework for the town centre as a whole and appears to ignore key outcomes such as transition in scale and appropriate built form emphasis in the centre.

Although additional density may be appropriate for this town centre, it may not be appropriate for this site given its immediacy to the R2 zone and its edge of centre location. If such a height increase is considered to be appropriate, then, in our opinion, it should be accompanied by a site-specific DCP that ensures a scale transition is achieved in any future development.

The concept plan illustrates a scheme that would result in adverse impacts and would not respond appropriately to the streetscape character of the area. We have provided a commentary to assist Council and the applicant to understand what amendments would be considered necessary to achieve a reasonable outcome on the site for the height contemplated. We note that these changes would reduce the maximum FSR and as such further study should be undertaken on what would constitute an appropriate FSR if the height is supported.

Our specific recommendations in relation to the Concept plan (if the height is supported) to ensure a reasonable outcome are

Built form and scale-

- A streetscape analysis is recommended to determine the street wall height. It also should be managed based on the proportion of the podium and tower element to arrive at human-scaled base and a slender tower element.
- It is likely that a streetwall height of 15m would be reasonable ie 4 storeys with upper floors setback a minimum of 3m.

Architectural character-

- A greater level of articulation/break should be provided to mitigate the perceived bulk and scale, provide a more human-scaled development and create a slender tower form.
- An attractive roof design is recommended to enhance the architectural character of the building.

Setbacks and separation distances-

- The proposed setback and separation distances to the western boundary adjacent to the low-density development should be increased to 9m up to the 4th level with an additional 3m for built form above level 4.

Landscape-

- A minimum depth of 6m should be provided for the deep soil planting along the rear (western boundary) to accommodate mature trees and planting as a landscape buffer to the adjacent neighbouring sites and to ensure vehicle ramps are not provided within this setback area
- The width of the vehicle entrance is to be minimised and the parking ramp is to be encapsulated with the building form set a minimum of 6m off the boundary to mitigate streetscape and amenity impacts to the R2 neighbours.