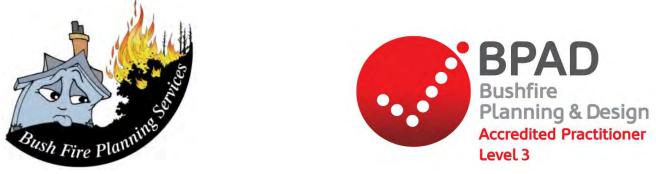
Attachment 7: Bushfire Risk Assessment prepared by Bushfire Planning Services



Member of the Fire Protection Association of Australia

# 112 Talavera Road, Macquarie Park NSW 2113.

#### Sunday, 12 August 2018

Prepared and certified by:	Matthew Willis BPAD – Level 3 Certified Practitioner Certification No: BPD-PA 09337	Mathink.	12/08/2018 28/08/2018	
Can this proposal comply with AS3959-2009 (inc PBP addendum 3)?		Yes		
Is referral to the RFS required?		Yes. Integrated development and SFPD		
Can this development comply with the requirements of PBP?		Yes		
Plans by Meriton.		(Attached)		

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## **Bushfire Risk Assessment**

#### Sunday, 12 August 2018

Contact Tom Hutchison Meriton Level 11 Meriton Tower 528 Kent Street Sydney NSW 2000 9287 2888

Subject Property Lot 422, DP 1221081 112 Talavera Road Macquarie Park NSW 2113

#### 1. Contents

2.	Executive summary.	4
3.	General	5
4.	Block description	6
5.	Vegetation	8
6.	Slope	10
7.	Significant features	11
8.	Threatened Species	11
9.	Aboriginal Heritage	11
10.	Bushfire Assessment Methodology	11
11.	Setbacks	12
12.	Water	13
13.	Access	14
14.	Fire trails	14
15.	Property Access	15
16.	Maintenance plans	15
17.	Building construction standards	15
18.	Sprinkler systems	16
19.	Compliance with chapter 4 of PBP (Residential subdivision)	16
	Compliance with chapter 4 of PBP (Special Fire Protection velopment)	24
	Compliance with the Aims and Objectives of Planning for Bushfire tection	28
22.	Conclusions	29
23.	References	30
24.	Appendix 1 Plans	32
Enc	d of document	36

#### 2. Executive summary.

Bushfire Planning Services has been requested by Mr Tom Hutchison of Meriton to supply a bushfire compliance report on Lot 422, DP 1221081, number 112 Talavera Road, Macquarie Park, NSW 2113.

The proposal is for the construction of a multi-use residential unit development containing residential units (class 2) a day-care centre (class 9b) and retail units (class 6).

The development can meet all the bushfire requirements for residential subdivision and the Special Fire Protection Development (SFPD) component as outlined in the RFS document Planning for Bushfire Protection.

The retail spaces are not covered by bushfire requirements as the general fire safety provisions of the NCC are considered as appropriate as per Planning for Bushfire Protection.

The hazard to the proposal is an area of forest vegetation to the northeast of the subject lot.

The subject lot is separated from the hazard by the M2 Motorway, a distance of at least 68m to the lot boundary.

The separation distance within the lot to the closest part of the residential development is 6.5m and the setback to the closest part of the day-care is 6.5m to the outdoor area and 22m to the closest part of the rooms.

The slope beneath the hazard is 0-5 deg downslope.

Given the above the residential section of the development complies with the setback needed to achieve BAL-12.5 in accordance with table 2.2.4 of AS3959-2009.

The SFPD area can achieve compliance with table A2.6 of Planning for Bushfire Protection.

All other aspects of this proposal can or can be made to comply with the acceptable solutions for subdivision and SFPD as outlined in Planning for Bushfire Protection.

Based on the assumptions and measurements contained within this assessment the development is considered to be able to meet the requirements of clause 44 of the Rural Fi res Regulation 2008 and the RFS requirements as outlined in Planning for Bushfire Protection.

#### 3. General.

As this proposal includes the construction of a multi dwelling building and that the building also includes a Special Fire Protection Development (day-care) the proposal **is considered to be "integrated development"** and is required under section 91 of the Environmental Planning and Assessment Act to obtain a section 100B Bushfire Safety Authority from the Rural Fire Service.

For the Rural Fire Service to issue the 100B Bushfire Safety Authority it must be satisfied that the proposal can meet the requirements of clause 44 of the Rural Fires Regulation.

This assessment is based around the requirements of clause 44 and indicates if and how the proposal meets these requirements.

The following text in italics is a copy of clause 44 of the Rural Fires Regulation 2008;

#### 44 Application for bush fire safety authority

For the purposes of section 100B (4) of the Act, an application for a bush fire safety authority must be made in writing and must include the following:

(a) a description (including the address) of the property on which the development the subject of the application is proposed to be carried out,

(b) a classification of the vegetation on and surrounding the property (out to a distance of 140 metres from the boundaries of the property) in accordance with the system for classification of vegetation contained in Planning for Bush Fire Protection,

(c) an assessment of the slope of the land on and surrounding the property (out to a distance of 100 metres from the boundaries of the property),

(d) identification of any significant environmental features on the property,

(e) the details of any threatened species, population or ecological community identified under the <u>Threatened Species Conservation Act 1995</u> that is known to the applicant to exist on the property,

(f) the details and location of any Aboriginal object (within the meaning of the <u>National Parks and Wildlife Act 1974</u>) or Aboriginal place (within the meaning of that Act) that is known to the applicant to be situated on the property,

(g) a bush fire assessment for the proposed development (including the methodology used in the assessment) that addresses the following matters:

(i) the extent to which the development is to provide for setbacks, including asset protection zones,

(ii) the siting and adequacy of water supplies for firefighting,

(iii) the capacity of public roads in the vicinity to handle increased volumes of traffic in the event of a bush fire emergency,

(iv) whether or not public roads in the vicinity that link with the fire trail network have two-way access,

(v) the adequacy of arrangements for access to and egress from the development site for the purposes of an emergency response,

(vi) the adequacy of bush fire maintenance plans and fire emergency procedures for the development site,

(vii) the construction standards to be used for building elements in the development,

(viii) the adequacy of sprinkler systems and other fire protection measures to be incorporated into the development,

(h) an assessment of the extent to which the proposed development conforms with or deviates from the standards, specific objectives and performance criteria set out in Chapter 4 (Performance Based Controls) of Planning for Bush Fire Protection.

Any wording that appears in blue italics are quotes from Planning for Bushfire Protection 2006 (PBP).

Some of the distance measurements used in this report have been taken from aerial photographs and as such are approximate only. If doubt exists, the distances should be verified by survey.

#### 4. Block description

Clause 44 requirement. "a description (including the address) of the property on which the development the subject of the application is proposed to be carried out"

The subject lot is a large allotment with road frontage on 3 sides, the north-east, south-east and south-west.

There is existing commercial development to the north-west.

- Lot 422,
- DP 1221081,
- 112, Talavera Road Macquarie Park,
- LGA, Ryde,
- Area, 19398m2,

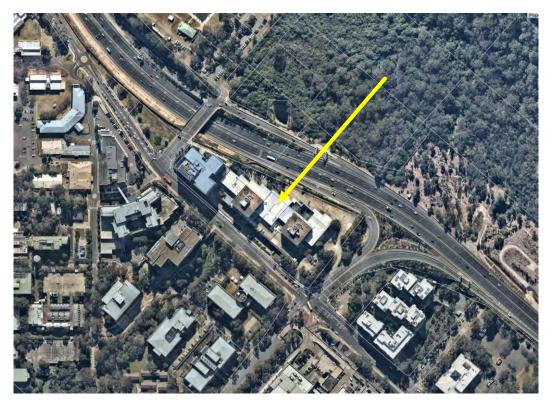
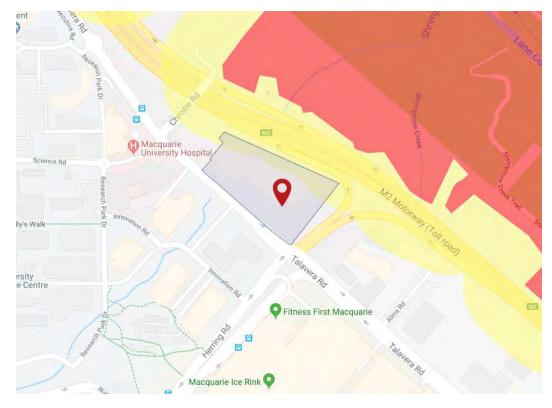
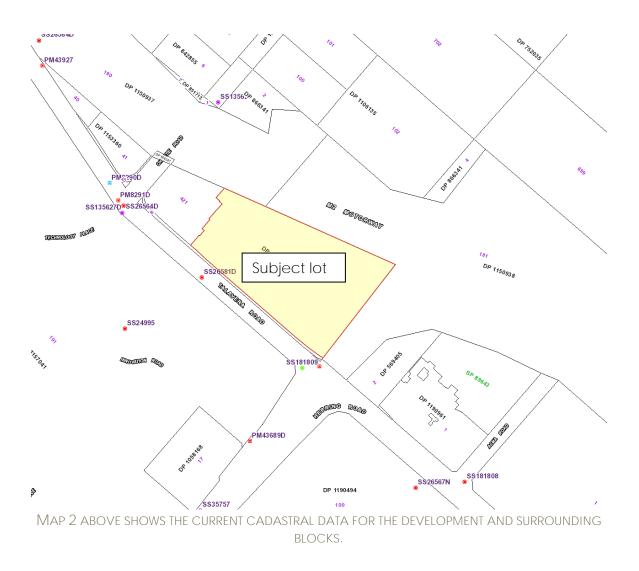


PHOTO 1 (ABOVE) SHOWS A GENERAL OVERVIEW OF THE SURROUNDING AREA.



Map 1 is an extract from the councils bushfire prone land map.



#### 5. Vegetation

Clause 44 requirement\_"a classification of the vegetation on and surrounding the property (out to a distance of 140 metres from the boundaries of the property) in accordance with the system for classification of vegetation contained in Planning for Bush Fire Protection"

The study area for the vegetation is 140m surrounding the development site. The vegetation assessment has been undertaken using the methodology of "Ocean Shores to Desert Dunes, Native Vegetation of New South Wales and the ACT" by David Keith.

The lands within the study area for this proposal comprises of mostly commercial development, urban landscaping and transport infrastructure.

The vegetation that is mapped as the hazard to this proposal is to the north-east of the subject lot.

The vegetation has several classifications, these include:

- Weeds and Exotics
- Western Vine Thickets
- Southern Tablelands Dry Sclerophyll Forest
- Hornsby Enriched Sandstone Exposed Woodland.

In general, the vegetation is highly disturbed and doesn't fit any one recognised category.

For the purpose of this assessment a worst-case scenario of Southern Tablelands Dry Sclerophyll Forest has been used,

This is considered as an overestimation of the current vegetation structure.



 $\ensuremath{\text{Photo}}\xspace 2$  . The yellow dashed line defines the extent of the 140m vegetation study area for this proposal.

#### 6. Slope

Clause 44 requirement\_"an assessment of the slope of the land on and surrounding the property (out to a distance of 100 metres from the boundaries of the property)",

For the purpose of the slope analysis for this proposal 1 slope run beneath the mapped hazard to the north-east has been evaluated.

The run is shown on the following topographical map and the run details are shown in table 1.



TABLE 1. THE ABOVE TABLE SHOWS THE PARTICULARS OF THE SLOPE RUNS USED IN THIS ASSESSMENT.

Slope run	Starting height (m)	Finish height (m)	Length of run (m)	Height difference (m)	Slope (deg)
1	38	34	100	4	2.3
2	40	35	100	5	2.8
3	47	43	100	4	2.3

#### 7. Significant features

Clause 44 requirement "identification of any significant environmental features on the property"

I have not been informed of any significant environmental features that would be affected by this proposal.

#### 8. Threatened Species

Clause 44 requirement "the details of any threatened species, population or ecological community identified under the Threatened Species Conservation Act 1995 that is known to the applicant to exist on the property,"

I have not been informed of any threatened species that would be affected by this proposal.

#### 9. Aboriginal Heritage

Clause 44 requirement "the details and location of any Aboriginal object (within the meaning of the National Parks and Wildlife Act 1974) or Aboriginal place (within the meaning of that Act) that is known to the applicant to be situated on the property,"

I have not been informed of any places of cultural significance that would be affected by this proposal.

#### 10. Bushfire Assessment Methodology

Clause 44 requirement"a bush fire assessment for the proposed development (including the methodology used in the assessment) that addresses the following matters:

The methodology used in the assessment of bushfire threat to the subject property is outlined in;

- Planning for Bushfire Protection 2006 as published by the New South Wales Rural Fire Service, and
- Australian Standard 3959-2009, Construction of buildings in Bushfire Prone Areas.

#### 11. Setbacks

## (i) Clause 44 requirement "the extent to which the development is to provide for setbacks, including Asset Protection Zones,"

The available setbacks between the proposal and the hazard have been taken from the north-eastern boundary of the subject lot then the setbacks available within the lot have been added where required.

These setbacks have been specifically targeted as this area contains the Special Fire Protection aspect of the proposal.

See following plan for details of setback within the lot.

Setback	Available distance
Outside lot	74m
To external face of building	6.5m
To SFPD (excluding outdoor area)	22m
Total setbacks available	80.5m and 96m



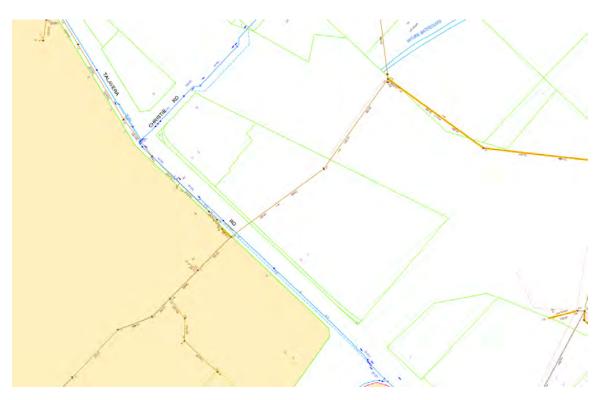
The following overlaid aerial photo shows the setback available over the larger area of the lot. It should be noted that the setbacks shown do not include the setback of the building within the lot which is nominally 6.5m.



#### 12. Water

(ii) Clause 44 requirement\_"the sighting and adequacy of water supplies for firefighting,"

The following map is an extract from the Sydney Water hydrant map for the area. Hydrants are shown as blue dots on a blue line.



As can be seen there are multiple hydrants indicated around the subject lot.

#### 13. Access

(iii) Clause 44 requirement "the capacity of public roads in the vicinity to handle increased volumes of traffic in the event of a bush fire emergency,"

The subject lot has road frontage onto Talavera Road with access proposed from Christie Road to the north-west and a M2 entry/exit to the south-east.

Talavera Road is a two-way road that are considered to be capable of handling emergency service vehicles.

#### 14. Fire trails

(iv) Clause 44 requirement "whether or not public roads in the vicinity that link with the fire trail network have two-way access,"

Fire trails are not planned or recommended as part of this development proposal. All roads in the vicinity have two-way access.

#### 15. Property Access

## (v) Clause 44 requirement\_"the adequacy of arrangements for access to and egress from the development site for the purposes of an emergency response,"

In accordance with the requirements of Planning for Bushfire Protection there are no access requirements for this proposal.

#### 16. Maintenance plans

## (vi) Clause 44 requirement "the adequacy of bush fire maintenance plans and fire emergency procedures for the development site"

No additional advice or information regarding bushfire maintenance plans & fire emergency procedures has been provided by the proponent.

Under the Rural Fires Act 1997 sect 52, the local council's bushfire management committees are required to prepare and submit management plans for the rural fire district or part of the state which it is constituted.

The plan covers the following,

- a plan of operations and
- a bushfire risk management plan.

The plan of operations must be reviewed within every 2 years and the bushfire risk plan must be reviewed within each 5 years.

Should a bushfire emergency impact upon this area, the implementation of the existing councils Sect. 52 Operations & Risk Plan should be adequate for bushfire suppression, hazard management and maintenance.

I have not been informed of any site-specific bushfire plans.

#### 17. Building construction standards

(vii) Clause 44 requirement "the construction standards to be used for building elements in the development,"

Table 2.4.2 of AS 3959-2009 'Construction of Buildings in a Bushfire Prone Area' outlines the appropriate level of construction to be used once analysis has been undertaken in accordance with the methodology of that standard. Given the variables of slope, vegetation classification and achievable setback distances from the classified vegetation have been considered the resultant BAL (Bushfire Attack Level) for this proposal has been determined as being less than or equal to BAL 29.

The appropriate construction standards for construction in bushfire prone areas are;

- AS 3959-2009 (amendment 3) Construction of Buildings in Bushfire Prone Areas.
- Building Code of Australia and the applicable referenced standards.
- The addendum to appendix 3 of Planning for Bushfire Protection.

#### 18. Sprinkler systems

(viii) Clause 44 requirement "the adequacy of sprinkler systems and other fire protection measures to be incorporated into the development,"

It is assumed that the building will be fitted with an internal sprinkler system that will be installed in line with the appropriate Australian Standards.

## 19. Compliance with chapter 4 of PBP (Residential subdivision)

Clause 44 requirement "assessment of the extent to which the proposed development conforms with or deviates from the standards, specific objectives and performance criteria set out in Chapter 4 (Performance Based Controls) of Planning for Bush Fire Protection."

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
Radiant heat levels at any point on a proposed building will not exceed 29 kW/m <sup>2</sup>	an APZ is provided in accordance with the relevant tables and figures in PBP	Yes	In accordance with table A2.4 of Planning for Bushfire Protection.
kW/m²	the APZ is wholly within the boundaries of the development site	Νο	The APZ for this development is contained within the subject lot and on the neighbouring developed or otherwise historically managed lands.
Applicants demonstrate that issues relating to slope are addressed: maintenance is practical, soil stability is not compromised and the potential for crown fires is negated	the APZ is not located on lands with a slope exceeding 18 degrees	Yes	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
APZs are managed and maintained to prevent the spread of a fire towards the building	in accordance with the requirements of 'Standards for Asset Protection Zones (RFS 2005)	Achievable	APZ's used in this assessment are all developed land or otherwise historically managed.
Fire fighters are provided with safe all-weather access to structures (thus allowing more efficient use of firefighting resources)	public roads are two-wheel drive, all weather roads	Yes	Existing roads provide this.
Public road widths and design that allow safe access for fire fighters while residents are evacuating an area	urban perimeter roads are two- way, that is, at least two traffic lane widths (carriageway 8 metres minimum kerb to kerb), allowing traffic to pass in opposite directions	N/A	The subject development does not incorporate any new or redesigned public roadway.
	Non-perimeter roads comply with Table 4.1 – Road widths for Category 1 Tanker (Medium Rigid Vehicle)	N/A	
	the perimeter road is linked to the internal road system at an interval of no greater than 500 metres in urban areas	N/A	
	roads are through roads. Dead end roads are not more than 200 metres in length from a through road, incorporate a minimum 12 metres outer radius turning circle, and are clearly sign posted as a dead end	N/A	
	traffic management devices are constructed to facilitate access by emergency services vehicles	N/A	
	there is a minimum vertical clearance to a height of four metres above the road at all times	N/A	
	curves have a minimum inner radius of six metres and are minimal in number to allow for rapid access and egress	N/A	
	the minimum distance between inner and outer curves is six metres	N/A	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	maximum grades for sealed roads do not exceed 15 degrees and an average grade of not more than 10 degrees or other gradient specified by road design standards, whichever is the lesser gradient.	N/A	
	Public roads have a cross fall not exceeding 3 degrees	N/A	
	the internal road surfaces and bridges have a capacity to carry fully-loaded fire fighting vehicles (15 tonnes)	N/A	
The capacity of public road surfaces and bridges is sufficient to carry fully loaded fire fighting vehicles	the capacity of road surfaces and bridges is sufficient to carry fully loaded fire fighting vehicles (approximately 15 tonnes for areas with reticulated water, 28 tonnes or 9 tonnes per axle for all other areas). Bridges clearly indicate load rating	N/A	
Roads that are clearly sign- posted (with easily distinguishable names) and buildings/properties that are clearly numbered	public roads greater than 6.5 metres wide to locate hydrants outside of parking reserves to ensure accessibility to reticulated water for fire suppression	N/A	
	public roads between 6.5 metres and 8 metres wide are No Parking on one side with the services (hydrants) located on this side to ensure accessibility to reticulated water for fire suppression	N/A	
There is clear access to reticulated water supply	public roads up to 6.5 metres wide provide parking within parking bays and locate services outside of the parking bays to ensure accessibility to reticulated water for fire suppression	Yes	There are several hydrants located in the surrounding area.
	one way only public access roads are no less than 3.5 metres wide and provide parking within parking bays and locate services outside of the parking bays to ensure accessibility to reticulated water for fire suppression	Νο	
Parking does not obstruct the minimum paved width	parking bays are a minimum of 2.6 metres wide from kerb edge to road pavement. No services or	N/A	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	hydrants are located within the parking bays		
	public roads directly interfacing the bush fire hazard vegetation provide roll top kerbing to the hazard side of the road	N/A	
Access to properties is provided in recognition of the risk to fire fighters and/ or evacuating occupants	at least one alternative property access road is provided for individual dwellings (or groups of dwellings) that are located more than 200 metres from a public through road	N/A	
The capacity of property access road surfaces and bridges is sufficient to carry fully loaded fire fighting vehicles	bridges clearly indicate load rating and pavements and bridges are capable of carrying a load of 15 tonnes	N/A	
All weather access is provided	roads do not traverse a wetland or other land potentially subject to periodic inundation (other than a flood or storm surge)	N/A	
Property road widths and design enable safe access for vehicles	Note: No specific access requirements apply in a urban area where a 70 metres unobstructed path can be demonstrated between the most distant external part of the proposed dwelling and the nearest part of the public access road (where the road speed limit is not greater than 70kph) that supports the operational use of emergency fire fighting vehicles (i.e. a hydrant or water supply)	Yes	Access will be available from Talavera Road which has a speed limit of less than 70khph.
	in forest, woodland and heath situations, rural property access roads have passing bays every 200 metres that are 20 metres long by two metres wide, making a minimum trafficable width of six metres at the passing bay	N/A	
	a minimum vertical clearance of four metres to any overhanging obstructions, including tree branches	N/A	
	internal roads for rural properties provide a loop road around any dwelling or incorporate a turning circle with a minimum 12 metre outer radius	N/A	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	curves have a minimum inner radius of six metres and are minimal in number to allow for rapid access and egress	N/A	
	the minimum distance between inner and outer curves is six metres	N/A	
	the cross-fall is not more than 10 degrees	N/A	
	maximum grades for sealed roads do not exceed 15 degrees and not more than 10 degrees for unsealed roads	N/A	
	Note: Some short constrictions in the access may be accepted where they are not less than the minimum (3.5m), extend for no more than 30m and where the obstruction cannot be reasonably avoided or removed. The gradients applicable to public roads also apply to community style development property access roads in addition to the above		
	access to a development comprising more than three dwellings have formalised access by dedication of a road and not by right of way	N/A	
The width and design of the fire trails enables safe and ready access for firefighting vehicles	a minimum carriageway width of four metres with an additional one metre wide strip on each side of the trail (clear of bushes and long grass) is provided	N/A	The subject development does not incorporate nor require any new or redesigned fire trail access.
	the trail is a maximum grade of 15 degrees if sealed and not more than 10 degrees if unsealed	N/A	
	a minimum vertical clearance of four metres to any overhanging obstructions, including tree branches is provided	N/A	
	the cross-fall of the trail is not more than 10 degrees	N/A	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	the trail has the capacity for passing by: - reversing bays using the access to properties to reverse fire tankers, which are six metres wide and eight metres deep to any gates, with an inner minimum turning radius of six metres and outer minimum radius of 12 metres; and/or - a passing bay every 200 metres, 20 metres long by three metres wide, making a minimum trafficable width of seven metres at the passing bay	N/A	
Fire trails are trafficable under all weather conditions. Where the fire trail joins a public road, access shall be controlled	the fire trail is accessible to fire fighters and maintained in a serviceable condition by the owner of the land	N/A	The subject development does not incorporate nor require any new or redesigned fire trail access.
to prevent use by non authorised persons	appropriate drainage and erosion controls are provided	N/A	
	the fire trail system is connected to the property access road and/or to the through road system at frequent intervals of 200 metres or less	N/A	
	fire trails do not traverse a wetlands or other land potentially subject to periodic inundation (other than a flood or storm surge)	N/A	
	gates for fire trails are provided and locked with a key/lock system authorised by the local RFS	N/A	
Fire trails designed to prevent weed infestation, soil erosion and other land degradation	fire trail design does not adversely impact on natural hydrological flows	N/A	
	fire trail design acts as an effective barrier to the spread of weeds and nutrients	N/A	
	fire trail construction does not expose acid-sulphate soils	N/A	
(Reticulated water supplies) Water supplies are easily accessible and located at	reticulated water supply to urban subdivisions uses a ring main system for areas with perimeter roads	N/A	This proposal will utilize existing water infrastructure.
regular intervals	fire hydrant spacing, sizing and pressures comply with AS 2419.1	Achievable	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	<ul> <li>2005. Where this cannot be met, the RFS will require a test report of the water pressures anticipated by the relevant water supply authority. In such cases, the location, number and sizing of hydrants shall be determined using fire engineering principles</li> </ul>		
	hydrants are not located within any road carriageway	Achievable	
	all above ground water and gas service pipes external to the building are metal, including and up to any taps	Achievable	
	the provisions of parking on public roads are met	N/A	
(Non-reticulated water supply areas) For rural-residential and rural developments ( or settlements) in bush fire prone areas, a water supply reserve dedicated to firefighting purposes is installed and maintained. The supply of water can be	the minimum dedicated water supply required for firefighting purposes for each occupied building excluding drenching systems, is provided in accordance with [PBP] Table 4.2	N/A	
an amalgam of minimum quantities for each lot in the subdivision (community titled subdivisions), or held individually on each lot	a suitable connection for firefighting purposes is made available and located within the IPA and away from the structure. A 65mm Storz outlet with a Gate	N/A	
	Gate or Ball valve is provided Gate or Ball valve and pipes are adequate for water flow and are metal rather than plastic	N/A	
	underground tanks have an access hole of 200mm to allow tankers to refill direct from the tank. A hardened ground surface for truck access is supplied within 4 metres of the access hole	N/A	
	above ground tanks are manufactured of concrete or metal and raised tanks have their stands protected. Plastic tanks are not used. Tanks on the hazard side of a building are	N/A	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	provided with adequate shielding for the protection of fire fighters		
	all above ground water pipes external to the building are metal including and up to any taps. Pumps are shielded	N/A	
(Electricity Services)	where practicable, electrical transmission lines are underground	Achievable	
Location of electricity services limits the possibility of ignition of surrounding bushland or the fabric of buildings	where overhead electrical transmission lines are proposed: - lines are installed with short pole spacing (30 metres), unless	Achievable	
Regular inspection of lines is undertaken to ensure they are not fouled by branches.	crossing gullies, gorges or riparian areas; and - no part of a tree is closer to a power line than the distance set out in accordance with the specifications in 'Vegetation Safety Clearances' issued by Energy Australia (NS179, April 2002)		
(Gas Services) Location of gas services will not lead to ignition of surrounding bushland or the fabric of buildings	reticulated or bottled gas is installed and maintained in accordance with AS 1596 and the requirements of relevant authorities. Metal piping is to be used	Achievable	
	all fixed gas cylinders are kept clear of all flammable materials to a distance of 10 metres and shielded on the hazard side of the installation	Achievable	
	if gas cylinders need to be kept close to the building, the release valves are directed away from the building and at least 2 metres away from any combustible material, so that they do not act as a catalyst to combustion. Connections to and from gas cylinders are metal	Achievable	
	polymer sheathed flexible gas supply lines to gas meters adjacent to buildings are not used	Achievable	

## 20. Compliance with chapter 4 of PBP (Special Fire Protection Development)

Clause 44 requirement "assessment of the extent to which the proposed development conforms with or deviates from the standards, specific objectives and performance criteria set out in Chapter 4 (Performance Based Controls) of Planning for Bush Fire Protection."

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
Radiant heat levels of greater than 10kW/m <sup>2</sup> will not be experienced by occupants or emergency services workers entering or exiting a building	an APZ is provided in accordance with the relevant tables and figures in PBP	Yes	Compliance with table A2.6 is achievable.
	exits are located away from the hazard side of the building	Achievable	
	the APZ is wholly within the boundaries of the development site	Νο	The Asset Protection Zone for this proposal is contained within the subject lot itself and already established and maintained allotments.
Applicants demonstrate that issues relating to slope are addressed: maintenance is practical, soil stability is not compromised and the potential for crown fires is negated	mechanisms are in place to provide for the maintenance of the APZ over the life of the development	Achievable	As the Asset Protection Zone is contained within already developed allotments it is considered reasonable to expect that this situation will continue into the future.
	the APZ is not located on lands with a slope exceeding 18 degrees	Not applicable	
APZs are managed and maintained to prevent the spread of a fire towards the building	in accordance with the requirements of 'Standards for Asset Protection Zones (RFS 2005) (Note - a Monitoring and Fuel Management Program should be required as a condition of development consent)	Reasonably Assumed	All land within the required APZ is managed land.

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
Vegetation is managed to prevent flame contact and reduce radiant heat to buildings, minimise the potential for wind driven embers to cause ignition and reduce the effect of smoke on residents and fire- fighters	compliance with Appendix 5 (PBP)	Achievable	
Internal road widths and design enable safe access for emergency services and allow crews to work with equipment about the vehicle.	internal roads are two-wheel drive, sealed, all-weather roads	Not applicable	No new internal roads are planned as part of this proposal.
	internal perimeter roads are provided with at least two traffic lane widths (carriageway 8 metres minimum kerb to kerb) and shoulders on each side, allowing traffic to pass in opposite directions	Not applicable	
	roads are through roads. Dead end roads are not more than 100 metres in length from a through road, incorporate a minimum 12 metres outer radius turning circle, and are clearly sign posted as a dead end	Not applicable	
	traffic management devices are constructed to facilitate access by emergency services vehicles.	Not applicable	
	a minimum vertical clearance of four metres to any overhanging obstructions, including tree branches, is provided.	Not applicable	
	curves have a minimum inner radius of six metres and are minimal in number to allow for rapid access and egress	Not applicable	
	the minimum distance between inner and outer curves is six metres	Not applicable	
	maximum grades do not exceed 15 degrees and average grades are not more than 10 degrees	Not applicable	
	cross-fall of the pavement is not more than 10 degrees	Not applicable	
	roads do not traverse through a wetland or other land potentially subject to periodic inundation (other than flood or storm surge)	Not applicable	
	roads are clearly sign-posted and bridges clearly indicate load ratings	Not applicable	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	the internal road surfaces and bridges have a capacity to carry fully-loaded firefighting vehicles (15 tonnes)	Not applicable	
Water supplies are easily accessible and located at regular intervals.	Access points for reticulated water supplies to SFPP developments incorporate a ring main system for all internal roads. Fire hydrant spacing, sizing and pressures comply with AS 2419.1/2005. Where this cannot be met, the RFS will require a test report of the water pressures anticipated by the relevant water supply authority, once development has been completed. In such cases, the location, number and sizing of hydrants shall be determined using fire	Achievable	
	engineering principles. The provisions of public roads in section 4.1.3 in relation to parking are met.		
Non-reticulated water supply areas. A water supply reserve dedicated to firefighting purposes is installed and maintained. The supply of water can be an amalgam of minimum quantities for each lot in the development and be reticulated within dedicated firefighting lines.	10,000 L is the minimum dedicated water supply required for firefighting purposes for each occupied building, excluding drenching systems. The provision for suitable connection for RFS and or New South Wales fire brigades purposes in section 4.1.3 in relation to water supplies is made available.	Not applicable	
Electricity location of electricity services will not lead to ignition of surrounding bushland or the fabric of buildings or risk to life from damaged electrical infrastructure.	Electrical transmission lines are underground.	Achievable	
Gas location of gas services will not lead to ignition of surrounding bushland or the fabric of buildings.	Reticulated or bottled gas is installed and maintained in accordance with AS 1596/2002 and the requirements of relevant authorities. Metal piping is to be used. All fixed LPG tanks are kept clear of all flammable materials and located on the non-hazard side of the development. If gas cylinders need to be kept close to the building the release valves must be directed away from the building and away from any combustible material, so that they do not act as catalysts to combustion.	Achievable	

Performance Criteria	Acceptable Solution	Compliance	Assessment / Comment
	Polymer sheathed flexible gas supply lines to gas meters adjacent to buildings are not to be used.		
An emergency and evacuation management plan is approved by the relevant fire authority for the area.	An emergency/evacuation plan is prepared consistent with the RFS guidelines for the preparation of emergency evacuation plan. Compliance with AS 3745/2002 emergency control organisation and procedures for buildings, structures and workplaces for residential accommodation". Compliance with AS 4083/1997 "planning for emergencies - for health care facilities".	Achievable	
Suitable management arrangements are established for consultation and implementation of the emergency and evacuation plan.	An emergency planning committee is established to consult with residents (and their families in the case of aged care accommodation and schools) and staff in developing and implementing an emergency procedures manual. Detailed plans of all emergency assembly areas including "on-site" and "offsite" arrangements as stated in a S3745/2002 are clearly displayed, and an annual (as a minimum) trial emergency evacuation is conducted.	Achievable	
In relation to eco-tourism accommodation: suitable refuge areas and evacuation/management arrangements are in place commensurate with the bushfire risk.	At least one building should be used as a local refuge area and comply with the APZ and construction requirements for residential buildings Cavan's are within 50 m of a refuge building and are clearly signposted. The pass from cabins to the refuge areas are safe with management of surface fuels to less than or equal to 4 t per hectare. The overall accommodation for tourist does not exceed 12 persons. A mechanism for the relocation of occupants on days of a total fire ban or adverse fire activity is provided in the local area in which the development operates.	Not applicable	

Explanation of terms;

- 'Achievable'. With appropriate design this aspect can achieve the acceptable solution.
- 'Reasonably assumed'. It is considered reasonable to assume this requirement has been met.
- $\succ$  'N/A'. This item is not considered as relevant to this proposal.
- > 'Yes'. This item can/does comply with the acceptable solution.

# 21. Compliance with the Aims and Objectives of Planning for Bushfire Protection.

<u>Aims of Planning for</u> <u>Bushfire Protection</u>	<u>Opinion</u>	<u>Compliant</u>
The aim of PBP is to use the NSW development assessment system to provide for the protection of human life (including fire fighters) and to minimise impacts on property from the threat of bush fire, while having due regard to development potential, on-site amenity and protection of the environment".	The development assessment procedure has identified that the subject lot is considered to be bushfire prone land. It is considered that this proposal can comply with the aim of PBP of minimising the impacts of a bushfire on the property.	Yes
PBP specific objectives	<u>Opinion</u>	<u>Compliant</u>
Afford occupants of any building adequate protection from exposure to a bush fire;	In accordance with table A2.6 of Planning for Bushfire Protection and table 2.4.2 of AS3959-2009.	Yes
Provide for a defendable space to	It is considered there is adequate defendable	Yes

be located around buildings;	space around the development.	
Provide appropriate separation between a hazard and buildings which, in combination with other measures, prevent direct flame contact and material ignition	Flame contact between the building and identified hazard is considered highly unlikely.	Yes
Ensure that safe operational access and egress for emergency service personnel and residents is available	It is considered that the access and egress for the site is adequate for firefighting purposes.	Yes
Provide for ongoing management and maintenance of bush fire protection measures, including fuel loads in the asset protection zone (APZ);	Normal maintenance can provide for this.	Yes
Ensure that utility services are adequate to meet the needs of fire fighters (and others assisting in bush firefighting).	Assumed	

#### 22. Conclusions

It is shown through this assessment that this proposal has all the necessary requirements to meet the conditions of clause 44 of the Rural Fires Regulations and the requirements of Planning for Bushfire Protection and that it is reasonable to expect that the Rural Fire Service will issue a section 100B Bushfire Safety Authority for this development. The proposal has sufficient setback from the hazardous vegetation to achieve a BAL of less than or equal to BAL-29 for the residential component and less than 10kwm2 of radiant heat energy for the SFPD area.

Bushfires are affected by many external influences such as climactic conditions, vegetation type, moisture content of the fuel, slope of the land and human intervention to name a few and are difficult to predict.

This report does not intend to provide a guarantee that the subject property will survive if a bushfire should impact the surrounding area. The purpose of this report is to show the developments level of compliance or in some cases non-compliance with the New South Wales legislation regarding building in bushfire prone areas.

Where non-compliance is found measures will be suggested that should make the building less susceptible to the various attack mechanisms of a bushfire and comply with the performance requirements of the Building Code of Australia.

The opinions expressed in this report are based on the writers experience and interpretation of the relevant guidelines and standards. Notwithstanding the above, these guidelines and standards are open to interpretation. All care has been taken to ensure that the opinions expressed in this report are consistent with past successful outcomes.

If any further clarification is required for this report please do not hesitate to contact me using the details above.

Yours Sincerely

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Matthew Willis Grad Dip Planning for Bushfire Prone Areas Bushfire Planning Services Pty Limited.

#### 23. References

Australian Building Codes Board

Building Code of Australia Volumes 1&2 Canprint

New South Wales Rural Fires Act 1997 Section 100b

Planning NSW [2006]

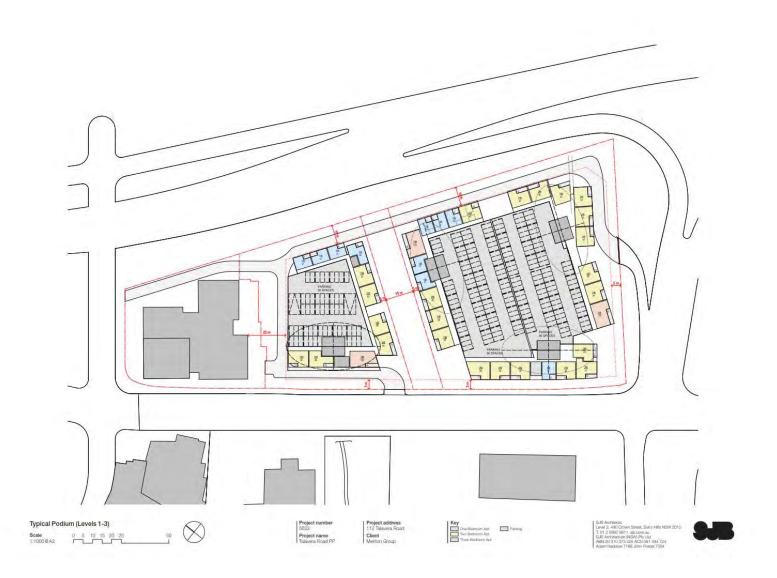
Planning for Bushfire Protection

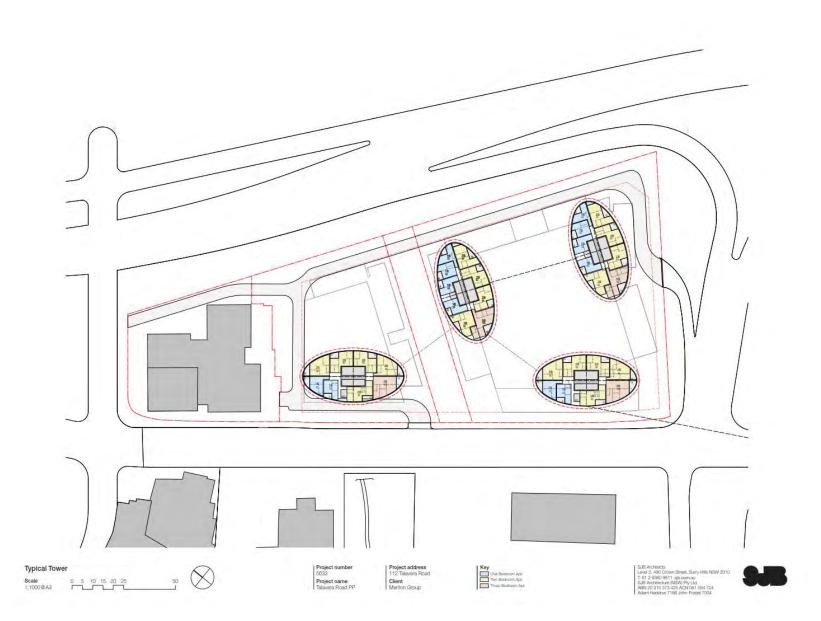
A Guide for Councils, Planners, Fire Authorities, Developers and Home Owners

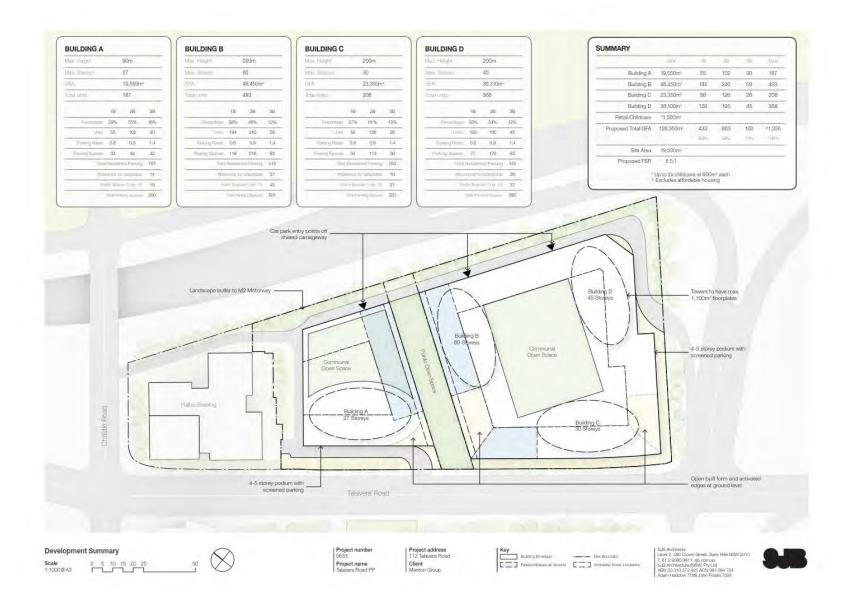
Standards Australia [2009] Australian Standards 3959 Australian Building Code Board Edition 2009

Rural Fires Regulation 2008 Clause 44

#### 24. Appendix 1 Plans







Bushfire Planning Services Pty Limited ACN 115714826.



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