

# Public Domain Technical Manual City Of Ryde



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# 1. Introduction

## 1.1 Purpose

This manual is a guide for the future planning and design of the public domain for the town centres of Ryde. It provides an overall direction for the design and implementation of DCP objectives within the public domain and its interface with the private realm.

The manual has been prepared by the City Of Ryde to complement the LEP and DCP, and to promote the revitalisation of key town centres within the Ryde Local Government area.

The Public Domain Technical Manual is intended for use by Council, developers, consultants and residents involved in the development of the Ryde area. It will ensure consistency for supply and contracting, and in the provision of maintenance.

## 1.2 Ryde 2021

Ryde 2021 is a Community Strategic Plan for the City of Ryde and is a requirement for all councils in NSW. It is the sole strategic document for the organisation and will determine how Council will meet the opportunities and challenges of the City over a ten year period, and beyond.

Ryde 2021 commenced with consulting the community regarding their hopes and concerns for the future of the City. The community responded by informing Council about what they value most about the City, where they want to see change and improvement, and the things they want to see maintained. This strategic plan endeavours to balance the challenges of growth and change with maintenance of the current lifestyle. The Plan will direct Council's Delivery Program from July 2011.

Because Ryde 2021 is a community plan, eventually the state government, businesses and the not-for-profit sector will also be involved in delivering on the outcomes identified in it.

## 1.3 What Is The Public Domain?

Within this document, the public domain represents all urban and natural elements, structures and spaces that exist within the publicly-owned areas of the Ryde town centres and the relationship between them. The public domain is also considered to include many privately owned arcades, building forecourts, internal streets and other semi-public spaces, given that they also influence the overall character of the public domain.

## 1.4 Centres Included In This Manual

This Public Domain Technical Manual is intended to cover the town centres of Ryde, Gladesville, Eastwood, West Ryde, Meadowbank and Macquarie Park. The Ryde and Macquarie Park public domain manuals are existing documents that have already been adopted by Council. The

Gladesville town centre has been prepared in conjunction with its recent DCP and master plan. Eastwood and West Ryde town centres have been prepared in anticipation of new DCPs and master plans being prepared. These sections will be reviewed and updated when these documents are adopted by Council.

The Public Domain Technical Manual also covers small and neighbourhood centres.

## **1.5 Objectives of This Manual**

The objectives of this manual fall under the City of Ryde Community Strategic Plan outcomes of *Liveable Neighbourhoods* and *City of Prosperity*. The manual aims to establish a framework of design principles to guide improvement of the public domain within the local centres.

The main objectives are to:

- Promote design solutions that enhance the amenity and character of the local area for residents, visitors, businesses and other users
- Encourage the development of spaces that are attractive and safe for all users
- Promote better access and new connections that provide improved pedestrian and vehicular networks in local centres
- Create active public spaces that are flexible to encourage diverse and different uses over time
- Facilitate high quality design solutions that encourage social interaction and a range of sensory experiences as well as functionality
- Protect heritage elements and precincts within the town centres
- Promote the physical integration of public and private domains
- Encourage sustainable practices and environments in our town and neighbourhood centres
- Encourage access to appropriate outdoor dining areas.

## **1.6 How to Use This Manual**

This Public Domain Technical Manual should be read in conjunction with the relevant LEP and DCP. See the *Document Hierarchy Table* below that shows the relationship of the manual to other Council documents.

Specifically, the manual should be used to inform the design of proposed development sites/works in order to:

- Influence the understanding of the site context, including its future infrastructure and character,
- Guide design and development decisions to ensure a seamless integration of the public and private domain,
- Inform the choice of materials, street furniture, plants and other streetscape elements to be used within the centres,
- Provide physical solutions for recommendations contained within the various planning instruments and policies that apply to the centres in the Ryde LGA.

Detailed specifications for paving and street furniture can be obtained from Council's Public Works Department. The Public Domain Technical Manual refers to items of street furniture by number (eg Seat 01) and Public Works can provide the details for these items such as manufacturer, model, materials, colour and installation method.



Like all planning documents, this manual should be considered within an optimum timeframe to ensure its continued relevance and usability as the town centres develop over time. It is considered that the life time of this document is 10 years with a review of the key provisions occurring every 2 years.

## **1.7 Associated documents**

### Outdoor Dining Policy (effective 6 January 2010)

This policy applies to outdoor dining on any public land in the City of Ryde Local Government Area that is owned or under the care, control and management of Council. In particular it applies to footpath areas adjacent to public roads situated within the town and neighbourhood centres in the City.

The purpose of the policy is to provide simple procedures and design guidelines for outdoor dining activities in plazas, closed roads, footpaths, parks and other public places. The policy outlines Council's requirements such as the minimum area for outdoor dining (4 m<sup>2</sup>), and maintenance of a clear unobstructed pedestrian corridor of 2 metres width.

Council requires all boundaries of the approved outdoor dining area to be marked on site. The methods of identifying the boundaries which may be considered acceptable include:

- markers (metal discs installed flush with the pavement);
- barriers/bollards – fixed or removable;
- planter boxes, seating walls and other landscape elements.

### Management Plan

The City of Ryde's Four Year Delivery Plan 2011 - 2015 is a blueprint of Council's activities over four years. It includes the annual budget for the new financial year and Council's 4 year capital works program.

## Document Hierarchy Table

Overall Strategic Document for the City of Ryde

**The Ryde 2021 Community Strategic Plan**

Regulatory controls under the EP&A act

**Ryde Local Environmental Plan**

**Ryde Development Control Plan**

Physical Outcomes of the regulatory Controls

**Ryde Public domain technical Manual - Scope**

**Ryde Standard Drawings and Specifications Manual - Details**

Related Documents

**Ryde Public Domain Upgrades Report 2011**

**Ryde Outdoor Dining Policy**

**Private Development works**

**Ryde 4yr Delivery Plan**

## 2. Gladesville

# 2.1 Analysis of Existing Character

### 2.1.1 Introduction to the Gladesville Town Centre and Victoria Road Corridor

Gladesville is located 10 km west of Sydney. The suburb is bisected by Victoria Road which is a major traffic corridor linking Sydney and the western suburbs. The business district includes the historic centre of Gladesville and is located along Victoria Road. Away from Victoria Road Gladesville is predominantly residential in character.

The Gladesville Town Centre and Victoria Road Corridor extend along Victoria Road from Punt Road in the south to Monash Road in the north.

The area is broken into four precincts as defined in the DCP:

- South Gladesville,
- North Gladesville,
- Gladesville Town Centre and
- Monash Road.

The City of Ryde and Hunters Hill Council (HHC) share the precincts of South Gladesville and the Town Centre (up to Pittwater Road). This manual is relevant to the City of Ryde Local Government Area (LGA) only. Refer to Figure 1 for details.

The Ryde Local Environmental Plan (LEP) 2010 Gladesville Town Centre and Victoria Road Corridor and Part 4.6 of the Ryde Development Control Plan are the controlling documents for the area. The Public Domain Manual reflects the objectives of these plans and it should be read in conjunction with these other instruments.



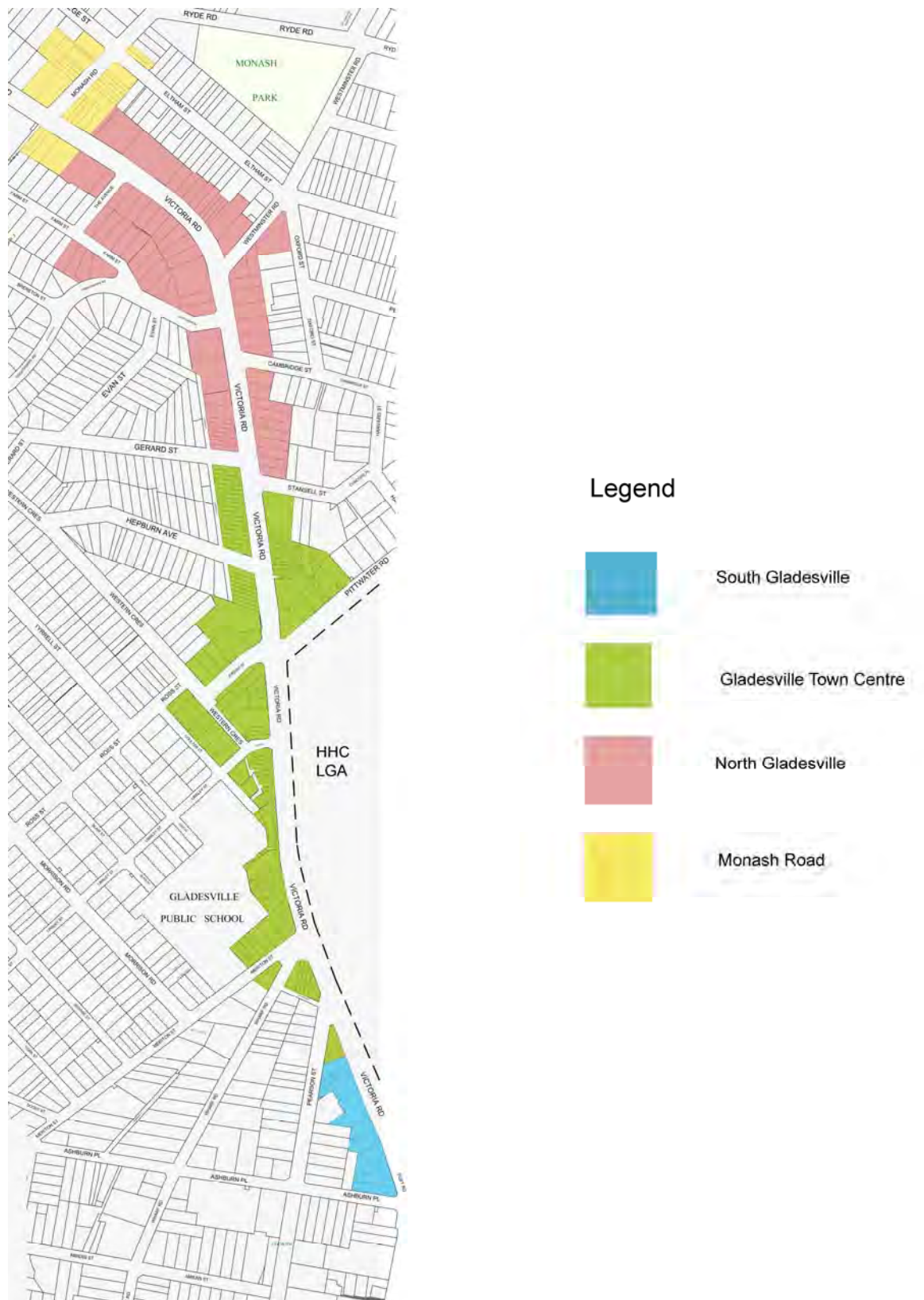


Figure 1. The Gladesville Town Centre and Victoria Road Corridor

### 2.1.2 Historical Overview of the Gladesville Town Centre and Victoria Road Corridor

The Gladesville area was settled by Europeans for farming in the late 1790's. However, it was not until after the release of residential land under the Gladesville Estate subdivision of 1855 that the village of Gladesville began to grow. The commercial centre of Gladesville developed along the main transport corridor, the Great Northern Road, which would later become Victoria Road.

The completion of important infrastructure such as the Gladesville Bridge in 1881, Pittwater Road in 1884 and the tramway to Sydney in 1910 improved the accessibility to the area. This created the impetus for rapid growth of the village of Gladesville with further residential subdivision following each of the major infrastructure improvements.

In the two decades following the arrival of the tramway the Gladesville Town Centre developed into a successful strip shopping precinct. Its success was driven by accessibility, convenience and pedestrian amenity.

A major change to the commercial district occurred in the 1930's when Victoria Road was widened. This resulted in the demolition of many of the existing buildings along the western side of Victoria Road. The 1950's saw further changes beginning with the removal of the tramways due to an increase in car numbers using Victoria Road. This began to erode the 'village' character and accessibility of the Gladesville Town Centre. Sydney's continued growth westwards; the completion of the new Gladesville Bridge and its connecting road network in the 1960's brought a dramatic and continuing growth in the volume of traffic using Victoria Road. This further reduced pedestrian amenity and accessibility of the Gladesville commercial centre.

The 1960's also saw suburban shopping patterns change from the local strip shopping model to the regional shopping complex with extensive parking facilities. The result was fewer people using local shops in areas like Gladesville. This was exacerbated in Gladesville with parking on Victoria Road being limited to improve traffic flow. Gladesville remained largely unchanged as a strip shopping area until the Gladesville shopping centre development that opened in 1999.

Over time these factors made the commercial centre more reliant on local trade. This constrained the renewal of the Town Centre and lead to diminished amenity in the area.

The remainder of the Victoria Road corridor initially developed from farming to residential use in the late 19<sup>th</sup> to early 20<sup>th</sup> century. In North Gladesville the corridor changed from a rural / residential area into a light industrial precinct from the 1900's into the middle of the century. This shaped the character of the area. In the 1980's bulky goods retail and car orientated retail outlets began to supplant the light industries. The trend has continued to the present day. South Gladesville on the other hand has remained a predominantly residential precinct.

The Monash Road precinct developed into a local retail hub in a similar way to the Gladesville Town Centre and was particularly influenced by the arrival of the tramline. During the second half of the 20<sup>th</sup> century it was subject to the same challenges as the town centre. However, these challenges had a greater impact and over time its role as a local centre was lost. Instead the Monash Road precinct developed in a similar way to North Gladesville.



### 2.1.3 Existing Character

#### Built environment



**The Gladesville Town Centre** is bisected by Victoria Road. The historic town centre runs from the clock tower at Wharf Road in the south to the Christ Church and St Andrews Church in the North. However, commercial and retail properties extend past these landmarks to the south and particularly the north. Figure 2 shows the extent of the town centre as defined by the DCP. The buildings are generally 2 stories high and are characterised by narrow allotments along Victoria Road. Most properties have their shopfront facing onto Victoria Road. There is little activity on side streets or in rear laneways. With a few exceptions the streetscape dates from the 1930's when the commercial centre had to be rebuilt due to road widening on Victoria Road.

The character of the built form in the Gladesville Town Centre reflects its function as a main street strip shopping precinct. As noted above the growth of the shopping precinct has been affected by the large volumes of traffic on Victoria Road and changes in shopping habits. There has been little renewal of the built environment to revitalise economic and social activity in the town centre.

The Gladesville shopping centre based around the Coles supermarket located in the Hunters Hill Council LGA is the exception. It is the first major retail development to move away from the 'strip shopping' model in the precinct. However, this development is insular. It does not connect well with the other retail and commercial buildings in Gladesville.

Focus on traffic management along Victoria Road has led to narrow footpaths, restrictions to on-street parking and a core retail street that does not efficiently accommodate local vehicular and pedestrian traffic.

Figure 2. The Gladesville Town Centre Precinct





Figure 3. North Gladesville Precinct

**North Gladesville** extends from Gerard and Stansell Streets on Victoria Road to north of Primrose Hill as shown in Figure 3. The area is predominantly a retail strip. Much of the retail in this precinct is servicing the traffic along Victoria Road. This includes fast food and petrol outlets. There are also bulky goods retail stores and some small scale commercial / retail buildings closer to the town centre. Our Lady Queen of Peace Church and the Gladesville Hotel, although incongruous in use, are two properties whose services are aimed at the local community.

The buildings vary greatly - from single storey residential buildings to 3 storey high warehouses. Most of the light industrial businesses that once characterised this area have closed and there are several vacant properties in this precinct. Primrose Hill is the most prominent of these.

North Gladesville is now an area that people drive through to get to somewhere else. It does not have a well defined local character.



Figure 4. Monash Road Precinct

**The Monash Road** precinct is located between the intersection with Victoria Road to the south and College and Eltham Streets to the north. It is the smallest of the 4 precincts.

Monash Road has a similar character to North Gladesville where it joins Victoria Road. However, the height and size of the individual buildings reduce to a residential scale at the College Street intersection. Apart from a café the other shops along Monash Road are specialist in nature and do not activate the street. The existing mix of shops does not provide key amenities to attract and service local residents. The shops and businesses at the junction with Victoria Road are subject to the same traffic conditions as the Gladesville Town Centre.



Figure 5. South Gladesville Precinct

**South Gladesville** is the southern entry into the town centre. The precinct begins at Punt Road and finishes at Junction Street where it meets the town centre. South Gladesville has strong historic links with the former hospital buildings nearby and contains a number of buildings from the 19<sup>th</sup> and early 20<sup>th</sup> century.

It has a stronger residential character than the other parts of the Victoria Road Corridor, particularly on the eastern side that is part of the Hunters Hill local government area. The western side of Victoria Road is more mixed. There are a number of commercial and retail buildings as well as small apartment blocks. So the junction between this precinct and the town centre is not well defined on the western side.

## Destinations, Topography and Views

- The primary destination in Gladesville is the Town Centre. The centre includes the majority of local retail stores and a Coles supermarket. A number of commercial buildings can be found in the Town Centre. There are also public community facilities such as the local library, youth centres and community nursing. These activities are facilitated by the council carparks.
- Monash Road, and North and South Gladesville have limited appeal as destinations. There is little to attract local residents or even nearby workers. Many of the businesses and built forms in these areas reflect this by catering for the one-off, vehicle borne customer. In North Gladesville where there are a large number of these retail outlets they reinforce a transient feel to the precinct and actively reduce the quality of the area as a local destination.
- The Victoria Road Corridor runs north – south along a ridge line. The land falls rapidly to the east and west. The corridor generally rises to the north. However, Monash Road and the Town Centre coincide with localised plateaus providing a relatively flat public domain for the 2 centres.
- There are potential views to the east and the west as well as to the south looking down Gladesville Road. Few of the existing buildings allow access to these views from the public/semi-public domain. Side streets, laneways and vacant sites such as Primrose Hill provide some public access to these views.
- In the Town Centre there are local views to landmarks such as Christ Church, St Andrews Church and the clock tower. Refer to figure 6.










Figure 6. Topography and Views



Photos 1a & 1b. District Views from North Gladesville

### Legend

- |   |                     |    |                                |
|---|---------------------|----|--------------------------------|
|  | Existing open space | 1. | Christ Church                  |
|  | Ridge line          | 2. | St Andrew's Church             |
|  | District Views      | 3. | Clock Tower                    |
|  | Local views         | 4. | Our Lady Queen of Peace Church |
|  | Places of interest  |    |                                |



## Open Spaces

Public open space is very limited along the Victoria Road corridor. The recently rejuvenated Trim place, the corner of Jordan and Victoria Road, and to a lesser extent Wharf Road near the clock tower are the only public open spaces.

## Paving Types

- Monash Road has 2 paving types. There is bitumen and concrete pavement with a concrete kerb and gutter adjoining Victoria Road. Further towards the east the paving becomes a 1.2m wide concrete pathway within a nature strip. The bitumen pavement is in a poor condition.
- North Gladesville has a mixture of pavement types. The most common is a concrete pathway within a nature strip. However, there are also bitumen, concrete and clay brick pavements.



Photo 2. Monash Road



Photo 3. North Gladesville

- The Town Centre has 3 pavement types. The main type is concrete block pavers. These are discoloured and stained, and there has been no consistency in colour selection when replacing damaged pavers. There is also bitumen; Trim Place has bitumen with clay paver banding. The third paver type is grey granite, which is the new standard for Gladesville (see Fig 14, Section 2.2).



Photo 4. Town Centre – Trim Place

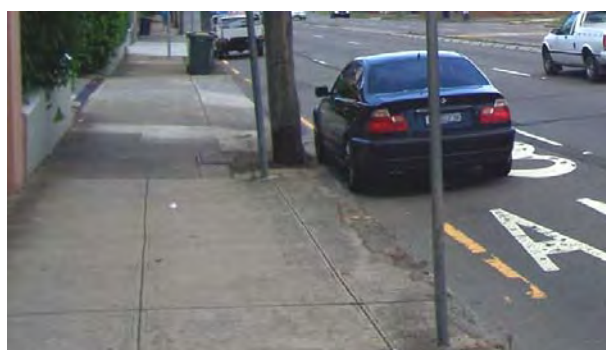


Photo 5. South Gladesville

- South Gladesville has a mixture of pavement types. These include a concrete pathway within a nature strip, bitumen, concrete and clay pavers.

### Street Tree Network and Street Furniture

- The Victoria Road corridor has few street trees. In the Town Centre and other areas that have street awnings the current footpath is too narrow to allow street trees to be planted. Therefore, tree planting is limited to open spaces such as Trim Place.
- Currently there is limited street furniture in Gladesville and it is mainly found in the Town Centre. The recently upgraded Trim Place sets a new standard for street furniture in the area.



Photo 6. Trim Place

### Pedestrian Network, Safety and Amenity

- The existing pedestrian network in Gladesville is concentrated along Victoria Road and is under pressure. Focus on vehicular traffic management along Victoria Road has led to narrow foot paths that do not provide good accommodation for pedestrian traffic. The pedestrian amenity when using Victoria Road is reduced due to the noise, pollution and proximity of the traffic. Pedestrian amenity is also affected by secondary factors such as built form, street trees and street furniture.



- North Gladesville is an unpleasant environment for pedestrians. In addition to the traffic there is little shelter or respite for pedestrians in the form of street trees, awnings or street furniture. There are no obvious destination points and the business and built form encourage car access over foot traffic. See below:



Photo 7. North Gladesville Streetscape

- Victoria Road divides the Town Centre. It is difficult for pedestrians to cross this road. The Town Centre would operate more effectively as a retail precinct if pedestrian links across Victoria Road were improved.
- The secondary streets and laneways are underutilised in the Town Centre. There is scope to improve the pedestrian network in Gladesville by better utilising secondary streets and laneways. An example of how this can be done is the Trim Place upgrade that has improved pedestrian access down Coulter Street, linking Victoria Road to the council carpark.

### Social and Cultural Resources

- Gladesville Town Centre provides a social hub for the local community. Community facilities include a public library on Pittwater Road, community nursing and a privately run youth centre in Coulter Street. There are 2 primary Schools along the Victoria Road corridor and a number of child care centres that cater for a number of different user groups.
- The Victoria Road corridor has no public art of any form. The clock tower at Wharf Road is recognised as a local monument. It replaced an earlier monument dating from 1910.

## 2.2 Urban Design Concepts

### 2.2.1 Vision and Structure Plan

This section presents design concepts and strategies for the improvement of the public domain. The secondary purpose of this section is to provide guidance to developers on how new developments should respond to the DCP public domain requirements.

#### Background

Section 2.1 has described the existing character of the Gladesville public domain. The character has changed with the historical development of the town centre along Victoria Road, and the evolution of this road into a major regional transport corridor.

A change from the current strip shopping model is required to improve the accessibility and amenity of the area for the local community. This has begun with the development of the Gladesville shopping centre. However, those shops do not connect well with other retail and commercial buildings or the public domain. A more coordinated and holistic approach is required to enhance the amenity and character of the area.

A possible model for Gladesville is the town centre of West Ryde which has started to address some of the challenges facing Gladesville. For example the laneways and streets behind Victoria Road have been activated, making the West Ryde precinct more accessible and pedestrian friendly.

To achieve these goals in Gladesville it will be necessary to work with individual building developments to extend and improve the public domain. It is expected that opportunities other than those described here will present themselves for specific developments and these should be considered.

#### Proposals

A summary of the key opportunities for improving the public domain in Gladesville are:

- 1. The creation and expansion of pedestrian connections to destinations around Gladesville to reduce the reliance on Victoria Road**
- 2. The improvement of pedestrian amenity on Victoria Road**
- 3. The improvement of the streetscape (landscaping, furniture, and artwork) along the Victoria Road corridor and secondary streets and laneways.**



### 2.2.2 Pedestrian Network, Safety and Amenity

There are a number of opportunities to extend the pedestrian network and improve the pedestrian amenity in the Victoria Road corridor.

#### a) Create new pedestrian pathways and upgrade existing pathways

New and upgraded pedestrian pathways will give local pedestrians alternative access to shops and facilities free from the traffic congestion, noise, pollution and overcrowding associated with Victoria Road. This will also alleviate pedestrian congestion on Victoria Road. The network should integrate public pathways with new connections through private properties to provide a safer pedestrian environment. These pathways will improve the local community's access to the Gladesville town centre.

The pedestrian connections can take many forms depending on their purpose and whether they are publicly or privately owned. These include:

- Footpaths,
- Retail arcades,
- Covered walkways,
- Laneways,
- Parks and open spaces &
- Right of ways



Photo 8. Royal Arcade Melbourne



Photo 9. Hardware Lane Melbourne



Photo 10. Internal street  
Rouse Hill Town Centre



Photo 11. Block Arcade entry  
Melbourne



In North and South Gladesville footpaths will be widened to provide greater amenity for pedestrians. On key sites the DCP nominates specific pathways be created. On other sites the pedestrian connections should be developed on a case by case basis. Generic options for future developments on these sites are shown in Figure 7.

All pathways should link to 'destinations' to encourage use (refer to Section 2.2.3). The Town Centre and North Gladesville will benefit most from this initiative.

Figure 7

## Legend

-  Study Area
-  Existing Open Areas
-  Existing Pedestrian Connections to be Upgraded
-  Possible Future Pedestrian Connections
-  Future Open Areas



### b) Provide better pedestrian access across Victoria Road

Providing better access across Victoria Road is an important step to improving the amenity of the Town Centre. Crossing Victoria Road is difficult for pedestrians because of the large volume of traffic. Victoria Road's importance as a main road also limits possible solutions. The DCP makes allowance for a pedestrian overpass within the zone noted in Figure 8.

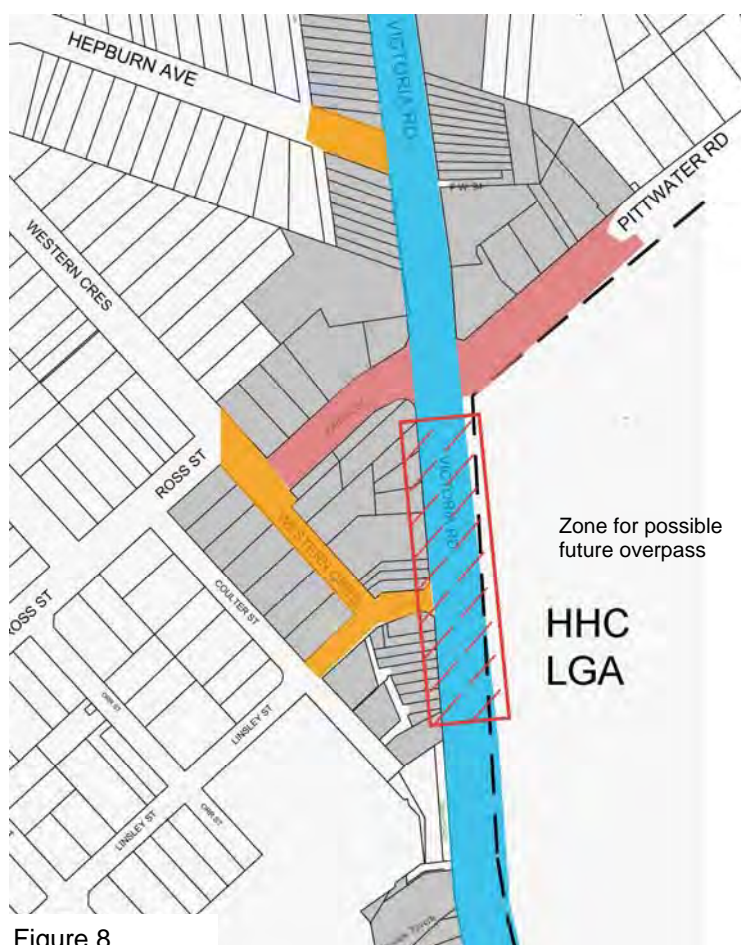


Figure 8.

The overpass will achieve the following:

- Provide a connection to adjacent buildings.
- Be a gateway element into Gladesville.
- Be a high quality design.
- Comply with RTA requirements.
- Coordinate with existing services and utilities.
- Meet the safety requirements noted below.

#### One other solution can be considered:

Create a large pedestrian crossing zone between Cowell and Massey Streets to operate in non-peak traffic periods (subject to RTA approval). The following issues need to be considered:

- Sequencing of traffic lights to create a large pedestrian crossing zone.
- The need to restrict carparking in this area during operation of the pedestrian zone.
- Review bus stop locations to maximise pedestrian zone.



### **c) Provide better pedestrian access along Victoria Road**

Improving the pedestrian access along Victoria Road will provide a better visual and physical connection within the Town Centre and the Victoria Road Corridor. Figure 9 shows 'major' and 'minor' intersections along the corridor.

Vehicles can be encouraged to use the major intersections where existing signalised pedestrian crossings will be retained. At the minor intersections such as Linsley Street, pedestrian right of way can be encouraged over vehicle movement.

In order to increase vehicle awareness of pedestrians and provide visual links across minor intersections for pedestrians, the following will be considered:

- Entry markers or precinct signage at crossings.
- Specific road treatment at crossings to provide a visual contrast to the general roadways.
- Increased width of pedestrian crossings.
- Widening of footpaths at intersections.

Monash Road is subject to different conditions to the remainder of the Victoria Road Corridor. The volume of traffic on Monash Road is smaller than on Victoria Road, however crossing the road is difficult for pedestrians. It is proposed that a pedestrian crossing be installed near the junction with Eltham Street to complement the signalised crossing at Victoria Road. Other measures to reduce traffic speeds and encourage driver awareness of pedestrians could be considered. These include:

- A roundabout at the junction with Eltham Street.
- Speed humps between Eltham Street and Victoria Road.
- Visual cues for pedestrians and motorists such as different road treatment and entry markers.

### **d) Safety**

- All pathways, private or public, need sufficient width, lighting and finishes to meet public safety requirements. A minimum width of 3m is required for new pathways.
- Where appropriate pathways should be adjacent to active street fronts to improve amenity and safety.
- Public access should be available between at least 7am to 7pm.
- Where possible pathways should be located separately to vehicular traffic.

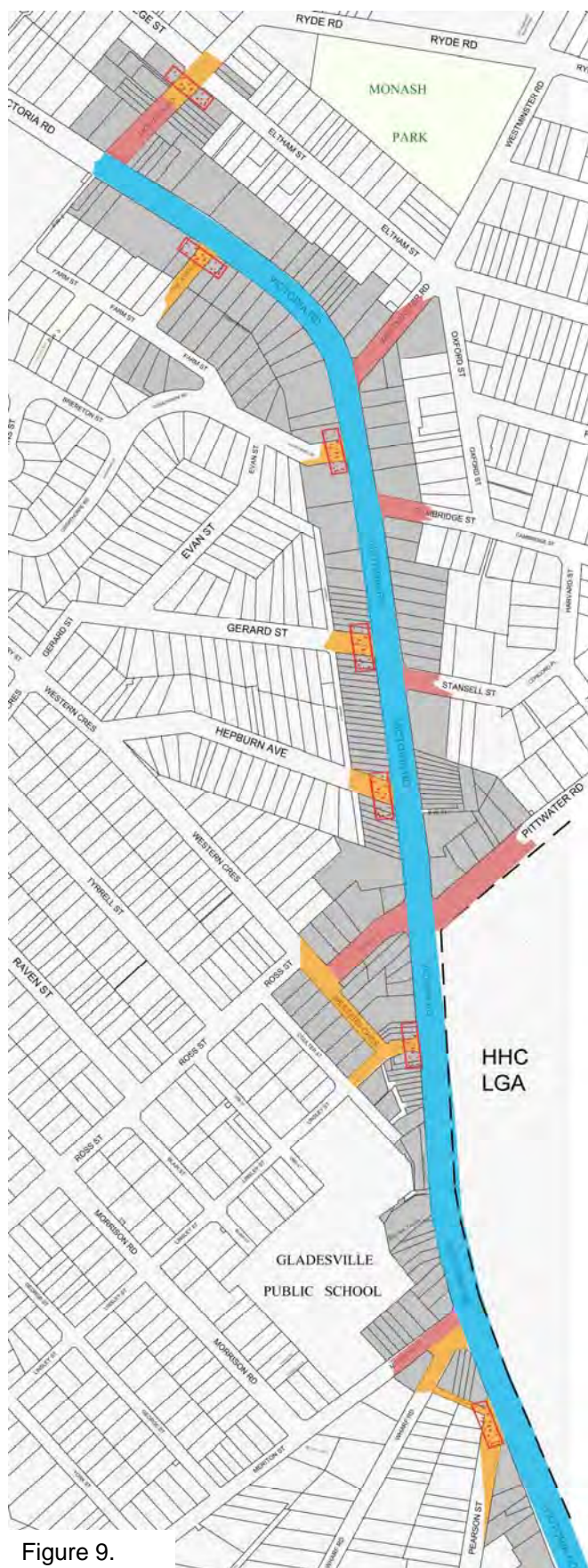







Figure 9.

## Legend

-  Study Area
-  Victoria Road
-  Major Intersections
-  Minor Intersections
-  Pedestrian Priority Points

### 2.2.3 Destinations

The new pedestrian connections should link with specific destinations. A destination can include one or more of the following:

#### Open spaces

The DCP nominates a number of key sites where open spaces will be developed. In addition to these key sites the DCP envisions that open spaces be created in other developments as appropriate on private and public property. Ideally the open spaces should occur away from the direct influence of the Victoria Road traffic, but retain a connection to this main commercial and transport spine. Open spaces may include urban parks, communal squares, widened footpaths, atria, retail courtyards and arcades. Figure 10 shows where opportunities to provide open spaces exist.

#### Active Streets

Street activation occurs where land use and built forms combine to attract people to an area. There are a number of land uses that encourage street activation including retail outlets, outdoor dining and community facilities. Spaces that allow for specific events such as markets and festivals are also important. Building form can assist in street activation if the building is attractive and provides interest. At street level the shopfront should make people feel safe and welcome, and the public and private zones should be integrated. To achieve this, the shopfront should be visually open to the street as shown in the example below.



Photo 12. Activated shopfront

#### Views

The significant views to east, west and south throughout the Victoria Road corridor should be utilised. Views to the east and west are available from the rear of most properties facing Victoria Road. New developments should take advantage of these views. Public access in the form of lookouts, lobbies, balconies and active retail will be encouraged.

#### Community facilities

The pedestrian connections will be located to access community amenities. These can be public institutions such as a school, library and community hall but equally these can be cafes,



newsagents, medical practitioners, bakeries and so on that service the local community. Figure 10 shows proposed new or upgraded connections to a number of public services.

### Landmark buildings

Landmark buildings are destination points. Landmark buildings include projects of architectural excellence, historic significance or nationally significant sustainable buildings. Refer to Section 2.2.4 for more information.



## 2.2.4 Semi-Public Spaces

### Built form

The built form has a significant impact on the public domain. The DCP controls the built form. New developments need to address issues such as street activation, access to daylight and articulation of the façades. These elements should be developed to complement and enhance the public domain.

The DCP controls, the relatively narrow blocks and the topography of the sites facing Victoria Road mean that appropriate building types are row or street wall buildings. Occasional 'laneways' between and through the buildings to provide visual and physical links through the sites will be encouraged to break up the blocks. The built form will generally step down the ridge line along Victoria Road and to the rear boundary of these properties. The key sites nominated generally have larger and deeper blocks and so have scope to develop other building types.

Meeting the DCP controls includes creating semi-public areas such as arcades, courtyards and lobbies that address and are consistent with the public domain. The entry to Our Lady Queen of Peace church is an example of a semi-public space linked with the public domain and the future pedestrian connection through Primrose Hill (Figure 11). A second example is the courtyard in front of the Christ Church as shown below in Figure 12. It can be seen that in these examples the semi-public spaces are located along view corridors from the public domain and this shall be supported in future developments.

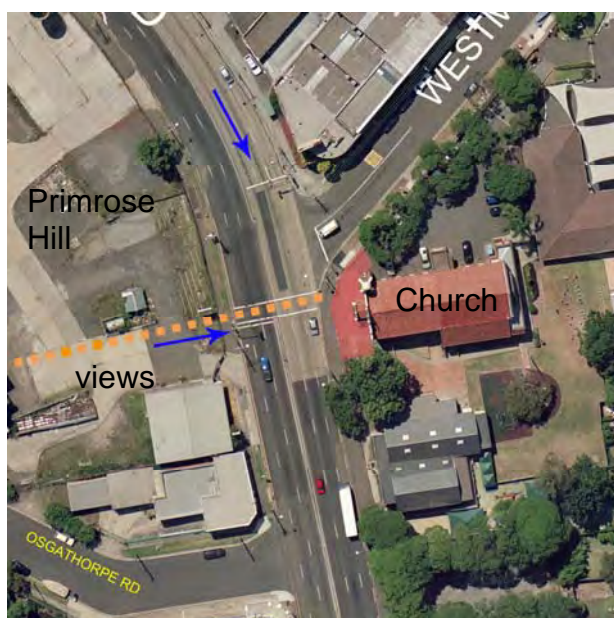


Figure 11. Our Lady Queen of Peace



Figure 12. Christ Church

### Landmark Buildings

Landmark buildings are buildings of excellence, and as such will attract interest from the wider community. They may include examples of outstanding urban design, architectural design,



heritage significance or sustainability. They can be the catalyst for commercial growth and tourism within a centre. See photos 13 – 15.

Landmark buildings improve the urban environment for the community. They provide premium sought-after spaces internally and a good commercial return for the owner, which flows on to the local community. The key sites are identified as locations suitable for landmark buildings in Gladesville. The development of landmark buildings on other sites will be supported.



Photo 13. Albury Library – City place marker



Photo 14. Commercial building  
Double Bay – urban re-activation



Photo 15. CH2 Melbourne –  
'World best' environmental  
performance



## **Car parking**

The location and accessibility of public car spaces is an important consideration in improving the amenity of the public domain in Gladesville.

The DCP requires:

- The current numbers of public car spaces to be retained in any new developments.
- Where possible, access to carparks should be off secondary streets and not from Victoria Road (RTA requirements).

In addition, the entries to carparks should be located to minimise the impact on pedestrian networks. Carparking should be located to provide easy access to new open spaces, shops, community facilities, etc but should not be immediately adjacent or visible from these areas.

In the Monash Road precinct short term on-street carparking will be maximised to provide accessibility. Future developments should consider incorporating public carparking to provide greater amenity for local residents.

### **2.2.5 Street trees, Paving and Street Furniture**

Improving the streetscape quality has a direct and immediate benefit for the public domain. The opportunities for improving the streetscape are noted in Section 2.1. All new developments are required to incorporate streetscape improvements under the current DCP.

The scope and general requirements for street trees, paving and street furniture are noted below. Refer to Council's Public Works Department for detailed specifications of these items. The detailed specifications should be considered when preparing development application documents. It is important these elements are coordinated between the public and semi-public domains.

#### **Street trees**

Refer to Street Tree Plan (Figure 13) for the location of future street trees. It also nominates possible future open spaces.

Extensive tree planting will generally not be possible in the Town Centre. The Town Centre area as defined in the DCP retains the existing building lines and awning line. Elsewhere the DCP nominates building setbacks for new developments. The setback will allow an increase in the width of the footpath that will accommodate extensive tree planting.

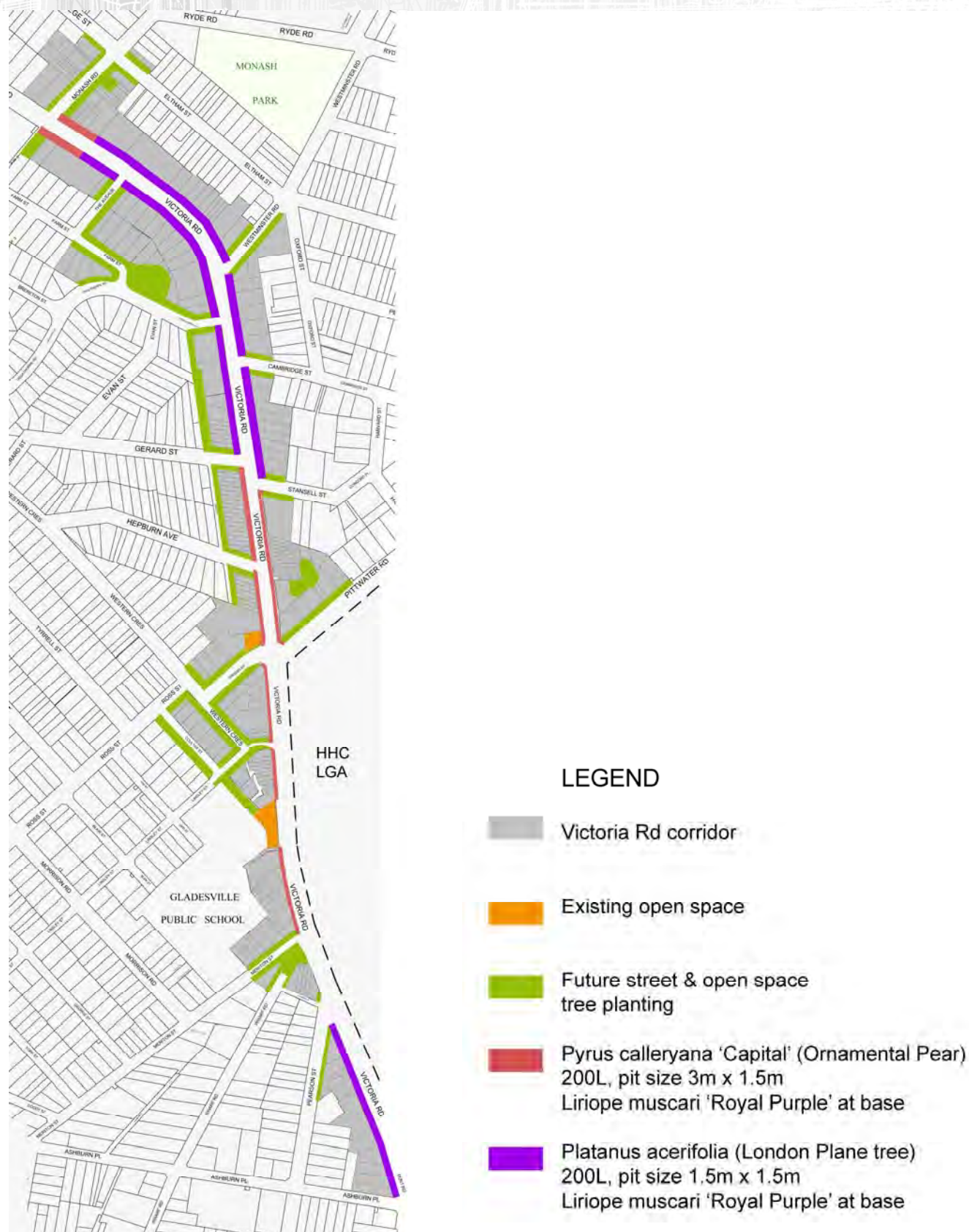


Figure 13. Street Tree Plan



## GLADESVILLE STREET TREES Data sheet



Pyrus flower and bark

Pyrus in Victoria Road, Gladesville (summer)

*Pyrus calleryana* 'Capital' – Capital Pear

- Narrow shape to fit limited space on Victoria Road
- Deciduous - good shade in summer, sun in winter
- Autumn colour
- White flowers in spring
- Tolerates full sun and air pollution



Platanus bark and foliage

*Platanus acerifolia* – London Plane Tree

- Excellent shade tree, hardy and long-lived
- Tolerates full sun and air pollution
- Quick growing, establishing in 3 to 5 years
- Attractive – interesting leaves and bark



## Paving

The existing paving will continue to be upgraded to granite throughout the Victoria Road corridor as shown in the paver layout plan and photo of Gladesville town centre below. Specifications can be obtained from Council's Public Works Department.

Refer to Figure 14 for the extent of paving and paving types.



Figure 14. Paving Plan



Paving type 1 – Victoria Rd, Gladesville

## Legend



Study Area

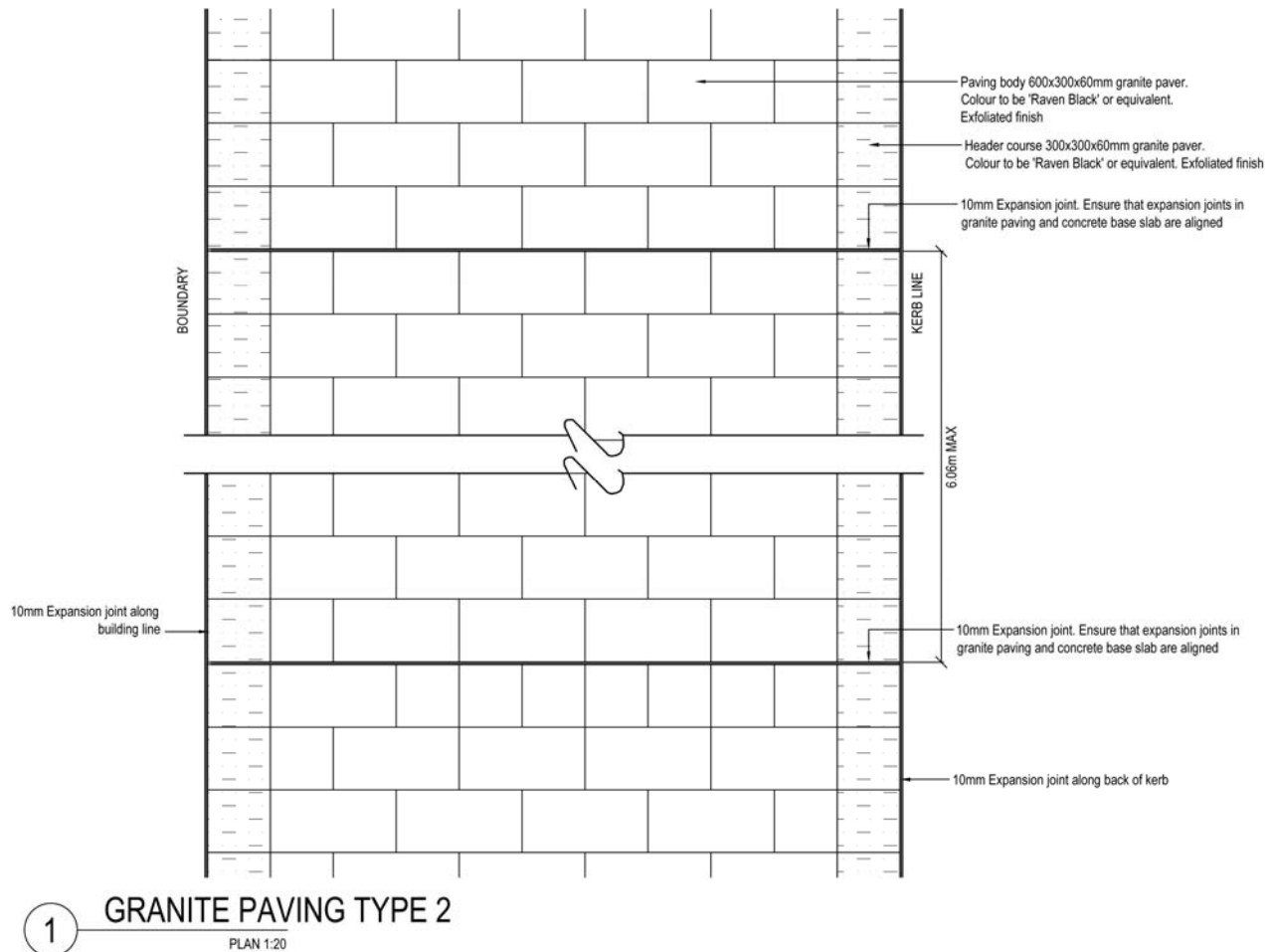


Paving Type 1  
grey granite with  
sandstone-coloured granite banding



Paving Type 2  
grey granite

## Paver layout plan



## Street furniture

The following street furniture shall be provided to Council's satisfaction for all new developments in the Victoria Road corridor:

- Seating (Seat 01 and Bench 01)
- Bins (Bin 01)
- Bollards (Bollard 03)
- Bus shelters

Additional street furniture that may be considered for specific sites includes:

- Tables
- Bike racks
- Drinking fountains
- Café barriers

Specific models can be obtained from Council's Public Works Department.

### Lighting and over-head services

Multifunction poles are to be installed in Victoria Road (from Monash Road to Pearson Street), Jordan Street (from Victoria Road to Western Crescent) and Pittwater Road (from Victoria Road to no.6 Pittwater Road). These will incorporate lighting, street signage, and banners into one element. See Figure 15 below.

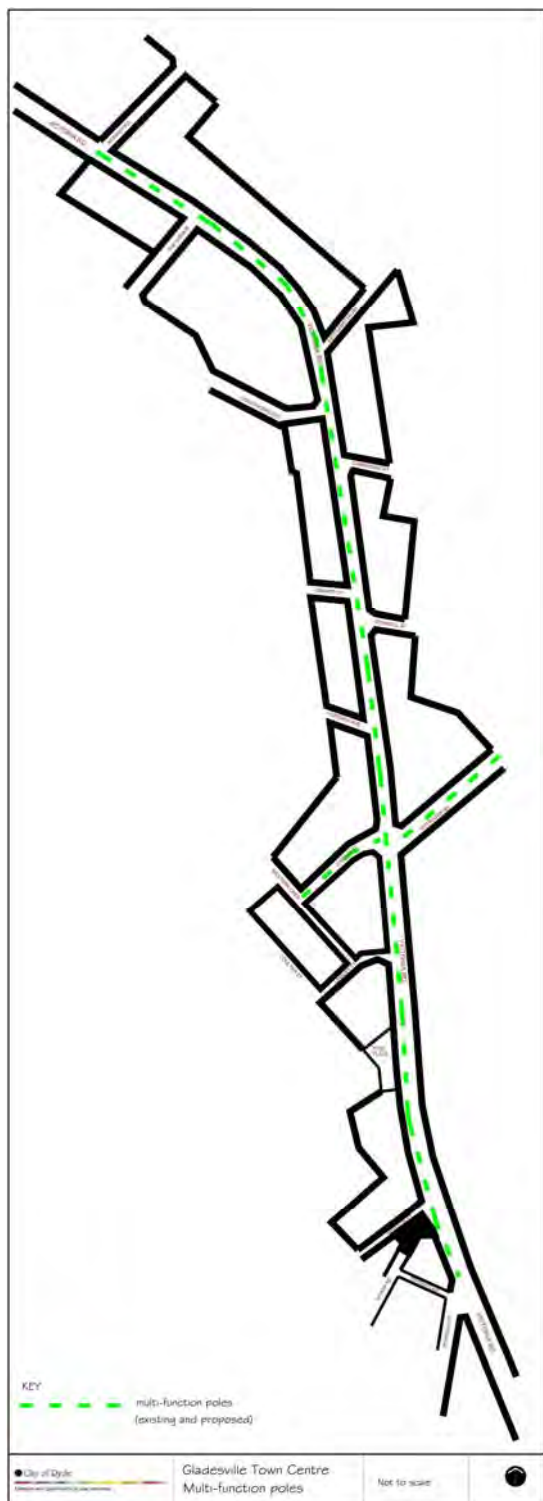


Figure 15 – Multi-function poles Plan



Power lines and other services shall be re-located underground. Refer to Figure 16 below for the general extent of under grounding required. Refer to Public Works for a more detailed extent.

### **Signage**

Refer to Section 9.1 of the DCP for general signage.

Signage can enhance the prominence of the major entry points into the Victoria Road corridor. The City of Ryde is implementing a program to provide 'gateway' signage in Gladesville. Precinct signage and local signage should be coordinated to improve the orientation and movement of pedestrians within the corridor. A Civic Signage Manual has been developed to create a consistently high standard of public signage in the Ryde Local Government area.

Smaller miscellaneous signage should be located on the multifunction poles where practical.

### **Environmental performance**

The environment performance can be enhanced by using the following when selecting and detailing materials:

- Recycled materials
- Functionally and aesthetically long lasting materials
- Materials that require minimal maintenance.

Water Sensitive Urban Design (WSUD) should be incorporated into the public domain design.

Refer to the City Of Ryde draft DCP for detail on achieving a water sensitive urban design.



Required extent of under  
grounding of overhead services  
is shown dotted.

Refer to Council's Public Works  
Dept for the extent required on  
an individual site.

Figure 16. Extent of under-grounding of overhead services

### 2.2.6 Public Art

Public art can make an important contribution to the local community in a number of ways. These include:

- Providing visual enjoyment and interest to everyday activities and social gatherings.
- Highlighting and enhancing the local character of an area. Making it a memorable place to visit and return to.
- Marking the identity of key sites:
  - Gateways into Gladesville
  - Historic precincts
  - Open spaces / recreation areas
  - Public facilities
  - Landmark buildings (see photos 13-15 above)
- Creating a point of reference for the local community and visitors.

Proposed art works should be site specific and respond to the distinctive character of the local area. Artworks can be integrated with buildings and landscaping elements or be stand alone objects.

Some general concepts that should be explored in art work include:

- Historic themes such as:
  - the area's development as a transport hub,
  - links to the Parramatta river,
  - communications (flagstaff station 1820's),
  - past industrial uses (North Gladesville)
  - the character of specific sites such as the Christ Church and St Andrew's Church.
- 'Gateways' into North and South Gladesville.
- Social and cultural values that have importance for the local community.

Opportunities exist to include custom made, artist designed furniture that will celebrate specific aspects of Gladesville's character. This would be appropriate for the key sites nominated in the DCP or landmark buildings.

Semi-public art work associated with individual buildings is encouraged. This can be in the form of art walls, screens or other building elements which provide exposure for the particular building but also interest for the community.



### 2.2.7 Cycleways

Residents have highlighted the need for better facilities for cyclists in the local area. The Ryde Bicycle Strategy and Masterplan (2007) shows the proposed regional bike network within Gladesville and surrounding suburbs. This Masterplan also shows there are opportunities to provide local bike routes within the Victoria Road Corridor. Additionally, facilities for cyclists such as convenient bike parking and change facilities are to be encouraged in the town centre.

The lack of open public space in Gladesville limits these opportunities at present. The development of the key sites and other large projects should provide the impetus for improving the environment for cyclists.

In terms of specific strategies for local cyclists it is proposed to create a bicycle path in Gerard Lane. This is based on the gradual widening of Gerard Lane as future development occurs along Victoria Road (in accordance with the DCP). The bicycle path will provide a link between North Gladesville and the Town Centre for local residents and students. It also links with the regional bicycle network shown in the Ryde Bicycle Strategy and Masterplan (2007) for other cyclists.

The path will terminate in the heart of the Town Centre at Trim Place. This is also convenient to the Gladesville Public school and provides access to public transport along Victoria Road. In addition bicycle parking would be provided as part of a future upgrade or extension of Trim Place. In this location parking will be accessible to shoppers, students and commuters.

Figure 17 shows the proposed route of the bicycle path and location of parking facilities.

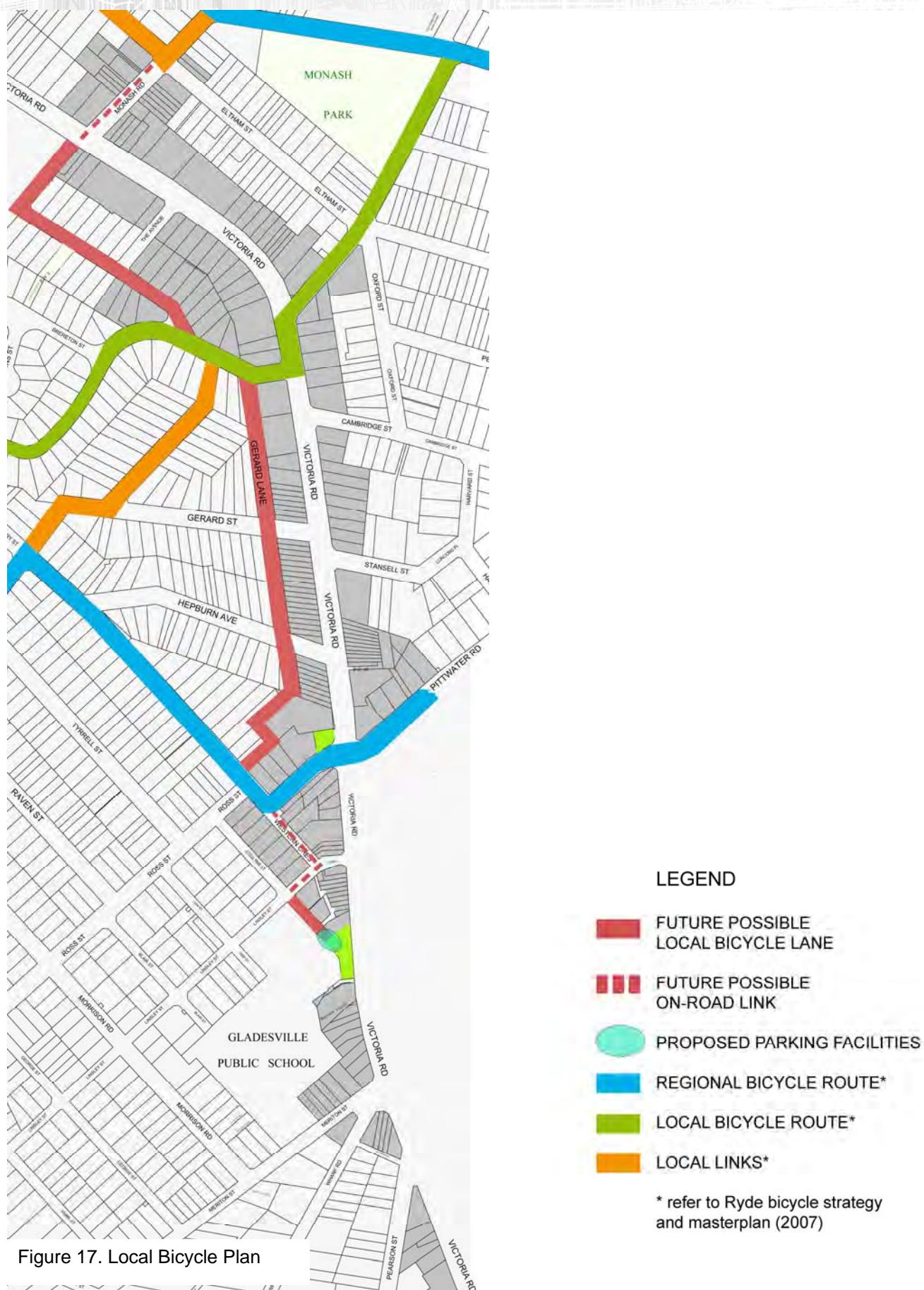


Figure 17. Local Bicycle Plan



## 3. Eastwood

### 3.1 Analysis of Existing Character

#### 3.1.1 Introduction to the Eastwood Town Centre

Eastwood is located in the City Of Ryde local government area approximately 16 km west of Sydney. This public domain plan applies to the Eastwood town centre as shown in Figure 1. A new draft Master Plan for Eastwood is currently on hold but is expected to proceed in the future. The outcomes of this study may impact on the extent and content of the public domain manual. It is expected that the public domain manual will be updated following approval of the future master plan.

Eastwood's town centre is unaffected by the major roads and associated traffic that characterise other town centres in the City Of Ryde. However, the Northern railway line divides the town centre into two parts (east and west of the railway line). The main commercial/retail centre of Eastwood is to the west of the railway. East of the railway line there is a smaller commercial precinct surrounded by medium density residential buildings.

Rowe Street is the central retail spine in Eastwood, extending across both sides of the centre. The west side is retail in character, while the east side is more mixed and generally has less activity. The character of the public domain also varies on either side of the railway. The western side has a greater diversity of public spaces and facilities than the eastern side.

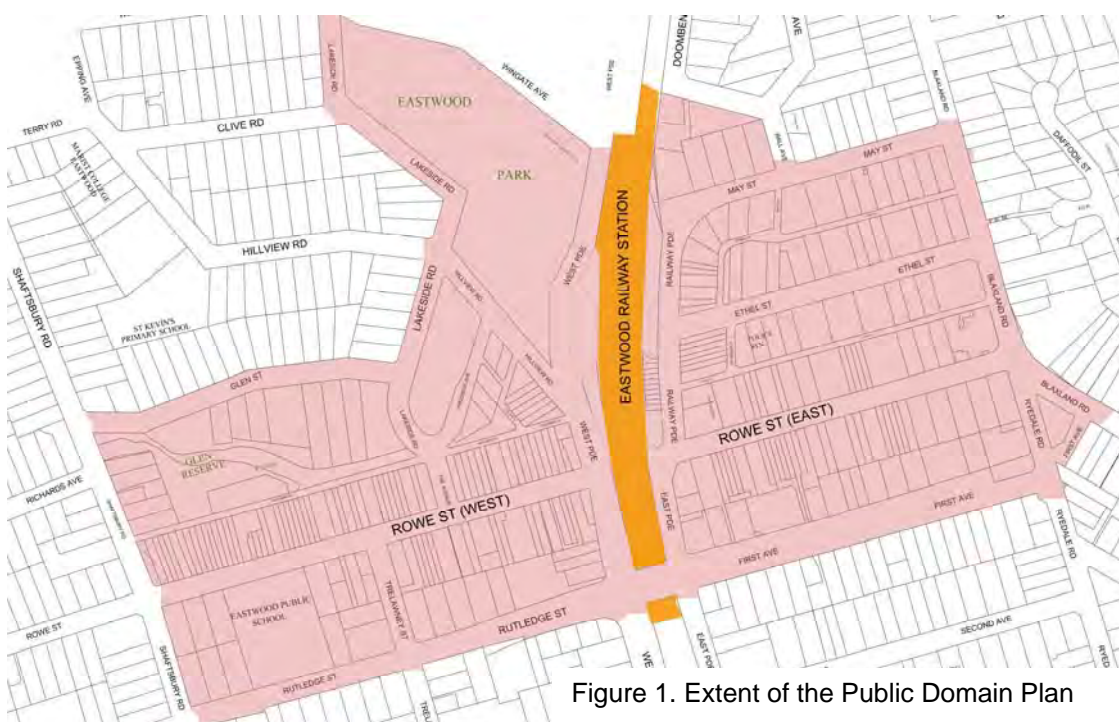


Figure 1. Extent of the Public Domain Plan



### 3.1.2 Historical Overview of the Eastwood Town Centre.

Originally the Eastwood area with its good rainfall and deep clay soils was occupied by Blue Gum High Forest (*Eucalyptus saligna*) from the ridgelines to the valley floors, and was home to the Wallumedde Aboriginal People.

The origin of Eastwood as an identifiable locality dates from 1840 when 'Eastwood' House was built by William Rutledge. However, it was Edward Terry who purchased Eastwood House and adjacent properties in 1865 who had the largest impact on the future character of the area. Terry first dammed the creek running through his property creating a series of lakes which would later become Eastwood Lake. More importantly he used his influence to route the new Northern Railway line through his property which he then began to subdivide east of the railway.

The opening of the railway line in 1886 and the earlier construction of the Gladesville Bridge in 1881 improved rail and road links to Sydney and provided the impetus for the development of Eastwood as a township. Thus the settlement of Eastwood was established in the late 1880s centred on the Eastwood railway station.

Timber felling provided a valuable income for the early settlers of the area. They were followed by farmers who planted orchards and the Granny Smith apple, now known worldwide, originated in Eastwood. The railway line provided greater access to markets for the farmers and production increased. Progressively land was further cleared for this purpose. Over time, further subdivisions occurred and residential dwellings overtook the farming acreage of the area.

The Eastwood town centre developed east of the railway along Rowe Street, Rowe Street at this time being the main road connecting to Parramatta in the west and to Ryde in the east. However, further subdivision of the Terry estate west of the railway occurred in 1912 and 1914. The release of this land led to commercial development on Rowe Street, west of the railway line.

Within a relatively short period of time the town centre shifted west of the railway. This was driven by accessibility, availability of land and population drift westwards. For example up until 1929 both sides of Rowe Street were connected by a level railway crossing. Following the closure of this crossing the town centre was divided making access to the eastern side more difficult. Also beginning in 1929 the Eastwood Lake and much of the creek was filled in. This provided additional land for the town centre (because it was unsuitable for residential development).

The 1960's brought changes in shopping behaviour that favoured larger shopping centres and convenient parking over smaller standalone strip shopping. This led to the development of the Eastwood shopping centre and Glen Street carpark in the 1970's, further focusing retail activity west of the railway. However, in Eastwood much of the village scale, built fabric and atmosphere developed in the first half of the 20<sup>th</sup> century has been retained despite increased suburban development.

Timeline:

- Pre 1788: Aboriginal tribe of the area called EORA (meaning people). The Aboriginal name for the district is Wallumetta, and was occupied by the Wallumede people.
- 1788: First Settlement of NSW
- 1795: Private John Love of the NSW Corps receives land grant in 'Eastwood'
- 1840: Eastwood House built by William Rutledge
- 1865: Eastwood House purchased by Edward Terry who creates Eastwood Lake
- 1881: Gladesville Bridge is opened allowing direct road access to Sydney
- 1883: First Post Office agency in Eastwood
- 1884: Eastwood Public School opens
- 1886: Northern railway line opens
- 1886: Eastwood village develops east of the railway station
- 1890: 'Granny Smith' apple became a commonly cultivated variety in the Parramatta district
- 1912 & 1914: The Terry estate is subdivided west of the railway
- 1920: new Post Office opens in Rowe Street (East)
- 1929:
  - Start of Depression – Reclamation of Eastwood Lake begins as part of local employment scheme.
  - Railway level crossing on Rowe Street is discontinued,
  - Rutledge Street two-lane overbridge is opened
- 1958: station subway is opened
- 1965: new Police station opens in Ethel Street
- 1969: Post Office is transferred to Progress Avenue
- 1975: Construction of Eastwood Centre
- 1978: Rutledge Street six-lane concrete overbridge completed
- 1984: Major flooding to the town centre
- 1985: Rowe Street mall created

### 3.1.3 Existing Character

#### Built environment

Eastwood has a busy and attractive town centre. The streetscape is dominated by small retail shops which create an active but relaxed atmosphere for residents and visitors. Culturally there is a diversity of backgrounds, with a particularly strong Chinese and Korean influence which is reflected in the local streetscape.

The majority of buildings in the town centre date from the interwar period. They are typically brick, 2 stories high and are characterised by narrow allotments. This gives the retail precinct a consistent appearance particularly along the western side of Rowe Street. Here the properties have their shopfronts facing onto Rowe Street and there is little activity on rear laneways.

Similar building types continue along Hillview Road and to some extent Progress Avenue. In these streets there is a diversity of public spaces which creates visual interest and opportunities for social interaction.

Generally the historic street layout and building forms are intact; however there are a number of more modern buildings that have altered the streetscape. The most significant of these is the Eastwood Centre which is the largest building in Eastwood in both floor area and height. There is an approved DA for the re-development of this site.

The Glen Street car park has a major impact on Eastwood. Visually and functionally poor, it nevertheless has a vital role in providing convenient parking in the town centre. The car park is particularly important for the shops adjoining Progress Ave because they have no parking of their own. This includes the Franklins supermarket site. The supermarket and adjacent shops have large floor plates relative to the older buildings.

The supermarket site and the Eastwood Centre have service areas and portions of blank facade that affect the streetscape. However, the two retail buildings have maintained active shopfronts along Progress Avenue and Rowe Street respectively, which has helped integrate these developments with the remainder of the streetscape on these streets.

On the eastern side of Rowe Street the built form is similar to the west but includes some larger, wider blocks. Many of these larger properties were previously bulky goods or light industrial premises. More recently a number of these sites have been redeveloped for commercial and residential use.

Surrounding the commercial strip along Rowe Street there are a number of medium density residential buildings. These buildings are quite distinct in character from the historic town centre and date from the 1960's.





Eastwood Village, Progress Ave

### **Destinations, Topography and Views**

The primary destination for visitors and residents in Eastwood are the retail facilities found in the town centre west of the railway line. These are popular for their diversity, convenience and value. Other important destinations include:

- The retail facilities on the eastern side of the railway line, particularly the restaurants.
- The local private and public schools.
- The railway station and associated transport facilities such as buses and taxis.
- Community services such as the local library, aged care support, child care, sports facilities and community nursing.

Geographically Eastwood is made up of several hills. The town centre is located in a valley surrounded by these hills. Much of the town centre is located over a former creek bed including Eastwood Park (which was once a lake) and Glen Reserve. The culvert in Glen Reserve follows the original creek line. The topography of the area is such that the town centre remains subject to flooding particularly on the western side of the railway line.

The topography combined with the built form restricts district views from within the town centre. However, there is a significant local vista looking east along Rowe Street culminating at Blaxland Rd. There is also potential to create better views into Eastwood Park and Glen Reserve although they are currently cut off from the town centre.

### **Open Spaces and Street Activation**

Open spaces play an important role in creating a high quality public domain and providing opportunities for community interaction. Eastwood benefits from a variety of open spaces within the town centre. Street activation is often related to the success of the open space network and this is encouraged in Eastwood. There are a number of land uses in the town centre that encourage street activation including retail outlets, space for outdoor dining and community

facilities. There are also larger spaces that allow for specific events such as markets and festivals.

The existing open spaces in Eastwood include:

- **The Rowe Street Plaza.** This is an attractive and well-used space, with many seats and trees.
- **The western side of Progress Avenue** which has recently been upgraded to provide opportunities for outside dining and other activities.
- **The Progress Avenue/ Lakeside Road intersection** which has seating and sun shading.
- **Eastwood Park.** This is a pleasant and well-used park with both active and passive recreation areas. It also contains the Eastwood library. However, the visual and physical links to the town centre could be improved.
- **Glen Reserve.** This is a passive recreation area with seating and play equipment. This park is adjacent but isolated from the centre. The paths that connect it to the town centre could be important linkages, but end awkwardly at Hillview Lane (behind the Rowe Street shops) and at the back of the Glen Street car park. Neither is an obvious or welcoming entrance to the centre.

The urban spaces are currently well used and some areas are at capacity. There is demand for more of these spaces throughout the town centre.

Community access to the two parks could be improved if they were better integrated with the town centre.

There is little open space east of the railway line. This limits potential social and commercial interaction on this side of the town centre.



Rowe Street Plaza



Progress Ave/ Lakeside Road

## **Paving types**

The quality of paving in Eastwood varies considerably. There are sections of good quality paving mixed with areas of poor quality paving. The key paving types are as follows:

- Brick paving predominates on the west side of the railway line with varying degrees of quality (eg Rowe Street, Rowe Street Plaza, and West Parade).
- Progress Ave in Eastwood Village has recently been repaved with clay pavers in a herringbone pattern and sets a new standard for the area.
- The eastern side of the railway has poor quality patched concrete pavement (eg Rowe Street (east)) and asphalt (Railway Parade). A section of Railway Parade has recently been paved with clay pavers in a herringbone pattern.

Visually the quality of the public domain is only as good as its weakest part. In Eastwood the lack of a unified paving makes the streetscape appear untidy and dated.

## **Street Tree Network and Street Furniture**

Street trees play an important role in creating a high quality public domain. Tree lined streets are attractive and can visually soften hard urban spaces. Eastwood has the basis of an effective urban street tree network. For example:

- Rowe Street Plaza has a large canopy of wisteria dominating its centre. This shaded area appears well used by the community. The plaza also contains mature shade trees and seats that are well used.
- An upgrade of Progress Ave has included effective street tree planting.
- Eastwood Park and Glen Reserve have a number of large trees that contribute to the street tree network. However, there could be better connection to the town centre with long term planning and plantings.

In other areas of the town centre tree planting is limited by the historic footpath widths, overhead awnings and the location of services. These constraints derive from the historic development of the town centre. The same constraints do not apply to the residential areas east of the railway line. There is potential to upgrade the street tree network in this precinct.

Overall the street tree network is incomplete. There is no strong boundary planting to delineate the centre, or consistent tree planting along Rowe Street to link the two sides of the railway line.

The availability and appearance of street furniture is mixed. East of the railway line street furniture is absent from the residential precinct and limited in the commercial precinct. The western side of the railway is better serviced. In the Rowe Street Plaza, the western side of Progress Ave and the two parks the quality and availability of street furniture is good. However, this is not matched in the surrounding streets.





Glen Reserve

### **Pedestrian Network, Safety and Amenity**

Eastwood has a strong and successful pedestrian network as shown in the diagram below. Pedestrian activity is focused on Rowe Street (west) and the surrounding streets. The pedestrian traffic is serviced by the public car parks in Glen Street and the Eastwood Centre. Eastwood railway station and the associated bus station on West Parade are also major sources of pedestrian traffic.

The east side of the railway line has fewer attractions and correspondingly less pedestrian activity but is a popular restaurant precinct.

In Eastwood the pedestrian activity has a number of positive effects:

- It improves interaction with open spaces (public and private).
- It helps create a vibrant shopping precinct.

## PEDESTRIAN MOVEMENT IN EASTWOOD TOWN CENTRE



Above: Pedestrian Movement

KEY

	8-9am	Noon	5-6pm
①	200	930	476
②	178	913	666
③	126	477	369
④	441	308	386
⑤	42	316	279
⑥	67	138	82
⑦	264	129	157



Source: Pedestrian Access and Mobility Plan 2009 (ARUP).



### **Traffic calming**

The high numbers of pedestrians act as a traffic calming device especially around the Progress Avenue/ Lakeside Road intersection, reducing vehicle speeds in the surrounding streets.

### **Safety**

High levels of pedestrian activity and associated street activity improve personal and property security, whilst calming vehicles is important in maintaining a safe environment for pedestrians.

However there are a number of instances where the safety and amenity of pedestrians could be improved:

- The railway station divides the centre into an east side and west side. The pedestrian connection between the two sides of the railway is poor, consisting of two tunnels.
- The bus station and railway entry create some pedestrian congestion in Eastwood Park near the library. The pedestrian pathways and links to the town centre and surrounding schools are not well defined in this area.
- There are potential pedestrian/traffic conflicts along West Parade and Hillview Lane.
- The smaller laneways generally do not have good amenity for pedestrians.
- Pedestrian links within Glen reserve are restricted due to a stormwater culvert.

### **Social and Cultural Resources**

- Eastwood Town Centre provides a social hub for the local community. Community facilities include a public library, aged care facilities, child care facilities and meeting rooms.
- There is currently no public art in the Eastwood centre.



## 3.2 Urban Design Concepts

### 3.2.1 Vision and Structure Plan

This section presents design concepts and strategies for the improvement of the public domain. The secondary purpose of this section is to provide guidance to developers on how new developments should respond to public domain requirements.

#### Proposals

A summary of the key opportunities for improving the public domain in Eastwood are:

- 1. The upgrade and creation of the pedestrian connections to destinations around Eastwood.**
- 2. The creation of publicly accessible open space in the eastern part of the town centre.**
- 3. The improvement of the streetscape (landscaping, furniture, and artwork) in the town centre particularly on the secondary streets and laneways to encourage pedestrian access.**

### **3.2.2 Pedestrian Network, Safety and Amenity**

There are a number of opportunities to extend the pedestrian network and improve the pedestrian amenity in the Eastwood town centre.

#### **a) Create new pedestrian pathways and upgrade existing pathways**

New and upgraded pedestrian pathways will give local pedestrians alternative access to shops and facilities. The pedestrian connections can take many forms depending on their purpose and whether they are publicly or privately owned. These include:

- Footpaths,
- Retail arcades,
- Covered walkways,
- Laneways,
- Parks and open spaces, and
- Right of ways.

Where opportunities present themselves to extend the pedestrian network these should be taken.

A long term objective is to provide an improved pedestrian link across the railway line. Providing better access across the railway line is an important step to improving visual and physical connections within the town centre.

The following upgrades to the existing pedestrian pathways are important:

- Redesigning Hillview Lane to reduce potential conflict between pedestrians and cars, including the provision of loading bays.
- Improving the existing links under the railway line to each side of the town centre and making the entries to the railway station more active public spaces.
- Upgrading the footpaths in the eastern side of the town centre including the commercial precinct and the residential precincts.
- Providing better facilities for pedestrians in the town centre (east). For example street furniture, street trees and links to publicly accessible open space.
- Improving links between the town centre and green spaces such as Eastwood Park and Glen Reserve. This includes improving visual links, signage, place marking and lighting to and from the green spaces.
- Increasing the number of pedestrian bridges across the storm water culvert in Glen Reserve (there is currently one).
- Widening of the footpath between Rowe Street (west) and Progress Avenue.

#### **b) Safety**

- All pathways, private or public, need sufficient width, lighting and finishes to meet public safety requirements. A minimum width of 3m is required for new pathways.
- Where appropriate pathways should be adjacent to active street fronts to improve amenity and safety.
- Public access should be available between at least 7am to 7pm.
- Where possible pathways should be located separately to vehicular traffic.

### 3.2.3 Destinations

The new pedestrian connections should link with specific destinations. A destination can include one or more of the following:

#### Open spaces

Future development in Eastwood should include new publicly accessible open spaces as appropriate on private and public property. This is particularly important for the eastern side of the town centre. On the western side opportunity exists to upgrade or extend the existing open spaces. Open spaces can take many forms including urban parks, communal squares, widened footpaths, atria, retail courtyards and arcades.

#### Active streets

Street activation occurs where land use and built forms combine to attract people to an area. As noted in Section 3.1 Eastwood is very successful in this regard. It is important for the community that this continues in the future with new developments. Building form can assist in street activation if the building is attractive and provides interest. At street level the shopfront should make pedestrians feel safe and welcome, and the public and private zones should be integrated. Buildings that have blank facades or conspicuous loading docks and service bays should be discouraged. Where these elements already exist public/private partnerships should be developed to mitigate the effects. For example, creation of a public art program to upgrade blank facades. Future development should retain the same façade/shop front modulation as the existing shops.

#### Views

The significant vistas through Rowe Street ending at Blaxland Road should be highlighted. The end points of the vista should be recognised with appropriate built forms. Views to Eastwood Park and Glen Reserve are encouraged. New developments should take advantage of these views. Public access in the form of lookouts, lobbies, balconies and active retail will be encouraged.

There is also a potential vista up Rutledge Street/ First Avenue towards Blaxland Road. This is currently a secondary pedestrian street when compared to Rowe Street but it is a major thoroughfare and an important public face to Eastwood.

#### Community facilities

These can be public institutions such as a school, library and community hall but equally these can be cafes, newsagents, medical practitioners, bakeries and so on that service the local community. Additional community facilities are encouraged, particularly on the eastern side of the town centre.

#### Landmark buildings

Landmark buildings are destination points. Landmark buildings include projects of architectural excellence, historic significance or nationally significant sustainable buildings. Refer to Section 3.2.4 for more information.



### **3.2.4 Semi-Public Elements**

#### **Built form**

The built form has a significant impact on the public domain. The DCP provides detailed built form controls. New developments need to address issues such as street activation, access to daylight and articulation of the façades. Occasional 'laneways' between and through the buildings to provide visual and physical links through the sites will be encouraged to break up the blocks. Semi-public areas such as arcades, courtyards and lobbies that address and are consistent with the public domain are also supported.

The building form should be developed to complement and enhance the public domain.

#### **Landmark Buildings**

Landmark buildings are buildings of excellence, and as such will attract interest from the wider community. They may include examples of outstanding urban design, architectural design, heritage significance or sustainability. They can be the catalyst for commercial growth and tourism within a centre.

Landmark buildings improve the urban environment for the community. They should provide premium sought-after spaces internally and a good commercial return for the owner, which flows on to the local community. The development of landmark buildings will be supported.

#### **Car parking**

The location and accessibility of public car spaces are critical to the success of Eastwood. Currently parking is a major issue in the town centre. The present numbers of public car spaces should be retained in any new development and access to car parks should be from secondary streets away from the main pedestrian routes. Car parking is to be planned to provide easy access to open space, shops, community facilities, etc but should not be immediately adjacent to or visible from these areas.

### 3.2.5 Street trees, Paving and Street Furniture

Improving the streetscape quality has a direct and immediate benefit for the public domain. Some opportunities for improving the streetscape are noted in Section 3.1. All new developments are required to incorporate streetscape improvements under the current DCP.

The scope and general requirements for street trees, paving and street furniture are noted below. Refer to Council's Public Works Department for detailed specifications for these items. The detailed specifications should be considered when preparing development application documents. It is important these elements are coordinated between the public and semi-public domains.

#### Street trees

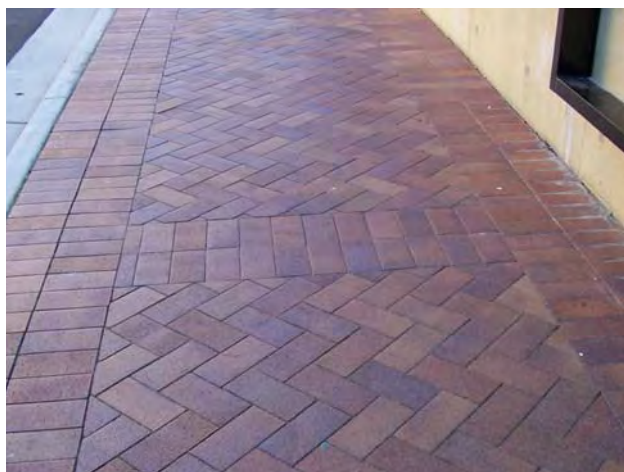
Refer to the Street Tree Master Plan (Figure 1) and Street Tree data sheets below for the species and location of street trees.

#### Paving

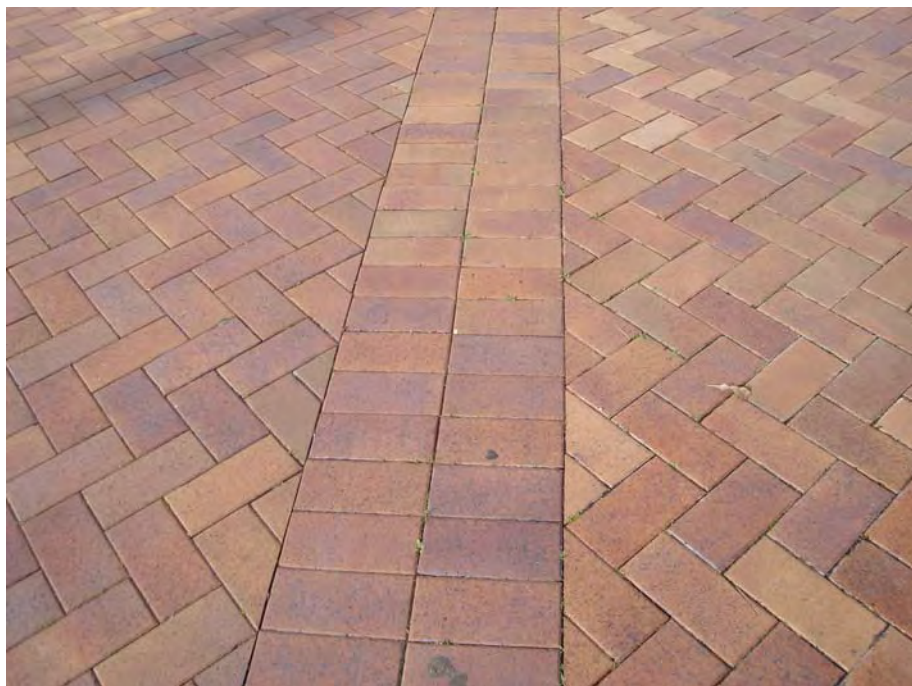
The entire town centre is to be paved with clay pavers in herringbone or stretcher bond pattern (as appropriate) with a double header course along both kerb line and building line. Refer to Council's Public Works Department for detailed specifications and see photos below:



Progress Avenue, Eastwood



Progress Avenue, Eastwood



Railway Parade, Eastwood (both photos)





### Street furniture

The following street furniture shall be provided where appropriate to Council's satisfaction for all new developments.

- Seating (Seat 01 and Bench 02)
- Bins (Bin 01)
- Bus shelters

Additional street furniture that may be considered for specific sites includes:

- Tables (Table and Stools 01)
- Bollards (Bollard 01)
- Tree guards and grates (Tree guard and grate 01)
- Bike parking (Bike Rack 02)
- Drinking fountains
- Café barriers
- Umbrellas (Umbrellas 01)
- Balustrading (Balustrade 01)
- Fencing (Fencing 01)

### Lighting

Multifunction poles (MFPs) are to be installed in Rowe Street (both sides of the station), Railway Pde/East Pde (First Ave to May Street), West Pde (Rutledge Street to Wingate Ave) and Trelawney Street (Rutledge Street to Rowe Street). The MFPs will incorporate lighting, street signage and banners into one element. See plan below.

Some parts of Eastwood such as Eastwood Plaza and Eastwood Village (on Progress Ave) have pedestrian lighting. The model is *LPB6 'City'* by Town and Park Furniture.

Power lines and other services shall be re-located underground. Refer to Figure 2 for the general extent of under grounding required. Refer to Public Works for a more detailed extent.



## Signage

Refer to Section 9.1 of the DCP for general signage.

Signage can enhance the prominence of the major entry points into the Eastwood town centre. The City of Ryde is implementing a program to provide 'gateway' signage in Eastwood. Precinct signage and local signage should be coordinated to improve the orientation and movement of pedestrians within the corridor. A Civic Signage Manual has been developed to create a consistently high standard of public signage in the Ryde Local Government area.

Smaller miscellaneous signage should be located on the multifunction poles where practical.

## Plaques

There are two existing plaques in the pavement at Eastwood Town Centre, both in Rowe Street Plaza. These commemorate the historical figures Gregory Blaxland and Maria Ann Smith ('Granny Smith').

Future plaques are to match the style shown **in the photograph below** and be installed as directed by Council along the circuit shown in the Plaque Location Plan below. The distance between plaques is to be a minimum of 8 metres, and Council's Public Works Department is to be contacted regarding the method of installation. Council's Urban Planning section is to make the final decision regarding the exact wording and location of each plaque along the circuit.







### KEY

---  
Circuit for location  
of plaques

• •  
Approx location of  
2 existing plaques



Eastwood Town Centre  
Plaque Location Plan

Not to scale  
May 2011



### Environmental performance

The environment performance can be enhanced by using the following when selecting and detailing materials:

- Recycled materials
- Functionally and aesthetically long lasting materials
- Materials that require minimal maintenance.

Water Sensitive Urban Design (WSUD) should be incorporated into the public domain design.

### 3.2.6 Public Art

Public art can make an important contribution to the local community in a number of ways. These include:

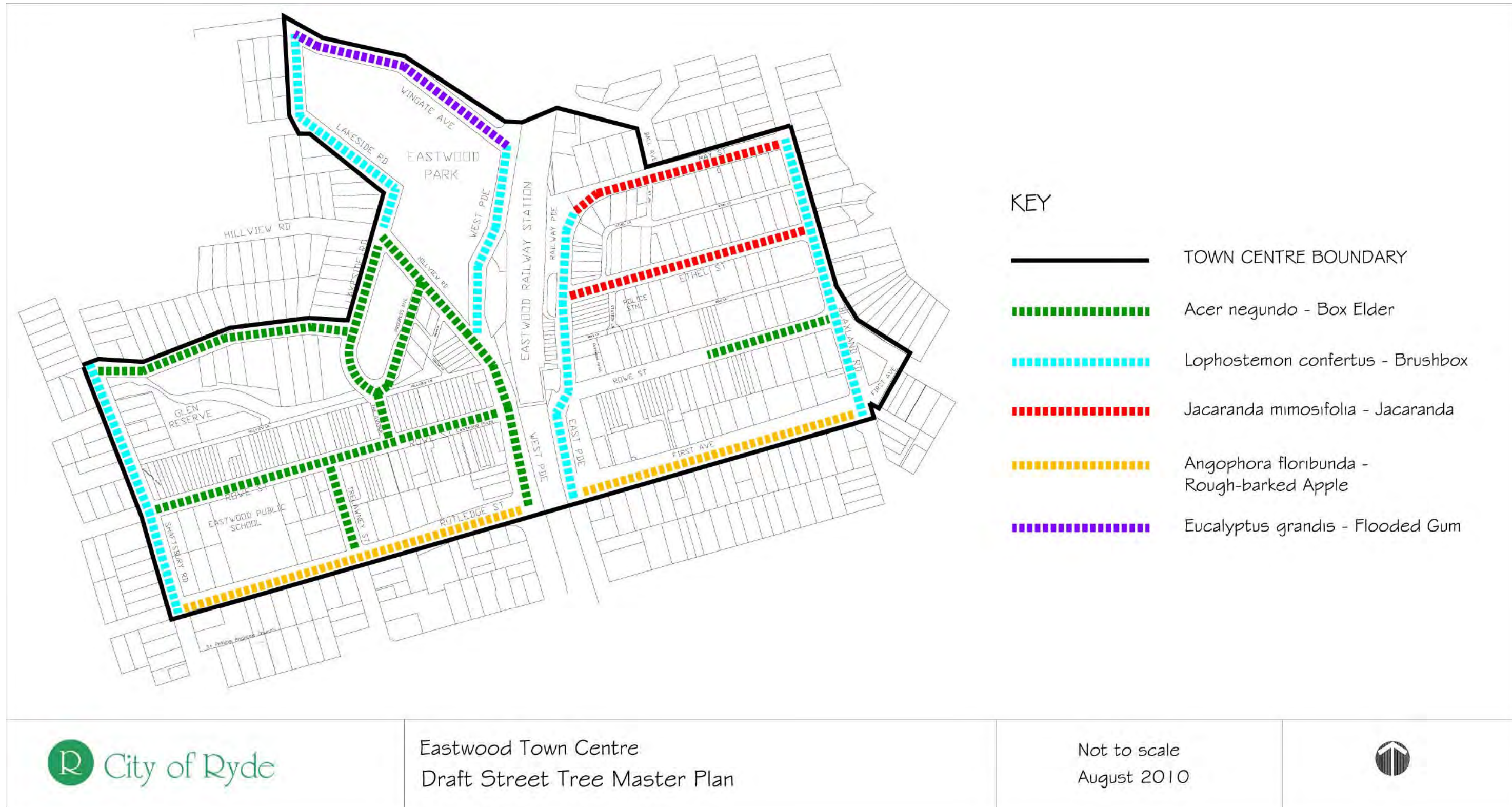
- Providing visual enjoyment and interest to everyday activities and social gatherings.
- Highlighting and enhancing the local character of an area, and making it a memorable place to visit.
- Marking the identity of key sites:
  - Gateways into the Eastwood town centre (eg Rowe Street, Glen Reserve, Lakeside Road)
  - Historic precincts
  - Open spaces / recreation areas
  - Public facilities
  - Landmark buildings
- Creating a point of reference for the local community and visitors.

Proposed art works should be site specific and respond to the distinctive character of the local area. For example the association with the Granny Smith apple is important for the local community. Artworks can be integrated with buildings and landscaping elements or be stand-alone objects.

Opportunities exist to include custom made, artist designed furniture that will celebrate specific aspects of Eastwood's character. Semi-public art work associated with individual buildings is encouraged. This can be in the form of art walls, screens or other building elements which provide exposure for the particular building but also interest for the community. If an overall Arts Plan for Eastwood is prepared, then art works can be gradually installed in appropriate places to enhance the public domain.



Figure 1





## EASTWOOD STREET TREES

### Data sheets



Acer in Hillview Road (summer)



Acer in Rowe Street (winter)

### *Acer negundo* – Box Elder

- Principle street tree of centre for many years – well established
- Favoured by councillors
- Deciduous - good shade in summer, sun in winter
- Autumn colour
- Tolerates frost, full sun, air pollution and flooding



Brushbox in Eastwood Park



Close up of Brushbox foliage

### *Lophostemon confertus* – Brushbox

- Australian native, evergreen
- Excellent shade tree
- Quick growing, establishing in 3 to 5 years
- Attractive – dark leaves, cream flowers, interesting bark
- Well established at Eastwood





Jacaranda in flower



Jacarandas in May Street

#### ***Jacaranda mimosifolia* – Jacaranda**

- Deciduous - good shade in summer, sun in winter
- Excellent shade tree
- Attractive purple flowers in Spring
- Already planted in May and Ethel Streets



Angophora floribunda



Close up of Angophora foliage

#### ***Angophora floribunda* – Rough-barked Apple**

- Indigenous, evergreen
- Broad-branching, open canopied - excellent shade tree
- Attracts native birds, quick growing, establishing in 3 to 5 years
- Long lived



Figure 2. Extent of under grounding of overhead services



## 4. West Ryde

### 4.1 Analysis of Existing Character

#### 4.1.1 Introduction to the West Ryde Town Centre

The West Ryde town centre is located in the City Of Ryde local government area approximately 14 km west of Sydney. The town centre is bisected east-west by Victoria Road and north-south by the Northern railway line. Victoria Road and the railway line effectively divide the existing town centre into quadrants.

The north-west quadrant contains the main commercial precinct. The north-east quadrant contains the historically significant Ryedale Road shops and more modern medium density housing. The south-east quadrant is an industrial precinct. The south-west quadrant contains larger commercial and semi-industrial lots along Victoria Road with older medium density housing behind.

Figure 1 shows the extent of the town centre for the purposes of the Public Domain Technical Manual.

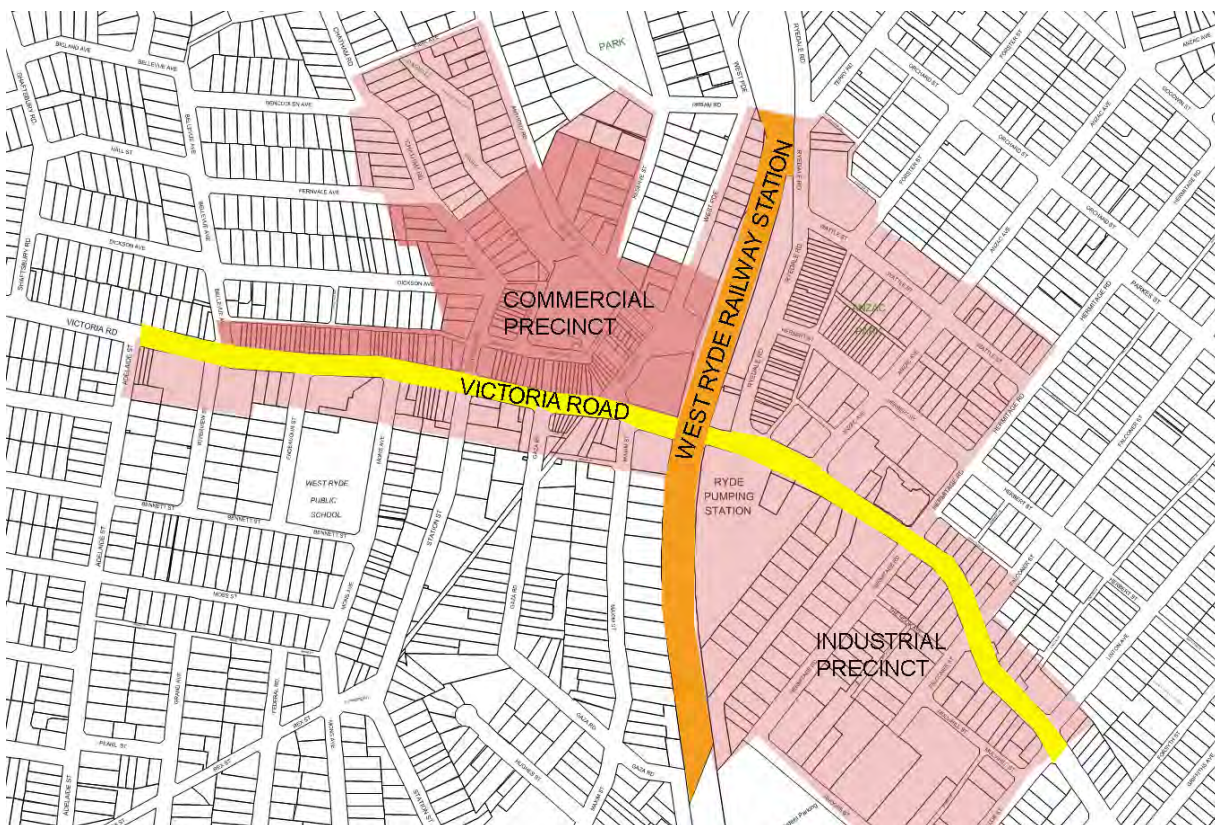


Figure 1 West Ryde Town Centre

### 4.1.2 Historical Overview of the West Ryde Town Centre

Originally the West Ryde area with its good rainfall and deep clay soils was occupied by Blue Gum High Forest (*Eucalyptus saligna*) from the ridgelines to the valley floors, and was home to the Wallumedde Aboriginal People. Early European settlers established farmland and orchards in the area. For much of the 19<sup>th</sup> century what is now the commercial centre of West Ryde was owned by the Darvall family. The Darvall property stretched from Shaftsbury Road to Ryedale Road and from Rowe Street to Victoria Road, a total of 360 acres (146 hectares).

A major catalyst for establishing the township of West Ryde was completion of the Northern Railway line in the late 1880s. West Ryde was also conveniently located on what would become Victoria Road. In the first quarter of the 20<sup>th</sup> century the Darvall property was progressively subdivided and residential dwellings overtook the farming acreage of the area. What became Ryedale Road shops was subdivided in 1888. Much of the village scale and built fabric developed in the first half of the 20<sup>th</sup> century has been retained in this street despite increased suburban development.

West Ryde's current commercial centre was transformed in the 1950s. In a period of three years more than 40 new shops were built, as well as six banks, two restaurants, a service station and a new post office. However with changes in shopping habits and the introduction of larger shopping malls (starting with the Ryde shopping centre), there was a gradual decline of West Ryde as a commercial centre.

The Ryde Pumping Station is a significant piece of infrastructure located in West Ryde. A pumping station was first established in 1892. By 1908 the station had switched to steam turbines, a first for Sydney and possibly for Australia. The present Ryde Pumping Station was built on adjacent land and opened in September 1921. A thriving industrial precinct has grown around the pumping station.

### 4.1.3 Existing Character

#### Built environment

##### North – West Commercial Precinct

The commercial/retail centre of West Ryde is located in the north-west quadrant. Historically the commercial centre faced Victoria Road. These buildings, which still dominate the commercial precinct, are generally 2 stories high and are characterised by narrow allotments along Victoria Road. Most properties have their shopfront facing onto Victoria Road.

The character of the built form reflects its function as a main street strip shopping precinct. Increasingly, growth of the commercial precinct has been negatively affected by the large volume of traffic on Victoria Road and changes in shopping habits away from this type of shopping environment.

Recently the commercial centre of West Ryde has started to move away from the 'strip shopping' model. A new West Ryde shopping centre based around the Woolworths supermarket and public car parking has developed. The laneways and streets behind Victoria Road have been upgraded with public domain improvements and traffic calming, making the

West Ryde precinct more accessible, convenient, visually pleasing and pedestrian friendly than before.

Further development is planned on the existing on-grade car park on Betts Street which will consolidate the town centre in this location away from Victoria Road. However with these positive changes to the West Ryde commercial precinct a challenge exists - to reinvigorate and maintain long-term an active streetscape along Victoria Road.

#### North – East Ryedale Road Precinct

East of the railway line is the historic Ryedale Road precinct which has strip shops with awnings dating from the early 1900s. It was once the main commercial strip. Today it is isolated from the main commercial precinct by the railway line and Victoria Road. Ryedale Road is located on a ridgeline and is the highest point in the town centre. Due to the location of the railway station it is not easily seen from the commercial precinct. In the heritage area the built form is generally 2 stories high and there is a consistency of materials and detail in these shops. This precinct also includes the local Leagues club which is outside but adjacent to the heritage zone. There is an approved development planned for the land adjacent to the railway line which will be up to 12 storeys high.

#### South – West Precinct

The south west precinct is dominated by a number of larger commercial sites along Victoria Road. A number of these businesses are servicing the car traffic using Victoria Road. The retail component is isolated from the main commercial centre by Victoria Road. There has been little renewal of the built environment to revitalise economic and social activity in this precinct.

#### South – East Precinct

The south east quadrant is predominantly a light industrial area. The built form is dominated by 1-2 storey factories and workshops which provide local amenity in terms of the variety of services offered by these businesses.

### **Destinations, Topography and Views**

The primary destination for visitors and residents in West Ryde are the retail facilities found in the town centre west of the railway line. These are popular for their diversity and convenience. Other important destinations in the West Ryde town centre include:

- The local schools and churches.
- The railway station and bus interchange.
- Community services such as the local library.
- The specialised services available in the industrial area.

The West Ryde town centre is located on a low point along Victoria Road. It is surrounded by a ridgeline to the north, east and west. South of Victoria Road the land falls away towards the Parramatta River. The town centre is located in a valley between these hills and in the past has been subject to flooding. The risk of major flooding has been alleviated to some extent through major infrastructure work. The commercial centre is still affected by an overland flow path.

The topography combined with the built form restricts district views from within the centre. However, the surrounding topography makes the town centre very visible from other areas in Ryde particularly buildings that are located on the ridgeline following the railway line. There are some district views east of the railway line.



## Open Spaces and Street Activation

Open spaces play an important role in creating a high quality public domain and providing opportunities for community interaction. They also provide space for recreation and relief from the urban city environment.

Street activation is often related to the success of the open space network. This has been facilitated in West Ryde through public domain improvements and encouraging development in streets and lane ways north of Victoria Road. There are a number of land uses in the town centre that encourage street activation including retail outlets, appropriate spaces for outdoor dining and community facilities. However the heavy traffic along Victoria Road does not encourage street activation, nor do the large areas of service zones and blank facades found on some of the newer buildings in West Ryde. In areas other than the commercial centre there are no compelling destinations that generate street activity.

The largest open space in West Ryde is Anzac Park, on the eastern side of the railway line. It has recently been upgraded and includes play equipment, barbecues, seats and paths. It is an old park with numerous mature trees and large areas of lawn. It is constantly used for passive recreation, mainly by residents of the surrounding apartments. The role of Anzac park as a breakout space for residents is likely to grow as the density of residential development increases.



Anzac Park

On the western side of the railway open space is dominated by a bus interchange. The interchange is an important piece of local transport infrastructure. It does not cater for other users. On the eastern side of the station much of what is currently open space or parking is approved for development.

In the commercial centre open space is limited. In Graf Ave and part of West Parade the pavement has been widened to allow more outdoor activities. The Marketplace development provides a small outdoor space at the mall entrance. There is demand for more of these spaces throughout the commercial areas either side of the railway.



West Ryde Marketplace forecourt

Overall the town centre has an absence of larger spaces that would allow for specific events such as markets and community activities. A new plaza is proposed in association with the development of the existing car park on Anthony Road which may provide a facility for some of these activities.

A number of green spaces are found just beyond the edges of the commercial precinct. They include Miriam Park, Darvall Park (the southern section with the model railway) and further north the local bowling club. Currently the green spaces are not integrated with the town centre.

There is little publicly accessible open space south of Victoria Road. This limits potential social and commercial interaction on this side of the town centre.

### **Paving types**

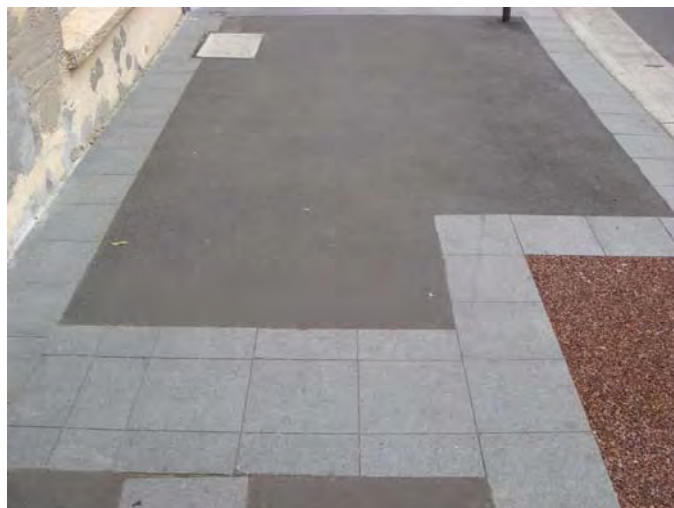
The quality of paving in West Ryde varies considerably. There are sections of good quality paving in the laneways north of Victoria Road mixed with areas of poor quality paving. In the commercial precinct the footpaths in Graf Ave and parts of Chatham Road have been recently renovated and set a new benchmark for paving in West Ryde. The paving in the other precincts is generally of a lesser quality. The key paving types are as follows:



- granite paving,
- asphalt with granite banding,
- plain asphalt,
- various types of concrete and clay pavers,
- concrete pavement.



Ryedale Road – clay pavers



Asphalt with granite banding

Visually the quality of the public domain is only as good as its weakest part. In West Ryde the lack of a unified paving makes sections of the streetscape appear untidy and dated.

### Street Tree Network and Street Furniture

Street trees play an important role in creating a high quality public domain. Tree lined streets are attractive, provide shade and can visually soften hard urban spaces. The West Ryde master plan proposes extensive tree planting to encourage green links into the town centre.

Generally West Ryde has a patchy, discontinuous street tree network and there is no strong boundary planting to delineate the centre. In contrast, new street trees at regular intervals have been planted in Graf Ave, Anthony Road and Market Street, in coordination with the redevelopment of the mall and library. This highlights the lack of street trees elsewhere in the centre, which is partly due to physical constraints. Victoria Road is without trees at all because narrow footpaths, awnings and overhead powerlines prevent it. In other areas such as the Ryedale Road conservation area tree planting is limited by the historic awnings and the location of services. Around the train station tree planting is very restricted because of the extensive parking and the bus interchange.





Graf Ave



Graf Ave

The same constraints do not apply to the remainder of the commercial precinct or residential areas. There is potential to restore or upgrade the street tree network in these precincts.

The availability and appearance of street furniture is mixed. East of the railway line and south of Victoria Road street furniture is limited with the exception of Anzac park where the quality and availability of street furniture is excellent. In the commercial precinct street furniture has been updated and is found at regular intervals in Graf Ave, Anthony Road and Market Street. However this is not matched in the surrounding streets.

### **Pedestrian Network, Safety and Amenity**

A good pedestrian network can improve retail activity and encourage visitors to an area in a number of ways:

- Increases interaction with open spaces (public and private)
- Creates an attractive and vibrant shopping precinct
- Traffic calming: high pedestrian traffic can act as a traffic calming device
- Increased safety: high levels of pedestrian activity and associated street activity improve personal and property security.

The pedestrian network in West Ryde has improved in recent years with new development and public domain upgrades in the commercial precinct. Pedestrian activity is focused on Graf Ave, Chatham Road, Anthony Road and Market Street. The pedestrian traffic is serviced by the public car parks off Anthony Road and at Woolworths. The West Ryde railway station and the associated bus interchange on West Parade are also major sources of pedestrian traffic. The east side of the railway line has fewer attractions and correspondingly less pedestrian activity but is busy close to the railway station and around the Leagues club.

There are a number of instances where the safety and amenity of pedestrians could be improved:

- Victoria Road divides the town north–south. Visual and physical pedestrian links across Victoria Road are poor.
- Pedestrian amenity either side of Victoria Road is poor. A large part of this is due to the noise and pollution associated with traffic using Victoria Road. The quality of the public domain also has an impact.
- The railway line divides the town centre in an east-west direction. Both the visual and physical connection between the two sides of the railway is poor.
- The bus interchange and railway entry on West Parade create pedestrian congestion at peak times. The pedestrian pathways and links to the town centre are not well defined. There is a potential conflict between pedestrians and buses.
- Pedestrian amenity could be improved on some of the smaller laneways and arcades in the commercial precinct.

### **Social and Cultural Resources**

- West Ryde Town Centre provides a social hub for the local community. Existing and proposed community facilities include a public library, aged care facilities, child care facilities and meeting rooms.
- There is currently no public art in the West Ryde centre.

## 4.2 Urban Design Concepts

### 4.2.1 Vision and Structure Plan

This section presents design concepts and strategies for the improvement of the public domain. The secondary purpose of this section is to provide guidance to developers on how new developments should respond to public domain requirements.

#### Proposals

A summary of the key opportunities for improving the public domain in West Ryde are:

1. Upgrading of the pedestrian connections to destinations around West Ryde
2. Creating publicly accessible open space in the town centre
3. Improvement of the streetscape (landscaping, furniture, and artwork) in the town centre and secondary streets and laneways.

### 4.2.2 Pedestrian Network, Safety and Amenity

There are a number of opportunities to extend the pedestrian network and improve the pedestrian amenity in the West Ryde town centre.

#### a) Create new pedestrian pathways and upgrade existing pathways

New and upgraded pedestrian pathways will give local pedestrians alternative access to shops and facilities. The pedestrian connections can take many forms depending on their purpose and whether they are publicly or privately owned. These include:

- Footpaths,
- Retail arcades,
- Covered walkways,
- Laneways,
- Parks and open spaces &
- Right of ways.

Where possible opportunities present themselves to extend the pedestrian network these should be taken.

A long term objective is to provide a second pedestrian link across the railway line. Providing better access across the railway line is an important step to improving visual and physical connections within the Town Centre.

The following upgrades to the existing pedestrian pathways are a priority:

- Improving the connection between the north and south sides of Victoria Road as identified in the West Ryde Master Plan.



- Improving the links across the railway line to each side of the town centre and making the entries to the railway station function as active public spaces.
- Providing better facilities for pedestrians in the town centre. For example improved street furniture, street trees and links to publicly accessible open space.
- Improving links between the town centre and green spaces such as Miriam Park. This includes improving visual links, signage, place marking and lighting to and from the green spaces.
- Widening of the footpaths where this can contribute to better pedestrian access and more active streets.
- Encouraging more privately owned connections from Victoria Road to the streets behind.

#### **b) Safety**

- All pathways, private or public, need sufficient width, lighting and finishes to meet public safety requirements. A minimum width of 3m is required for new pathways.
- Where appropriate pathways should be adjacent to active street fronts to improve amenity and safety.
- Public access should be available between at least 7am to 7pm.
- Where possible pathways should be located separately to vehicular traffic.



Miriam Park

### 4.2.3 Destinations

The new pedestrian connections should link with specific destinations. A destination can include one or more of the following:

#### Open spaces

Future development in West Ryde should include new publicly accessible open spaces as appropriate on private and public property. Open spaces can take many forms including urban parks, communal squares, widened footpaths, atria, retail courtyards and arcades.

A new public plaza is being built at the intersection of Betts Street and Anthony Road as part of a commercial development.

There is an opportunity to include the bus interchange and develop a more attractive and functional public space linking the railway station to the town centre.

The West Ryde master plan also proposes the following:

- A pocket park on the southern side of Victoria Road opposite West Parade.
- Improved linkages from the parks and reserves surrounding the area into the town centre.

#### Active Streets

Street activation occurs where land use and build forms combine to attract people to an area. As noted in Section 4.1 West Ryde has started on this path with the recent Marketplace development. It is important for the community that this continues in the future with new developments. Building form can assist in street activation if the building is attractive and provides visual interest. At street level the shopfront should make pedestrians feel safe and welcome. The public and private zones should be integrated. Buildings that have blank facades or conspicuous unscreened loading docks and service bays should be discouraged. Where these elements already exist public/private partnerships should be developed to mitigate the effects. For example, establishing a public art program to upgrade blank facades.

#### Views

The potential for significant vistas through the commercial precinct is limited by the topography, street network and rail infrastructure. The views are more internalised in the West Ryde town centre than other centres in Ryde. This makes it more important that significant corner sites are designed with the view in mind, and that public domain upgrades and extensions are similarly prioritised to make the most of these significant locations.

To the north east of the railway line there are possible district views to the east. There is also a potential to create vistas along Herbert and Wattle Streets, linking into Anzac Park.





View east down Herbert Street

### **Community facilities**

Community facilities form important destinations in a town centre. These can be public institutions such as schools, libraries and community halls but equally these can be cafes, newsagents, medical practitioners, bakeries and so on that service the local community. A new community building owned by the City of Ryde is currently under construction in Anthony Road. It is likely that this will become a significant destination for a number of residents.

### **Landmark buildings**

Landmark buildings are destination points. Landmark buildings include projects of architectural excellence, historic significance or nationally significant sustainable buildings. Refer to Section 4.2.4 for more information.



#### 4.2.4 Semi-Public Elements

##### Built form

The built form has a significant impact on the public domain. The Development Control Plan (DCP) controls the built form. New developments need to address issues such as street activation, access to daylight and articulation of the façades. Occasional 'laneways' between and through the buildings to provide visual and physical links through the sites will be encouraged to break up the blocks. Semi-public areas such as arcades, courtyards and lobbies that address and are consistent with the public domain are also supported.

The building form should be developed to complement and enhance the public domain.

##### Landmark buildings

Landmark buildings are buildings of excellence, and as such will attract interest from the wider community. They may include examples of outstanding urban design, architectural design, heritage significance or sustainability. They can be the catalysis for commercial growth and tourism within a centre.

Landmark buildings improve the urban environment for the community. They should provide premium sought-after spaces internally and a good commercial return for the owner, which flows on to the local community. The development of landmark buildings will be supported.

##### Car parking

The location and accessibility of publicly available car spaces is important to improving the amenity of the public domain of West Ryde.

- The current numbers of public car spaces are to be retained in any new developments.
- Where possible, access to car parks should be off secondary streets and not from Victoria Road (this is an RTA requirement).

In addition, the entries to car parks should be located to minimise the impact on pedestrian networks. Car parking should be located to provide easy access to new open spaces, shops, community facilities, etc but should not be immediately adjacent or visible from these areas.

#### 4.2.5 Street trees, Paving and Street Furniture

Improving the streetscape quality has a direct and immediate benefit for the public domain. Section 4.1 noted there are opportunities for improving the streetscape in the town centre. The West Ryde master plan shows:

- Additional tree planting to develop green links into and through the town centre.
- Public domain upgrades in the commercial centre.
- Integration of public and private open space.

The scope and general requirements for street trees, paving and street furniture are outlined below. All new developments are required to incorporate streetscape improvements where noted.

Refer to Council's Public Works Department for detailed specifications for these items. The detailed specifications should be considered when preparing development application documents. It is important that these elements are coordinated between the public and semi-public domains.



Street furniture Rouse Hill Town Centre



Street furniture Kissing Point Park

### **Street trees**

Refer to the Street Tree Master Plan and Street Tree data sheets below for the location and species of future street trees.





Street Tree Master Plan



## WEST RYDE STREET TREES data sheets



Plane tree in Chatham Rd (winter)



Plane tree in Chatham Rd (summer)

### ***Platanus digitata* – Saw-edged Plane Tree**

- Good shade tree
- Attractive cream coloured bark that is very striking in winter
- Fast growing
- Deciduous - good shade in summer, sun in winter



Crepe Myrtle in Wattle Street (winter)



Crepe Myrtle in flower (summer)

### ***Lagerstroemia indica* – Crepe Myrtle**

- Excellent street tree under wires
- Hardy
- Very attractive – profuse flowers
- Fast growing

### ***Lophostemon confertus* - Brushbox**

- Australian native, evergreen
- Excellent shade tree
- Quick growing, establishing in 3 to 5 years
- Attractive – dark leaves, cream flowers, interesting bark
- Well established at West Ryde

Brushbox in Anthony Road



Pyrus in Graf Ave (winter)



Pyrus in Graf Ave (summer)

### ***Pyrus ussuriensis* – Manchurian Pear**

- Attractive - dark green leaves turning bronze in winter, scented white flowers in spring
- Deciduous - good shade in summer, sun in winter
- Excellent shade tree

### ***Ulmus parvifolia* – Chinese Elm**

- Deciduous - good shade in summer, sun in winter
- Long lived
- Excellent shade tree
- Pollution resistant

Chinese Elm





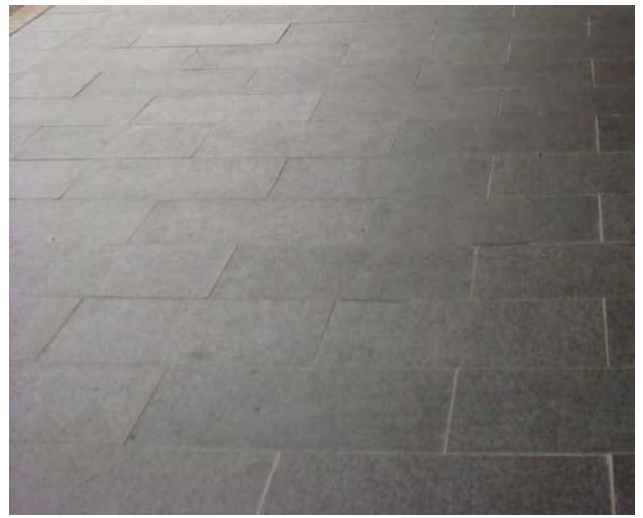
## Paving

The pavement of the commercial centre has been unified by the installation of plain dark grey granite paving, with a black and grey pattern as a feature at intersections and other locations. All new upgrades must continue this pavement treatment.

The asphalt with granite banding will be limited to that already existing, as it does not have the longevity of full granite paving. A uniform pavement will help link the east and west sides of the railway line.



Feature paving to mark corners



Grey granite paving

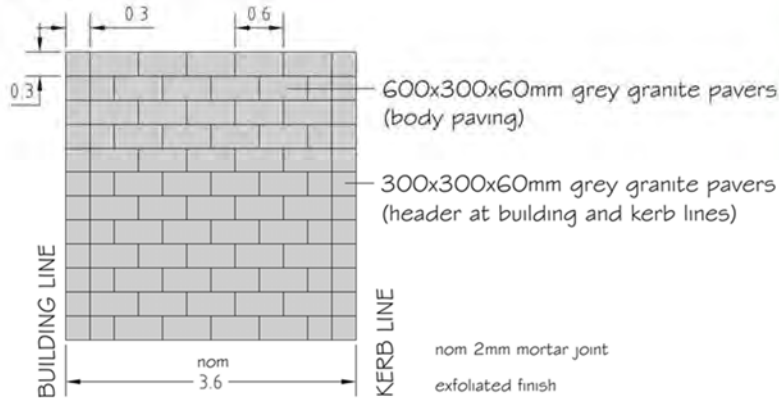
Refer to the West Ryde Paving Layout Plan below for details of the paver sizes and paver layout required.

Refer to the West Ryde Paving Plan below for details of the extent and location of the paving types required.



## Paving West Ryde

### Main paving

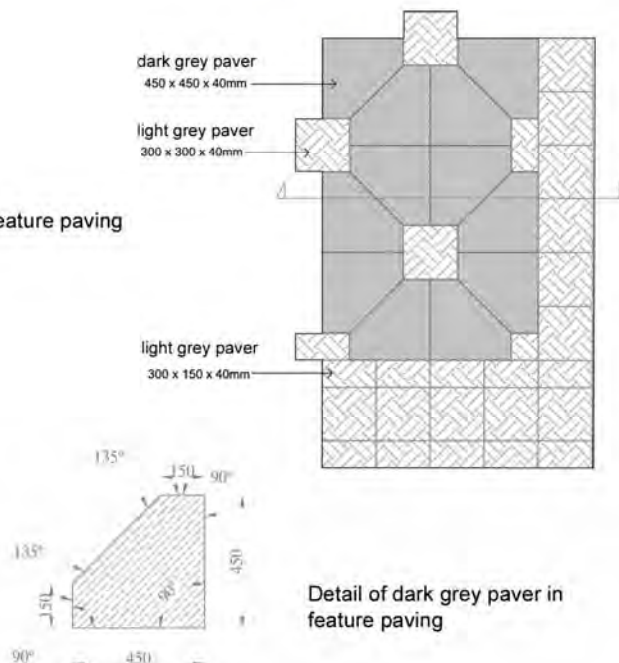


### Feature paving (see West Ryde Paving Plan for locations)

#### Feature paving at Anthony Road



### Detail of feature paving



Detail of dark grey paver in feature paving

WEST RYDE

### PAVERS:

Dark grey granite  
Light grey granite  
Exfoliated finish

Sizes as shown

Nom 2mm paving joint

### PAVING PATTERN:

#### Main paving

Stretcher bond with header course along kerb line and building line

#### Feature paving

Pattern as shown

### INSTALLATION:

Pavers to be laid on 30mm wet sand cement mix, on 100mm 25MPa concrete slab, on 50-75mm compacted roadbase.

### PAVER SEAL

Protective Sealant for Paving Stones and Concrete by Techni-Seal or approved equivalent.

### Granite paver suppliers

The Sydney Brick Paving Company: 'Mosaïque Black'

Sam the Paving Man:

'Raven Black' and

'Sesame Grey'

Morgan Stone:

'Raven Black'

or approved equivalent.

## West Ryde Paving Layout Plan



## Street furniture

The following street furniture shall be provided to Council's satisfaction for all new developments.

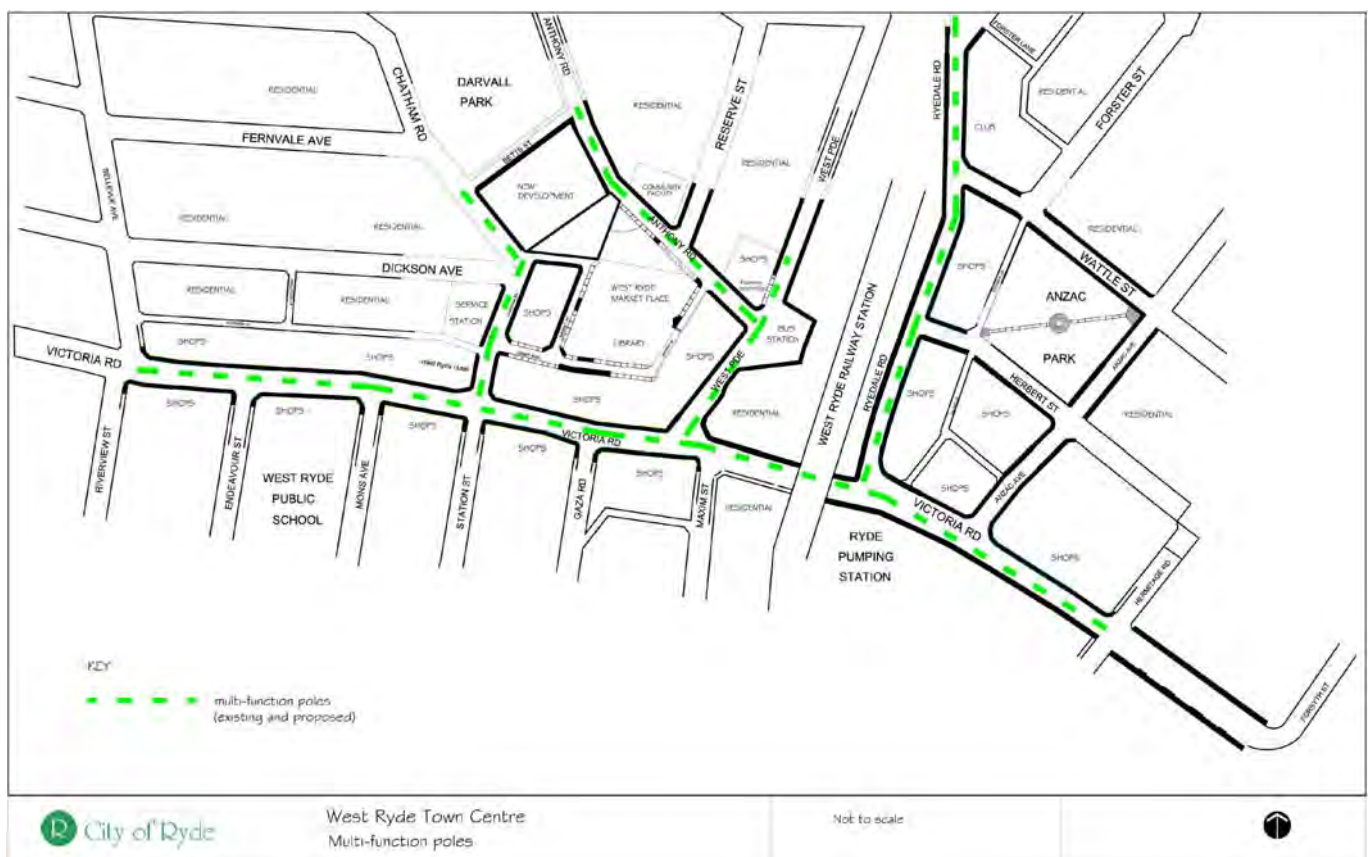
- Seating (Seat 01 and Bench 02)
- Bins (Bin 01)
- Bus shelters

Additional street furniture that may be considered for specific sites includes:

- Bollards (Bollard 02)
- Trellis (Trellis 01)
- Bike racks (Bike Rack 01)
- Picnic tables
- Pedestrian lights
- Bubblers
- Café barriers

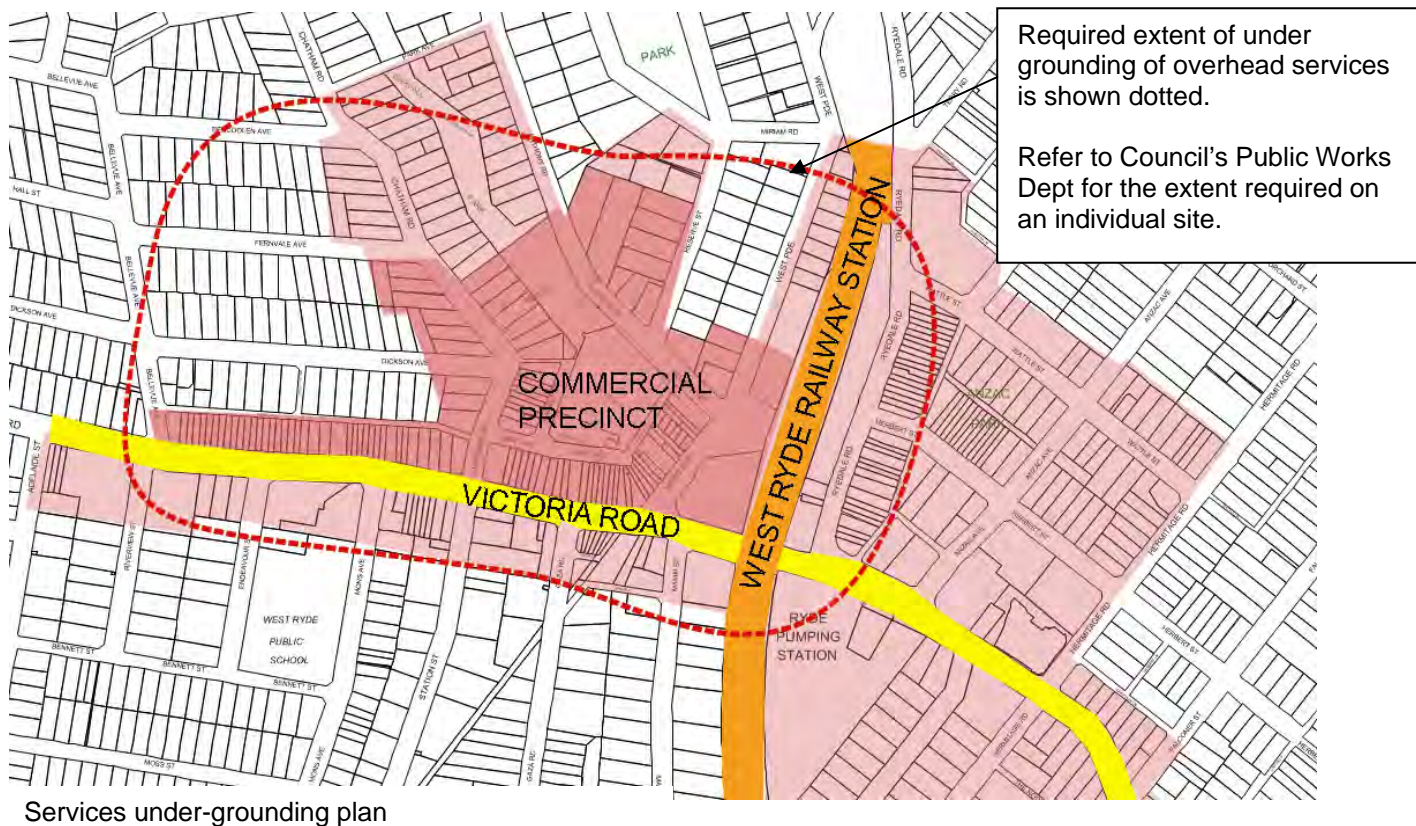
## Lighting

Multifunction poles are to be installed in Victoria Road (Hermitage Road to Bellevue Ave), Ryedale Road (Victoria Road to Forster Lane), West Pde (Victoria Road to the Koorong bookshop), Anthony Road (West Pde to Betts Street), Chatham Road (Victoria Road to Betts St). These will incorporate lighting, street signage, and banners into one element. See below:





Power lines and other services shall be re-located underground. Refer to the plan below for the general extent of under-grounding required. Refer to Council's Public Works Department for a more detailed extent.



## Signage

Refer to Section 9.1 of the DCP for general signage.

Signage can enhance the prominence of the major entry points into the West Ryde town centre. The City of Ryde is implementing a program to provide 'gateway' signage in West Ryde. Precinct signage and local signage should be coordinated to improve the orientation and movement of pedestrians within the corridor. A Civic Signage Manual has been developed to create a consistently high standard of public signage in the Ryde Local Government Area.

Smaller miscellaneous signage should be located on the multifunction poles where practical.

## Environmental performance

Environment performance can be enhanced by using the following when selecting and detailing materials:

- Recycled materials
- Functionally and aesthetically long lasting materials
- Materials that require minimal maintenance.

Water Sensitive Urban Design (WSUD) should be incorporated into the public domain design.

#### **4.2.6 Public Art**

Public art can make an important contribution to the local community in a number of ways. These include:

- Providing visual enjoyment and interest to everyday activities and social gatherings.
- Highlighting and enhancing the local character of an area, making it a memorable place to visit and return to.
- Marking the identity of key sites:
  - Gateways into the West Ryde town centre (eg Anthony Road, Chatham Road, West Parade, Victoria Road and Ryedale Road)
  - Historic precincts
  - Open spaces / recreation areas
  - Public facilities
  - Landmark buildings
- Creating a point of reference for the local community and visitors.

Proposed art works should be site specific and respond to the distinctive character of the local area. Artworks can be integrated with buildings and landscaping elements or be stand alone objects.

Opportunities exist to include custom made, artist designed furniture that will celebrate specific aspects of West Ryde's character. Semi-public art work associated with individual buildings is encouraged. This can be in the form of art walls, screens or other building elements which provide exposure for the particular building but also interest for the community. When an overall Arts Plan for West Ryde is prepared, then art works can be added gradually to enhance the public domain.





Public Art Rouse Hill Town Centre



Public art Camberwell Markets

#### 4.2.7 Cycleways

Residents have highlighted the need for better facilities for cyclists in the local area. The Ryde Bicycle Strategy and Master Plan (2007) shows the proposed regional bike network linking West Ryde to surrounding suburbs. The bicycle master plan also shows there are opportunities to provide local bike routes. The completion of the proposed cycle network through the town centre is a long term project and should be considered when preparing development application documentation.

Additionally, facilities for cyclists such as convenient bike parking and change facilities are to be encouraged in the town centre. The lack of open public space in the West Ryde commercial centre limits these opportunities at present. The development of the key sites and other large projects should provide the impetus for improving facilities for cyclists.



## 5. Meadowbank

### 5.1 Analysis of Existing Character

#### 5.1.1 Introduction to the Meadowbank Town Centre

Meadowbank is located in the City Of Ryde local government area approximately 14 km west of Sydney. It is strategically located near several of Sydney's centres of economic activity. It lies midway between the Sydney CBD and Parramatta, and close to Sydney Olympic Park and the Rhodes commercial precinct. The area is a gateway into Ryde.

This public domain plan applies to the Meadowbank town centre as shown in Figure 1. It extends from Meadow Crescent west of the Northern Rail Line to Church Street on the east. It is bounded by Constitution Avenue and Junction Street to the north and fronts the Parramatta River to the south.

Meadowbank has a unique and evolving character when compared to the other town centres in the City of Ryde. It is undergoing a fundamental change from an industrial area to an urbanised, predominantly residential area. However, it is not a dormitory suburb. Meadowbank is developing the commercial, retail and public facilities that an active urban residential suburb requires. The changes to the built form and the public domain are occurring rapidly. However, it retains much of its old character and remains an employment area.

Meadowbank is relatively unaffected by the major roads and associated traffic that characterise the other town centres in the City Of Ryde. However, the original small retail centre on Constitution Road is divided by the Northern railway line. Most of the changes in Meadowbank are occurring on the eastern side of the railway in the Meadowbank Employment Area (MEA).

The Public Domain Manual complements the Ryde LEP120 and the DCP Part 4.2 – Meadowbank Employment Area and it should be read in conjunction with these planning instruments.

#### 5.1.2 Historical Overview of the Meadowbank Town Centre

Land originally granted to Surgeon William Balmain in 1794, in the district of the Field of Mars, was given the name 'Meadow Bank'. By 1819 both the 'Meadow Bank Estate' and 'Chatham Farm' to the north, belonged to John Bennett. In 1823 he was joined by his nephew William Bennett. John Bennett died in July 1829, a bachelor, and his nephew inherited his estate, building Meadowbank House around 1835. The opening of the railway from Strathfield to Hornsby in 1886 provided impetus for development in the area. The estate was subdivided in the late 1880's either side of the railway line.

East of the railway line Meadowbank developed into an important industrial and manufacturing centre for the Sydney region because of its good rail, road and harbour links. This started with the Meadowbank Manufacturing Company in 1890. The 1950's began a renewal of Meadowbank as a manufacturing precinct with companies such as the Hoover establishing factories in the area.

The latter part of the 20<sup>th</sup> century saw a decline of the heavy industrial and manufacturing businesses. This was caused by several factors including increases in land values, competition from other areas, a loss of transport amenity and a general reduction in local manufacturing.

West of the railway line remained residential and in the 1950s and 1960s developed into a medium density housing precinct. These buildings remain largely intact.

A commercial/ retail centre developed on Constitution Road either side of the railway station. This remained a small local centre due to the proximity of the nearby West Ryde and later Top Ryde shopping centres.



Figure 1. Extent of Public Domain Plan

### 5.1.3 Existing Character

#### Built environment

The Meadowbank area still contains a large number of post world war two industrial buildings that have dominated and contributed to so much of the character of the area. Many of these are vacant or underutilised. The area is characterised by large footprint low rise industrial buildings with an infill of smaller footprint buildings housing smaller light industrial businesses. The oldest building on the site is the former Hoover Building, a handsome Deco style low rise rendered building. There is no consistency in style or quality throughout Meadowbank.

Medium density residential buildings now feature significantly near the foreshore. These residential areas include retail and commercial components and there is now an active local shopping centre at Bay Drive. The built forms vary in height from four to seven storeys, stepping down to the water. The buildings are similar in style, characterized by large balconies with a mixture of face brick and render. While the form of the buildings are broken up when viewed from the north and south (largely as a result of view sharing to the water), the buildings appear 'long' when seen from the east or west.

Outside the Employment area the Meadowbank Campus of the Northern Sydney Institute of TAFE is a substantial contributor to the character of the local area. The location of the TAFE adjacent to the railway station makes it accessible to public transport. West of the railway, the residential area consist mainly of 3 storey walk-ups dating from the 1960's.

There has recently been significant upgrade of the public domain in Meadowbank particularly around the foreshore. This has resulted in a series of high quality public spaces that have made the foreshore more accessible to the local community. These spaces are linked to existing parks west of the railway.

#### Destinations, Topography and Views

The primary destinations for visitors to Meadowbank are the employment areas and the TAFE College. The open spaces around the foreshore are increasingly becoming destinations for visitors and residents alike. Other important destinations include:

- The retail facilities and restaurants at Bay Drive.
- The railway station
- The ferry wharf
- The shops on Constitution Road

The topography of Meadowbank is undulating but generally falls from a high point at the ridgeline near the railway station down to the river edge. This ridge is an important element of the Meadowbank town centre.

There are a number of areas where steep gradients occur. This includes Constitution Road that falls to a central low point east of Bowden Street. There are also a number of significant gradient changes caused by the levelling of large sites to accommodate the existing industrial buildings.



The topography combined with the built form restricts district views out to other areas in Ryde. However, there are significant views across Parramatta River along the foreshore. Additionally a number of streets have vistas down to the water including Bowden Street and Belmore Street.

There are a number of minor vistas through some of the newer residential developments and there is also potential to create views into the recently created open spaces around the foreshore and at the original Constitution Road shops.

### **Open Spaces and Street Activation**

Open spaces play an important role in creating a high quality public domain and providing opportunities for community interaction. Street activation is often related to the success of the open space network and this is now being encouraged in Meadowbank. The extent of street activation is mixed; recent developments have improved street activation with sympathetic building design, on-street retail outlets, and spaces for outdoor dining. This contrasts with streets where the older industrial buildings dominate. These were generally designed to discourage public/private interaction and pedestrian activity. In addition, several of these sites are now vacant. The steep gradient of some streets such as Constitution Road and Belmore Street makes it difficult for buildings to interact with the street at grade.

The major open spaces include the existing waterfront and parks developed over the past few years. There are also linkages to waterfront parks to the east and west of the site. Waterfront park lands and the significant stands of figs are important open space and landscape features of this area. These larger green spaces provide opportunity for events such as markets and festivals in the town centre.

The other opportunity for open space is the existing street network. There are current projects underway to create new urban open spaces along the street network. The first stage includes a public plaza at the eastern railway station entrance. This plaza is complete and includes terraces for outdoor dining.

Meadowbank lacks a variety of small open urban spaces within the town centre, particularly on the western side of the railway. The urban spaces that do exist are currently well used and there is demand for more urban open spaces throughout the town centre.



Waterfront pedestrian and bike path



Ryde Wharf Park

## **Paving types**

The quality of paving in Meadowbank varies considerably. For example in the new parks and around the recent residential buildings the paving quality is good. In other areas there is only a grass verge. The key paving types are as follows:

- Brick paving predominates around the shops on the west side of the railway line.
- Grey granite predominates around the shops on the east side of the railway line.
- Concrete, concrete pathway within a nature strip, or grass verge in the intact industrial areas.
- Asphalt and clay paver banding.
- Sandstone in special areas and riverside parks.

## **Street Tree Network and Street Furniture**

Street trees play an important role in creating a high quality public domain. Tree lined streets are attractive and can visually soften hard urban spaces. In the older area of the Meadowbank town centre there are few street trees. The recently developed residential areas have the basis of an effective urban street tree network. Both the established and new foreshore parks have a good network of trees. West of the railway there are a number of mature trees, however more consistent planting could strengthen the streetscape throughout the town centre.

Overall the street tree network is incomplete and there is no strong boundary planting to delineate the centre.

The availability and appearance of street furniture is mixed. Again the streets and parks closest to the foreshore are well serviced by street furniture. In other areas east of the railway line street furniture is limited. The western side of the railway is better serviced around the local shops.





Tuckeroo street trees in Bowden Street

### **Pedestrian Network, Safety and Amenity**

In the past, Meadowbank has not had a strong pedestrian network. However, recent and planned improvements to the public domain have enhanced pedestrian amenity and provided a new network along the foreshore.

Pedestrian activity is focused on the station entries and along Railway Road, which links the station to the new development on Bay Drive. There is also significant activity along the foreshore. The pedestrian traffic is serviced by the public car parks in Bay Drive shopping centre and along the foreshore. The west side of the railway line has fewer attractions and correspondingly less pedestrian activity.

In Meadowbank the pedestrian activity has improved community interaction with open spaces (public and private). High levels of pedestrian activity and associated street activity improve personal and property security.

However there are a number of instances where the safety and amenity of pedestrians could be improved:

- The pedestrian connection between the two sides of the railway is poor.
- There are potential pedestrian/traffic conflicts along either side of the railway station along Constitution Road.

-

- The pedestrian network away from the station and foreshore could be better defined and upgraded to match the recent improvements along Parramatta River.

### **Social and Cultural Resources**

- The new community facilities in the Meadowbank Town Centre provide a social hub for the local community. These facilities include a supermarket and associated shops, restaurants, commercial offices and a Council owned community centre.
- There are currently a number of examples of public art in the Meadowbank town centre associated with the foreshore parks.

#### **5.1.4 Cycleways**

The Ryde Bicycle Strategy and Masterplan (2007) shows the proposed regional and local bike network within Meadowbank and surrounding suburbs. Facilities for cyclists such as convenient bike parking and change facilities are encouraged in the town centre.

Recreational bike paths have been created along the foreshore and linking with the regional routes. However, more facilities are required to meet demand for parking and refreshment / change areas.

The Ryde Riverwalk Masterplan Report (2007) details bike paths, pedestrian paths, planting, furniture and signage for the entire foreshore. This is being implemented in stages.

## 5.2 Urban Design Concepts

### 5.2.1 Vision and Structure Plan

This section presents design concepts and strategies for the improvement of the public domain. The secondary purpose of this section is to provide guidance to developers on how new developments should respond to public domain requirements.

#### Goals

Public spaces in the Meadowbank town centre will meet the needs of the community while creating an attractive, robust and durable character for the area. Community needs will be met by a range of public spaces that will support passive and active recreation. These will include small, intimate pocket parks, widened footpaths for outdoor dining, pedestrian through-site links, and large foreshore parks. Connectivity and permeability will be improved.

#### Objectives

- Provide a comfortable, people orientated place
- Enhance the existing fabric
- Provide a sense of place which draws on the character of the area
- Enhance the foreshore character of the area and appropriately link in with the ongoing Shepherds Bay foreshore improvements
- Provide a sense of renewal and excitement drawing on recent development in the area
- Give consideration to the access and recreation needs of local residents, daily employment workforce, tourists, commuters and cyclists
- Be realistic and affordable in relation to costs, ongoing maintenance and local conditions

#### Visual Quality Issues

- Developing an identity for the Meadowbank town centre
- Maintaining and enhancing water views
- Provision of unique design themes for public spaces
- Providing consistent design and quality of streetscape elements, e.g. street furniture, lighting, street tree plantings
- Selection of a standard colour palette to provide harmony/drama

#### Materials

The selection of materials and furnishings for the Meadowbank town centre is consistent with recent upgrades to the public domain, guiding the future development of the area as well as existing site and environmental conditions.

Central to the development of the future character of the area are the MEA Master Plan and MEA Development Control Plan (DCP). These documents collectively encourage a mixed use of development with an emphasis on high quality design, environmental sustainability and a desire to create a vibrant and desirable locality. Permeating this vision is a desire to enhance local environment and character, typified by an industrial heritage and a riverside setting.



The selection of materials therefore draws on two definable elements. Firstly it is reflective of a contemporary urban village, with high quality architecture and an industrial heritage. Secondly, the river environment and its geology are represented by natural materials such as granite, sandstone and timber. Landscaping and a healthy tree cover are vital to modify the microclimate and provide shade, wind control, habitat and beauty.

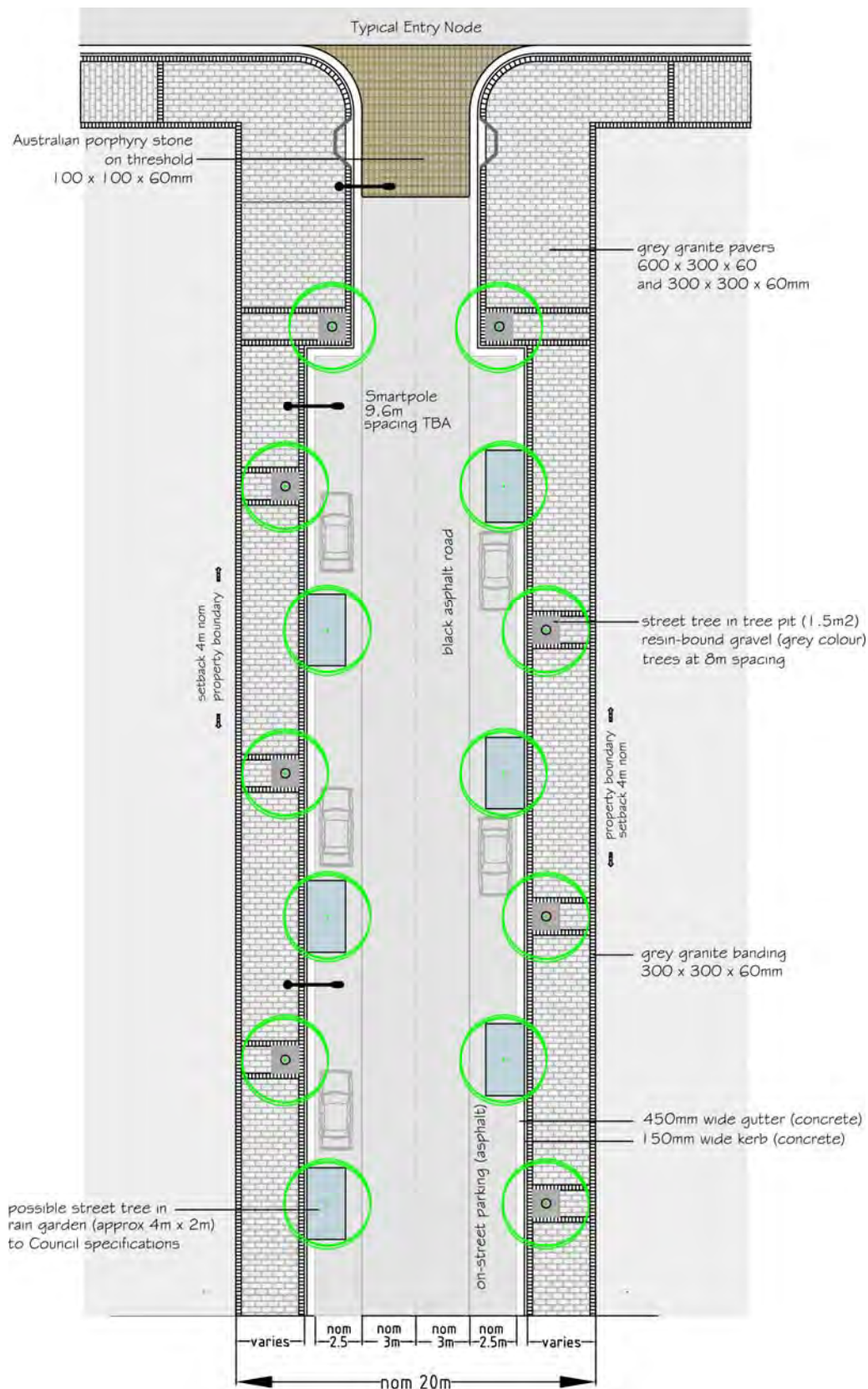
### **5.2.2 Design Concepts**

This section shows typical street plans and sections for the public domain in Meadowbank.

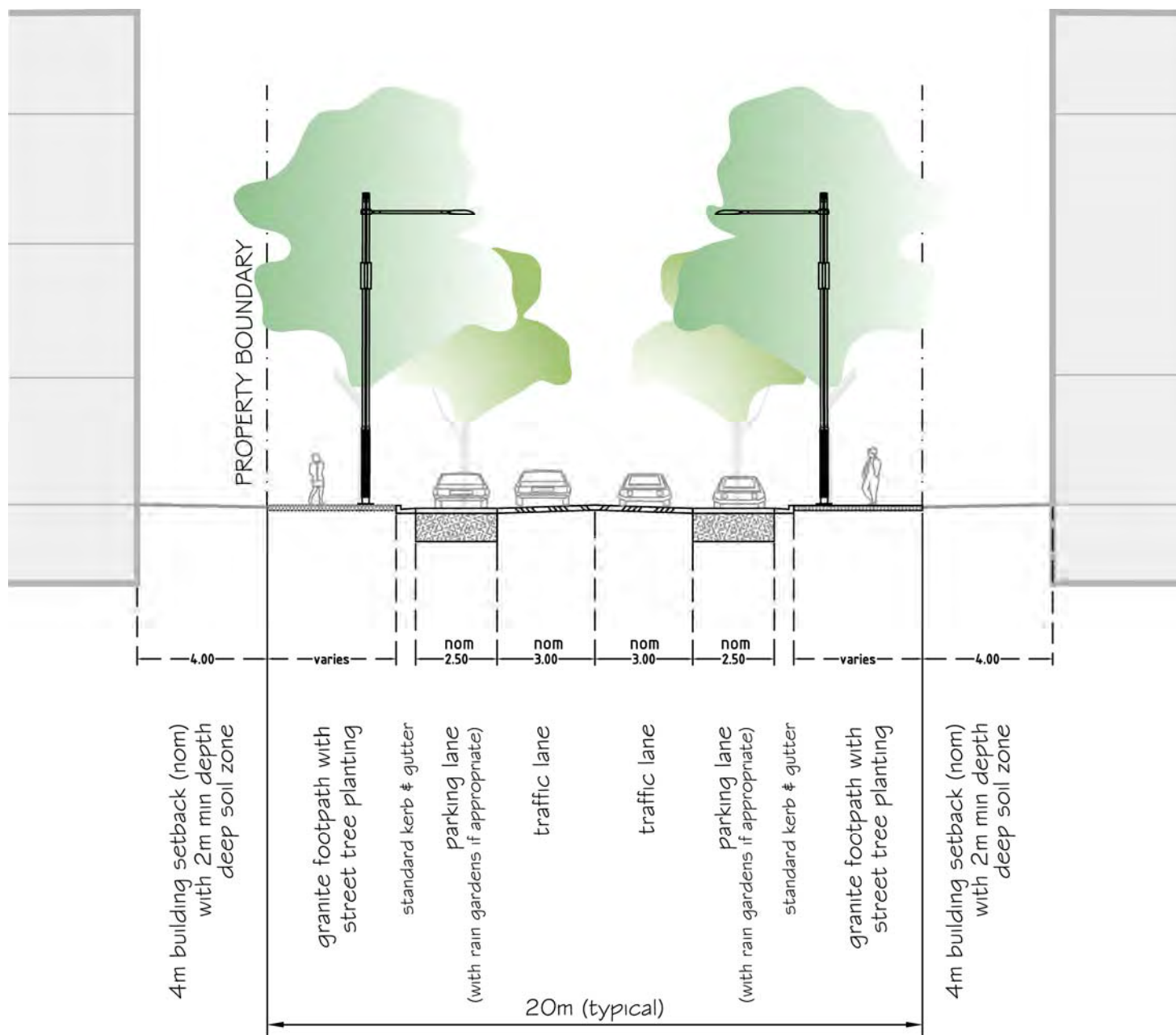
The first plan and section diagrams are for Nancarrow Road and new streets.

The 'typical street plan' and 'typical street section' are for all other streets in Meadowbank.

## Nancarrow Road and new streets (plan)

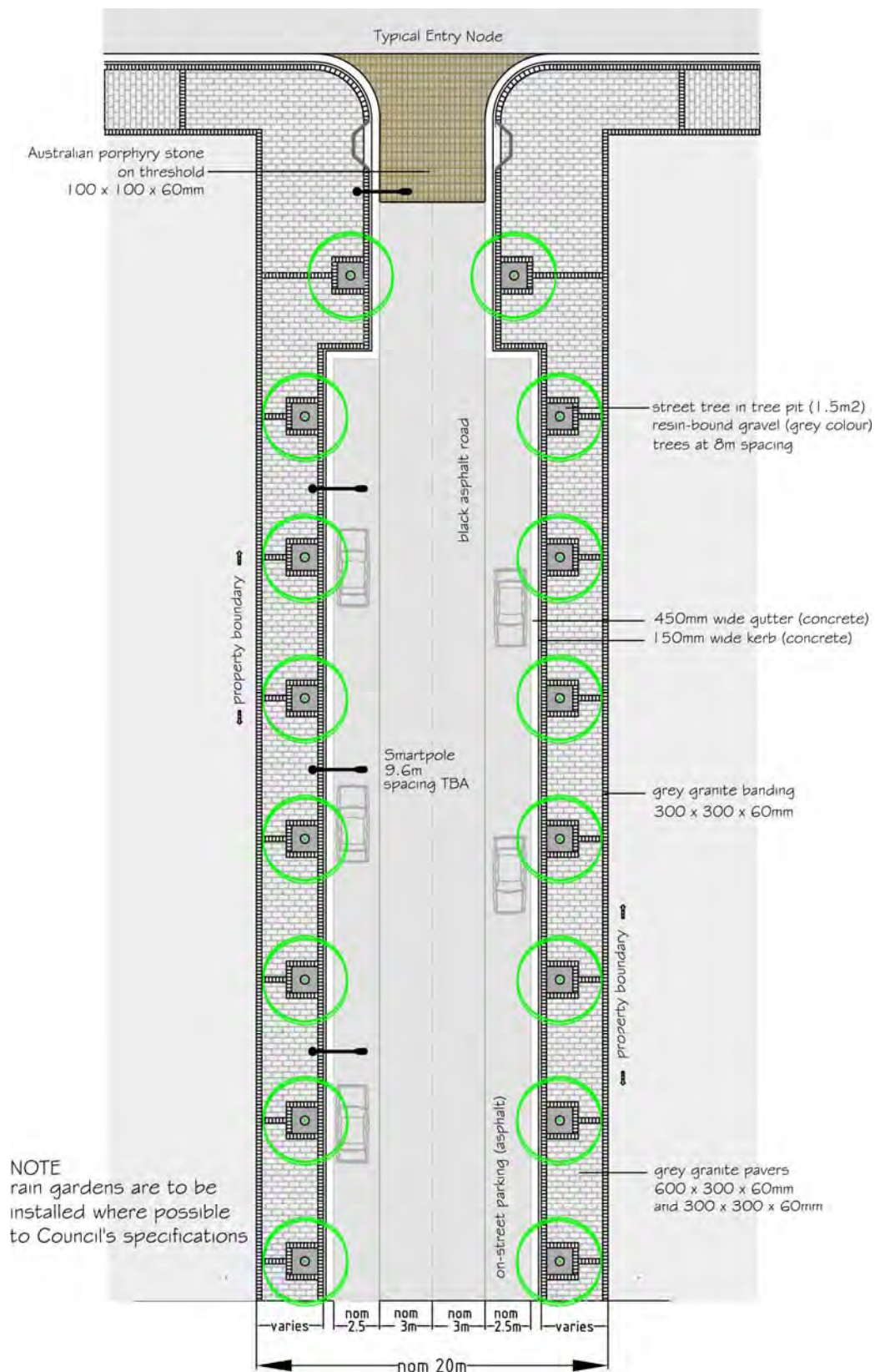


Nancarrow Road and new streets (section)





# Typical Street Plan for all other streets in Meadowbank



Typical Street Section for all other streets in Meadowbank



### **5.2.3 Street trees, Paving and Street Furniture**

Improving the streetscape quality has a direct and immediate benefit for the public domain. The opportunities for improving the streetscape are noted in Section 5.1.

The scope and general requirements for street trees, paving and street furniture are noted below. Refer to Council's Public Works Department for detailed specifications of these items. The detailed specifications should be considered when preparing development application documents. It is important these elements are coordinated between the public and semi-public domains.



## **Pavements**

### **Materials Palette— Summary**

Footpaths: grey granite paving.

Kerb & gutter: in-situ concrete.

Carriageway (typical): asphalt.

Foreshore pedestrian/cycleway: natural concrete.

Thresholds: Australian porphyry stone.

Special places: sandstone paving.

Tree pits: resin-bound gravel.

### **Footpaths**

Main paver: grey granite 600 x 300 x 60mm & 300 x 300 x 60mm.

Banding: grey granite 300 x 300 x 60mm.

*Concrete compressive strength  $\leq 25\text{Mpa}$ .*

### **Thresholds**

Australian porphyry stone 100 x 100 x 60mm laid at 30° to kerb.  
Concrete in-situ kerb.

### **Special places (low pedestrian traffic)**

Sandstone paving 600 x 300 x 100mm.

### **Tree pits and permeable paving**

Resin-bound gravel (grey colour).

### **Pedestrian/cycleways**

3m wide natural concrete.



Grey granite paving at Railway Road, Meadowbank.



Natural concrete shared pedestrian/cycleway, Parramatta River foreshore, Meadowbank.

Note sandstone paving provides a setting for artworks (special place).



Australian porphyry stone for thresholds.



Sandstone paving 600 x 300 x 100mm (Ryde Wharf Park).



## MEADOWBANK STREET TREE MASTER PLAN



## LEGEND

<span style="color: red;">—</span>	<i>Corymbia maculata</i> (Spotted Gum)	<span style="color: magenta;">—</span>	<i>Fraxinus griffithii</i> (Evergreen Ash)
<span style="color: green;">—</span>	<i>Angophora costata</i> (Sydney Red Gum)	<span style="color: yellow;">—</span>	<i>Fraxinus oxycarpa</i> 'Raywoodii' (Claret Ash)
<span style="color: blue;">—</span>	<i>Cupaniopsis anacardioides</i> (Tuckeroo)	<span style="color: orange;">—</span>	<i>Syncarpia glomulifera</i> (Turpentine)
<span style="color: cyan;">—</span>	<i>Pyrus calleryana</i> (Ornamental Pear)	<span style="color: purple;">—</span>	<i>Trostanopsis laurina</i> 'Luscious' (Water gum)
<span style="color: yellow;">—</span>	<i>Magnolia grandiflora</i> (Little Gem)	<span style="color: brown;">—</span>	<i>Metaleuca</i> sp. (Paperbark)
		<span style="color: green;">- - -</span>	<i>Angophora floribunda</i> (Rough-barked apple)
		<span style="color: green;">—</span>	<i>Eucalyptus tereticornis</i> (Forest Red Gum)

## NOTE

Existing street trees to be assessed in light of this plan and retained if appropriate





## **Street trees and rain gardens**

### ***Fraxinus griffithii* (Evergreen Ash)**

Planted as street trees on  
Railway Road, Meadowbank

Note kerb breaks for stormwater entry.



### **Rain garden**

At the corner of Railway Road  
and Constitution Road, Meadowbank



### **Rain garden**

At Meadowbank station entry, east side.





## **Furniture**

### **Seats (Seat 1)**

Urban Seat 11 (aluminium)  
Botton and Gardiner urban furniture  
ph (02) 9667 8100

### **Bins (Bin 1)**

Council's standard double bin.

### **Bollards (Bollard 4)**

1300mm high x 150mm dia  
1.6mm thick 316 stainless steel  
Core drilled to depth of 400mm.

Colorfen Constructions  
Ph (02) 9545 4284

### **Trellis (Trellis1)**

Stainless steel uprights 1200mm high at  
intervals of 1200mm. Five tensioned wires at  
220mm horizontal intervals.

Sub-surface mounted trellis within planter bed.  
Concrete footing for each post.

*(Contact Council's Public Works Department for  
more details of these items, such as  
installation).*



## **Pedestrian & Vehicular Lighting**

### **Street lighting**

Mounted on 9.6m multifunction poles to Council approval. Finish on poles to match existing poles in Belmore Street.

Poles to be provided with 1.5m banner arm, Rexel Optispan Aeroscreen luminaires complete with low loss ballasts and PE cells, and metal halide lamps.

### **Pedestrian/park lighting**

For areas such as shared bikeway/pedestrian pathways and plazas. Mounted on 4.8 m Smartpoles™ or approved equivalent. BEGA-8081 luminaires and metal halide lamps.

### **Solar lighting for parks**

Supplier: Solar G

Pole: galvanised utility pole

Light fittings: "Streetwalker" on 1 metre outreach arm

Lamps: 2x 14W fluorescent (T5 fluorescent technology, 96 lumens per watt).

### **General requirements**

Street and park lighting to be in accordance with AS/NZS 1158 to minimum lighting levels of subcategory V3 (collector roads), V5 (secondary streets) and P2 (cycle and footpath areas). Higher lighting levels where required by the standard (eg. transport interchanges, pedestrian crossings).

Weatherproof lockable cubicle to be provided to Energy Australia's requirements to house the switchboard and meter.

Connection to Ausgrid network to their requirements.

Spare 63mm dia electrical conduit to run between multifunction poles for future use.



Multifunction poles at Parsonage St, Meadowbank

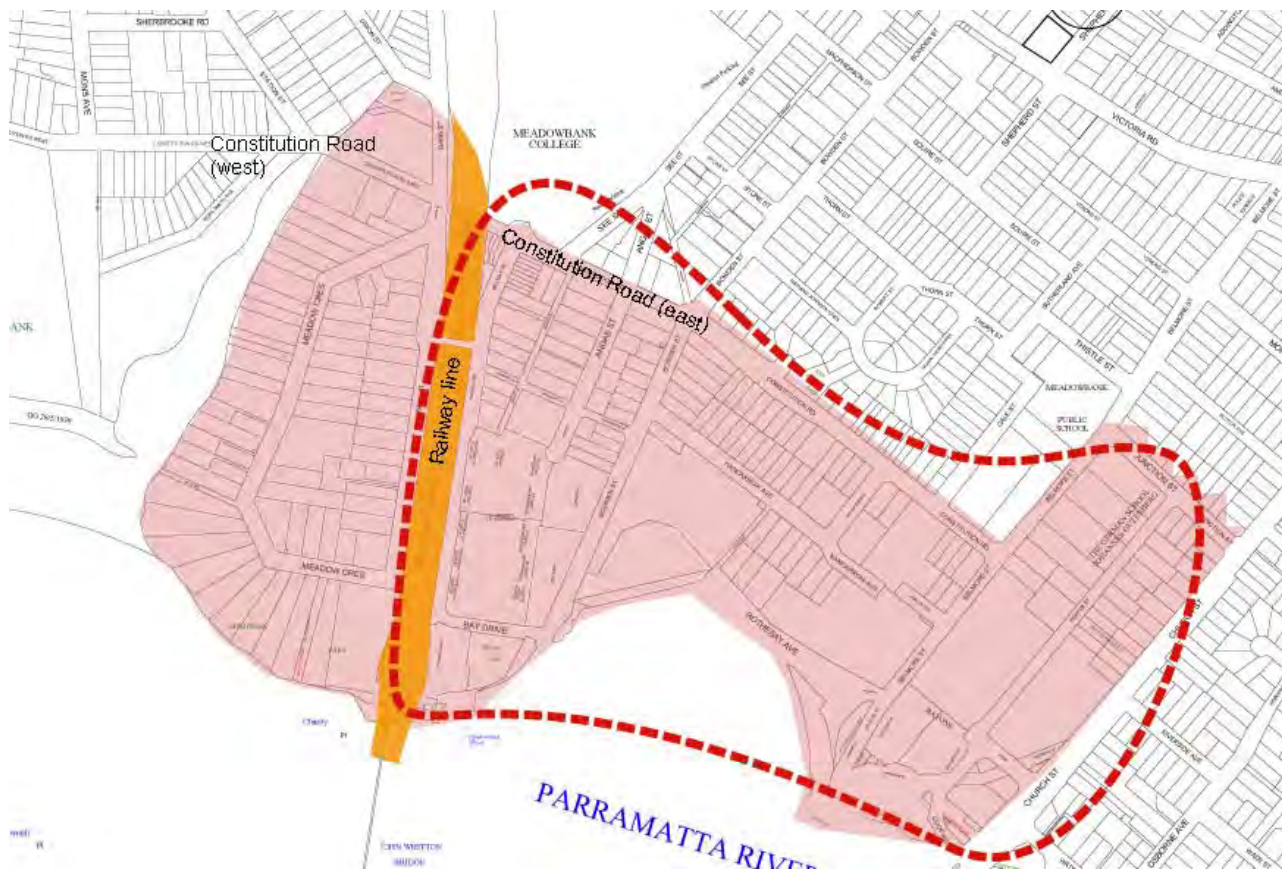


Pedestrian light at Meadowbank station plaza



Solar light at Meadowbank





Extent of under grounding of overhead services



# City of Ryde Public Domain Technical Manual

## Chapter 6: Macquarie Park Corridor

## Translation

### ENGLISH

If you do not understand this document please come to Ryde Civic Centre, 1 Devlin Street, Ryde Monday to Friday 8.30am to 4.30pm or telephone the Telephone and Interpreting Service on 131 450 and ask an interpreter to contact the City of Ryde for you on 9952 8222.

### ARABIC

إننا نعتذر عليك فهم محتويات هذه الوثيقة، نرجو للحضور إلى مركز بلدية رايد Ryde Civic Centre على العنوان: 1 Devlin Street, Ryde من الاثنين إلى الجمعة بين الساعة 8.30 صباحاً والساعة 4.30 بعد الظهر أو الاتصال بمكتب خدمات الترجمة على الرقم 131 450 لكي تطلب من أحد المترجمين الاتصال بمجلس مدينة رايد، على الرقم 9952 8222، لتلبية عنك.

### ARMENIAN

Եթե այս գրությունը չէք հասկնալ, խնդրեմ եկե՛ք Րայդ Բնակիչ Անկախ, 1 Տեվլին փողոց, Րայդ, (Ryde Civic Centre, 1 Devlin Street, Ryde) երկուշաբթիէն Ուրբաթ կա. ժամը 8.30 – կէ. ժամը 4.30, կամ հեռաձայնեցե՛ք Հեռաձայնի եւ Թարգմանության Ապաստանության՝ 131 450, եւ խնդրեցե՛ք որ թարգմանիչ մը Րայդ Բարադասանարանին հետ կապ հաստատուի: Հեղին համար, հեռաձայնելով՝ 9952 8222 թիվին:

### CHINESE

如果您看不懂本文，請在週一至週五上午 8 時 30 分至下午 4 時 30 分前往 Ryde 市政中心詢問 (Ryde Civic Centre, 地址: 1 Devlin Street, Ryde)。你也可以打電話至電話傳譯服務中心，電話號碼是: 131 450。接通後你可以要求一位傳譯員為你打如下電話和 Ryde 市政廳聯繫，電話是: 9952 8222。

### FARSI

اگر این مدرک را نمی فهمید لطفاً از 8.30 صبح تا 4.30 بعد از ظهر دوشنبه تا جمعه به مرکز شهرداری رايد، Ryde Civic Centre, 1 Devlin Street, Ryde مراجعه کنید یا به سرویس مترجم تلفنی شماره 131 450 تلفن کنید و از یک مترجم بخواهید که از طرف شما با شهرداری رايد شماره 9952 8222 تلفن کند.

### ITALIAN

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Amend. No.	Date approved	Effective date	Subject of amendment
1	21 January 2016	21 January 2016	<p>Revised structure and updated provisions for consistency with Part 4.5 Macquarie Park Corridor of the Ryde Development Control Plan 2014 (as amended in June 2015).</p> <p>Other key amendments include:</p> <ul style="list-style-type: none"> <li>• Addition of a new Open Space Network section to provide high-level design guidance relating to the function and character, paving and finishes, trees and landscaping, and lighting for public open space within Macquarie Park</li> <li>• Addition of a new Art in Public Spaces section to guide and encourage the incorporation of artworks in the public domain</li> <li>• Deletion of certain technical details that are addressed in relevant design guides and associated documents to remove repetition and ensure consistency</li> </ul>

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## 1.0 INTRODUCTION

### 1.1 Purpose

This Manual has been prepared by City Of Ryde to support the provisions of the *Ryde Local Environmental Plan 2014* (RLEP) and the *Ryde Development Control Plan 2014* (RDCP) in order to promote the revitalisation of the Macquarie Park Corridor. This document provides urban design guidelines and relevant information to assist developers and Council in undertaking public domain works within the Corridor.

### 1.2 What is the Public Domain?

Within the context of this document, the public domain represents all urban and natural elements, structures, and spaces that exist within the publicly-owned areas of the Ryde town centres and the relationship between them. The public domain also includes many privately-owned arcades, building forecourts, internal walkways, and other semi-public spaces as they also influence the overall character of the public domain.

### 1.3 Objectives of this Manual

The objectives of this manual is consistent with the *City of Ryde 2025 Community Strategic Plan* outcomes of Liveable Neighbourhoods, City of Connections, and City of Prosperity. This manual aims to establish a framework of design principles to guide the improvement of the public domain within the local centres. The main objectives are to:

1. Promote design solutions that enhance the amenity and character of the local area for residents, visitors, businesses, and other users.
2. Encourage the development of spaces that are attractive, well-used, robust, and safe for all users.
3. Provide new connections for better access and improved pedestrian and vehicular networks in centres.
4. Create flexible public spaces that will encourage diverse and different uses over time.
5. Encourage social interaction, a range of sensory experiences, and functionality through high-quality design.
6. Protect Macquarie Park's landscape character and acknowledge its natural heritage.
7. Promote the seamless integration of the public and private domains.
8. Encourage sustainable environmental practices in town and neighbourhood centres within the City of Ryde.
9. Promote outdoor dining and street activation by improving pedestrian amenity and access to outdoor dining areas.

## 1.4 How to Use this Manual

This manual is to be used to inform the design of proposed public domain works in order to:

1. Influence the understanding of the site context, including infrastructure needs and desired future character;
2. Guide design and development decisions to ensure seamless integration of the public and private domain; and
3. Inform the choice of materials, street furniture, tree planting and landscaping, and other streetscape elements to be used within the Macquarie Park Corridor.

This document is to be read in conjunction with the *Ryde Local Environmental Plan 2014*, *Ryde Development Control Plan 2014* (4.5 Macquarie Park Corridor), and relevant supporting Technical Guidelines and associated documents (listed in Table 1.6.1).

This Manual is to be reviewed annually or as required by Council to ensure its consistency with relevant State and local planning controls and standards.

**Note:** All proposed works covered by this Manual, including civic works relating to the open space network, are subject to the approval of the Manager Strategic City and/or Manager Asset Systems.

## 1.5 Structure of this Manual

This manual is comprised of the following sections:

### *Section 1: Introduction*

This section details the purpose and objectives of this Manual, its use, and its relationship with relevant technical guidelines and associated documents. This section also provides an overview of the planning framework for Macquarie Park and its existing and desired future character.

### *Section 2: Design Principles*

This section provides design principles to guide the development of the public domain within the Corridor.

### *Section 3: Access Network*

This section provides a framework for the public domain access network including key objectives, strategies, and specifications for paving, street trees, street furniture, and fixtures.

### *Section 4: Open Space Network*

This section provides a framework for the public domain open space network including key objectives, strategies, and specifications for paving, tree planting and landscaping, furniture, and fixtures.

### *Section 5: Public Art*

This section provides information to guide the integration of artworks in the public domain to establish a distinct identity and character.

### *Section 6: Typical Layouts*

This section provides information regarding the typical arrangement of streets with reference to streetscape elements including street lighting, tree planting and landscaping, street furniture, and fixtures.

## 1.6 Design Guides and Associated Documents

This manual is to be read in conjunction with the following design guides and associated plans and documents:

Table 1.6.1 Design Guides and Associated Documents

Document	Description
Design Guide for Streets	Guidelines to assist the design and construction of streets including paving, kerbing, driveways, etc.
Design Guide for Council-owned Street Lighting	Detailed guidelines for the design and installation of Council-owned street lighting, including multi-function light poles specifications, street lighting layout, and requirements for plan submission.
Street Lighting Schema for the Macquarie Park Business Area	Diagram to inform street lighting designers about the proposed configuration of the street lighting network owned by Council, including circuit zones for metering purposes.
Design Guide for Public Open Space	Guidelines to assist in the design and construction of parks including paving, landscaping, embellishment, and other associated public domain works.
Outdoor Dining Policy	Policy applying to land owned or under the care, control, and management of Council. Outlines Council's requirements for outdoor dining including having a minimum area of 4 square metres and maintaining a clear unobstructed pedestrian corridor (2 metres in width).
Shrimptons Creek Parklands Plan of Management	Sets out the vision for Shrimptons Creek Parklands to recognise the creek-line as an environmental and recreational spine in the City of Ryde. The parklands is also envisioned to provide clear connections to adjacent urban environments without compromising their natural character.
Ryde Street Tree Master Plan	Informs and guides all street tree planting and assists the City of Ryde and the community in managing and expanding the urban forest.
Ryde Bicycle Strategy	A comprehensive plan for improving the environment for people who ride bicycles for transport, health, and fitness to facilitate increased active transport patronage within the City of Ryde over the next 10 years and beyond.
Macquarie Park Pedestrian Access and Mobility Plan	Provides a framework for developing safe and convenient pedestrian routes and fostering improvements in personal mobility.
Ryde Public Art Policy	Provides a framework for the delivery and administration of public art in the City of Ryde.



## 1.7 Macquarie Park Planning Framework

This document is consistent with the provisions of the following :

- *Ryde Local Environmental Plan 2014* (RLEP)
- *Ryde Development Control Plan 2014* (RDCP) - Part 4.5 Macquarie Park

The RLEP and RDCP set out the broad framework for development within the Macquarie Park Corridor. This includes the expansion of the public domain to cater for people who will live, work, and visit Macquarie Park as it evolves from a business park that was established in 1960s to an urban centre supported by public transport and public domain infrastructure. The framework for the future public domain comprises:

- An Access Network that identifies and coordinates the delivery of proposed new roads and pedestrian ways.
- An Open Space Network that identifies new parklands and urban plazas.

Development in the Macquarie Park Corridor must occur within the framework of the objectives and controls of the RDCP 2014, which relate to the street layout, size and shape of city blocks, open spaces, and built form. The synthesis of these elements will define the public domain and strengthen the existing character and identity of the Corridor.

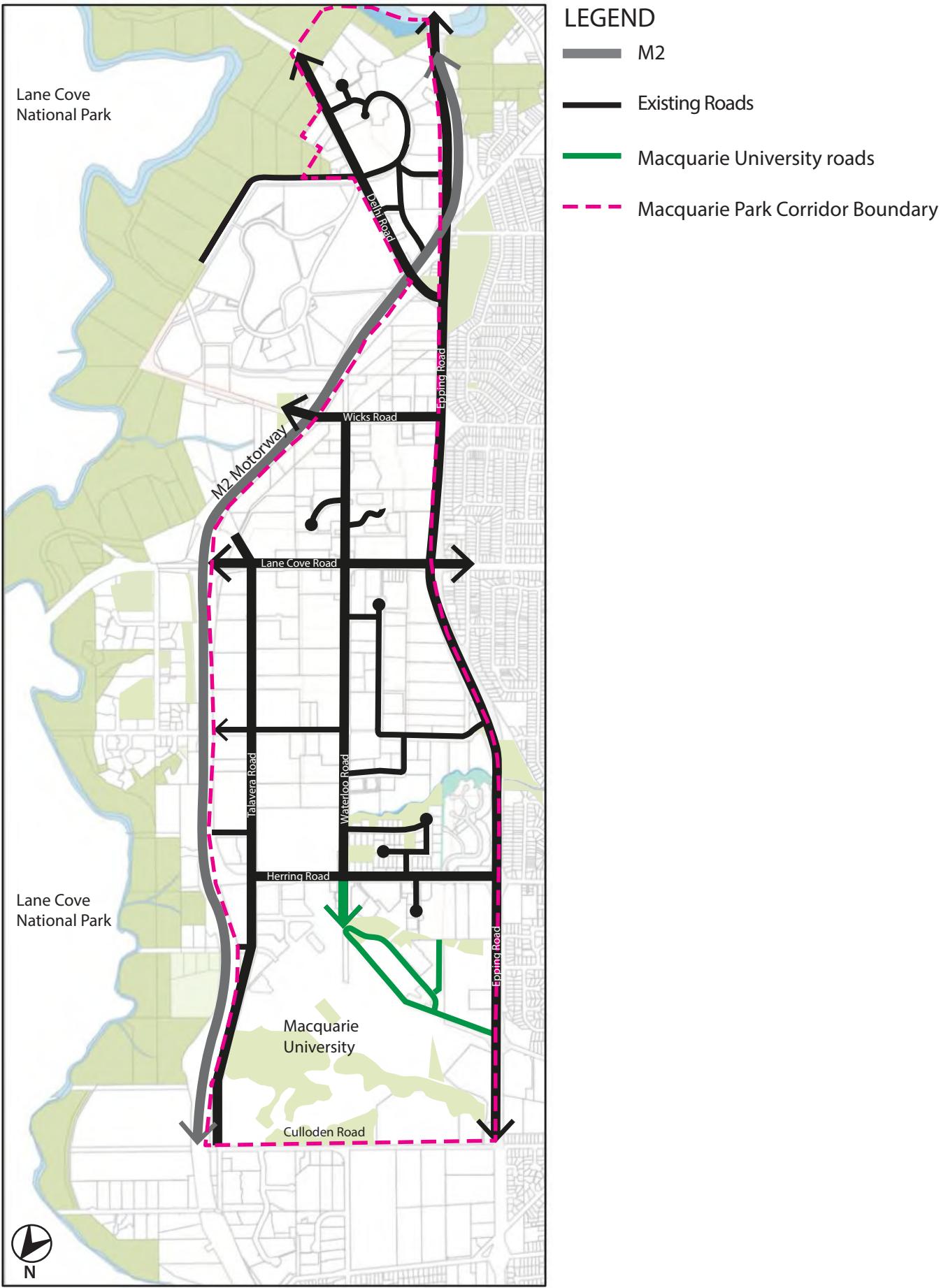


Figure 1.5.1 Macquarie Park Corridor Boundary and Existing Road Network

## 1.8 Macquarie Park Corridor Character

### *Existing Character*

The Macquarie Park Corridor is located 12 kilometres from Sydney CBD and 13 kilometres from Parramatta CBD, with over 200 hectares of mixed use, commercial, and business land use. It is a nationally significant economic corridor for research and business, specialising in the communications, medical research, pharmaceutical and IT&T sectors.

The Corridor has three railway stations and access to bus transport, but there are limited linkages between existing roads and the surrounding road network. The urban structure is characterised by wide streets and large street blocks with a commercial core centered around Macquarie Park Station and Waterloo Road, a retail core anchored on Macquarie Shopping Centre, an educational and health precinct comprising Macquarie University and Macquarie Hospital, business park areas, and protected vegetation in the Lane Cove National Park and Shrimptons Creek.

### *Vision*

The Macquarie Park Corridor is envisioned to mature into a premium location for globally competitive businesses with an enhanced sense of identity and strong links to the university and research institutions.

### *Desired Future Character*

The Macquarie Park Corridor will be characterised by a high-quality, well-designed, safe, and liveable environment that reflects the natural setting. The Corridor will have accessible and vibrant focal points around the three railway stations and enhanced access and open space network to support its continuous commercial and business growth and the planned residential communities centred on the North Ryde Station and Macquarie University Station Priority Precincts.

The implementation of the RDCP 2014 and this document will guide development to achieve the vision and desired future character for the Macquarie Park Corridor through improved vehicular, pedestrian, and cycle permeability and connectivity to surrounding centres and regional open spaces.

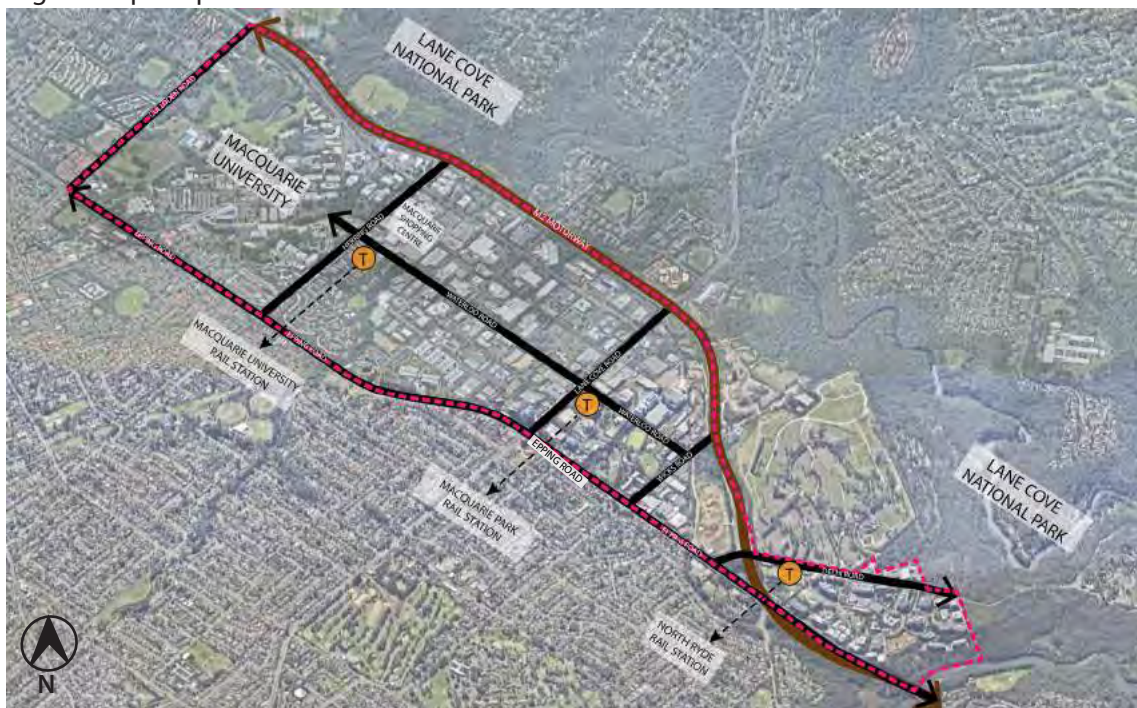


Figure 1.8.1 Macquarie Park Corridor - Aerial Photo (December 2015)



## 2.0 DESIGN PRINCIPLES

Promoting pedestrian access is central to the design of the public domain and increases opportunities for social interaction and community life. Streets and public spaces should be comfortable, safe, and engaging places that encourage people to stay. Pedestrian routes to and within Macquarie Park should be designed to be accessible to everyone, promote walking, and reduce conflict between pedestrians, bicycles, and cars.

Public art, trees, directional signage, and street furniture should be incorporated into streets and public spaces as they not only contribute to the visual and spatial quality of the public domain but also improve the human experience by providing amenities such as shade, seating, and wayfinding. Sustainability outcomes can also be promoted in streets, parks, and urban spaces by including street trees where possible and implementing measures to harvest and reuse water.

The following key principles guide the design and development of public spaces within the centres of the City of Ryde:

### *Liveability*

- Create spaces that provide a desirable setting and backdrop for social interaction and a variety of activity
- Provide unified streetscapes that are high-quality, durable, and timeless in design
- Eliminate level changes and obstructions and promote consistent paving patterns as much as possible

### *Active Transport*

- Prioritise pedestrian amenity by providing opportunities to expand the public domain
- Eliminate pedestrian, cycle, and vehicle conflict as much as possible
- Provide amenities such as seats, shelter, and bike racks to support pedestrian and bicycle use

### *Landform and Views*

- Retain and/or reveal significant views from the public domain to waterways, heritage items, and significant sites
- Identify significant views and consider them in the selection and placement of public art, street trees, and furniture

### *Trees*

- Integrate tree and landscape treatments to provide shade, interest, and amenity as well as unify streetscapes
- Create green corridors with increased tree planting and landscape treatments to reduce the extent of hard surfaces and increase canopy cover, biodiversity, and microclimate

### *Public Art*

- Create public art that aids legibility and defines a sense of place and identity
- Provide visual interest to everyday activities and social gatherings

### *Water Sensitive Urban Design*

- Collect and reuse stormwater for watering street trees and landscape elements where possible
- Treat stormwater as close to the source as possible
- Collect and treat stormwater in bio-retention tree pits and/or rain and filter gardens that are seamlessly integrated with the design of the public space

### *Safer by Design*

- Ensure that all publicly accessible pathways have sufficient width, lighting, and finishes to enhance public safety
- If possible, pathways should be adjacent to active spaces to improve amenity and safety
- If possible, locate pathways where conflicts with vehicular traffic are avoided

### *Equitable Access and Use*

- Enable equitable and safe access for people of all ages and abilities in accordance with the *Building Code of Australia* (BCA) and the *Disability (Access to Premises – Buildings) Standards (Premises Standards) - AS 1428*.
- Ensure continuous accessible paths of travel and circulation spaces and appropriate facilities for people who use wheelchairs and people who have ambulatory and sensory disabilities

### 3.0 ACCESS NETWORK

#### 3.1 Streets Hierarchy

##### Objectives

- a. Improve pedestrian, cycle, and vehicular accessibility and permeability to and within the Macquarie Park Corridor
- b. Establish a clear hierarchy of public streets within the corridor
- c. Accommodate increased traffic movement within the corridor
- d. Provide additional opportunities for on-street car parking

##### Strategy

The planning controls reinforces Waterloo, Herring, Talavera, and Lane Cove Roads as the principal streets within the Macquarie Park Corridor. In particular, the permissible height and FSR will see Waterloo and Herring Roads develop as main streets with a boulevard character. The network of existing streets will be augmented by proposed new streets to create an access network that aims to substantially improve traffic congestion and create a more permeable and amenable public domain for pedestrians and cyclists. The access network will also provide better access to parks and significant public places within the Corridor.

The hierarchy of existing and proposed new streets that comprise the access network is detailed below:

Table 3.1.1 Street Hierarchy - Primary and Secondary Streets

Existing Streets Hierarchy	Carriageway	Approx. Road Reserve Width
Primary		
Epping Road	26 metres (varies)	40 metres
Lane Cove Road	26 metres	30 metres
Herring Road	23 metres	30 metres
Waterloo Road (between Lane Cove and Herring Roads)	20 metres	30 metres
Delhi Road	12 metres (varies)	30 metres
Secondary		
Talavera Road	16 metres	25 metres
Khartoum Road (between Talavera Road and M2 Motorway)	14 metres	25 metres
Khartoum Road (between Talavera and Waterloo Roads)	12 metres	20 metres
Byfield Street	12 metres	22 metres
Lyon Park Road	12 metres	21 metres
Wicks Road	12 metres	20 metres
Waterloo Road (between Lane Cove and Wicks Roads)	12 metres	20 metres
Eden Park Drive	12 metres	20 metres
Coolinga Street	11 metres	20 metres
Giffnock Avenue	11 metres	20 metres



Table 3.1.2 Street Hierarchy - Tertiary and New Streets and Connections

Existing Streets Hierarchy	Carriageway	Approx. Road Reserve Width
<b>Tertiary</b>		
Rivett Road	8 metres	12 metres
Richardson Place	8 metres	12 metres
Julius Avenue	12 metres	18 metres
<b>Residential Character</b>		
Windsor Drive	13 metres	22 metres
Peachtree Road	11 metres	21 metres
Lachlan Avenue	10 metres	20 metres
Cottonwood Crescent	15 metres	20 metres
<b>Proposed New Streets and Connections Hierarchy</b>		
20-metre wide streets	11 metres	20 metres
14.5-metre wide streets	8.5 metres	14.5 metres
Pedestrian connections	2 metres (path)	6 metres



## 3.2 Access Network - Paving

### Objectives

- Create a visually seamless and unified public domain
- Utilise robust and durable materials with a consistent colour and finish to create a visually consistent ground plane
- Achieve a safe and convenient pedestrian environment that encourages social interaction and active transport, in accordance with the objectives of the Macquarie Park Corridor DCP

### Strategy

The strategy for paving is to create a clear and coherent public domain finish that provides a unified and recognisable character and ensures the amenity of pedestrians and cyclists alike. Paving materials have also been selected for ease of maintenance, durability, and robustness.

The paving specifications reflect the local context, scale of streets, and their role within the access network hierarchy. For example, full-width granite paving on majority of Waterloo Road reflects its role as the central civic spine and as a primary pedestrian route that links the rail stations. The same paving treatment also applies for Lane Cove Road, Herring Road, and Talavera Road, in lieu of their desired future character and role within the access network hierarchy. The paving treatment for Epping and Delhi Roads responds to their local context where the width of the verge varies significantly. The paving specifications are further detailed below:

Table 3.2.1 Paving Specifications for New and Existing Streets

Location	Description	Specification	Reference
New 20-metre wide street	To be dedicated to Council	Granite paving - full width footway	Section 6.1
New 16-metre wide street	To be dedicated to Council	Granite paving - full width footway	Section 6.2
New 14.5-metre wide street	To be dedicated to Council	Granite paving - full width footway	Section 6.3
New pedestrian ways	Subject to easement or right of way as required by Council	Concrete paving with granite banding	Section 6.5
Existing primary streets (Epping and Delhi Roads)	30 to 40 metres wide overall	Granite paving (2.4 metres) with nature strip	Section 6.6.1
Existing Mixed Use, Commercial, or Business Character	18 to 25 metres wide overall	Granite paving - full width of the verge  The footway may vary between 2 to 5 metres	Section 6.6.2
Existing residential streets	12 to 20 metres wide overall	Granite paving (2.4 metres; varies) with nature strip	Section 6.6.3

**Note:** The verge refers to the area between the kerb and property boundary



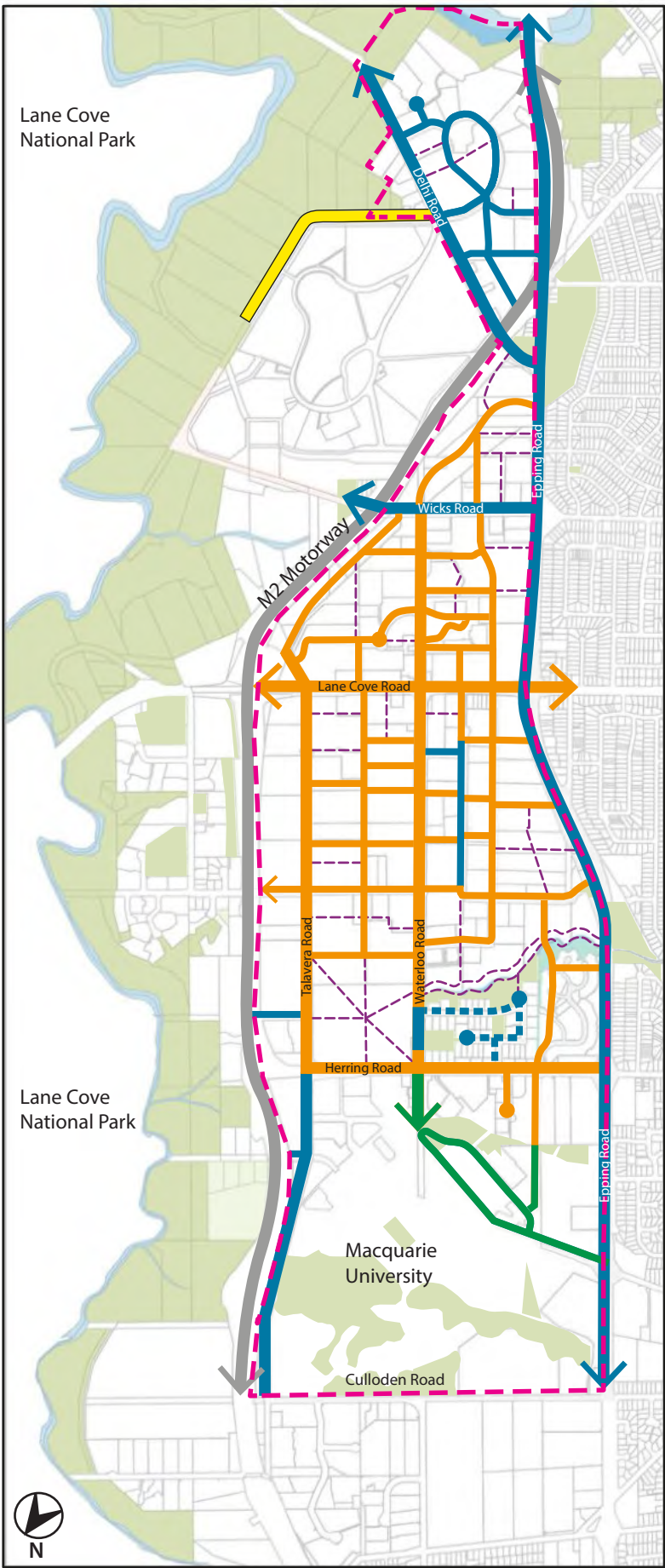
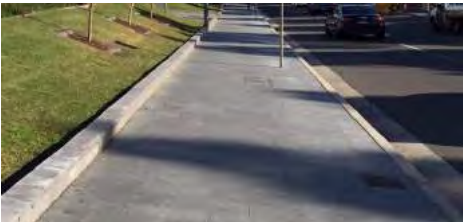


Figure 3.2.1 Macquarie Park Corridor Paving Plan

LEGEND

- M2
- Full-width granite paving
- 2.4-metre wide granite paving with nature strip
- Up to 2.4-metre wide (varies) granite paving with nature strip
- Pedestrian ways - concrete with granite banding
- Concrete path
- Macquarie University roads
- Macquarie Park Corridor Boundary

Full-width granite paving



E.g. Lane Cove Road

2.4-metre wide granite paving with nature strip



E.g. Epping Road

Concrete with granite banding



E.g. Quarry Road

Concrete path



E.g. Plassey Road

### 3.3 Access Network - Cycleways

#### Objectives

- a. Maximise cycle connections to regional cycle routes through and around the Corridor
- b. Support cycle use by creating a safe and high-quality cycle network
- c. Maximise permeability and interchange opportunities with public transport within the Corridor
- d. Minimise conflict between pedestrians, cyclists, and public domain elements

#### Strategy

The Regional Bicycle Network is to be implemented as off-street shared cycleways in accordance with the Macquarie Park Public Domain Technical Manual. The Regional Bicycle Manual comprises:

- Waterloo Road;
- Delhi Road;
- Epping Road;
- Lane Cove Road;
- Khartoum Road;
- M2 Motorway; and
- Shrimptons Creek pathways.

The Local Bicycle Network is to be implemented as off-street shared cycleways in accordance with the Macquarie Park Public Domain Technical Manual. The Local Bicycle Manual comprises:

- Lyon Park Road;
- Talavera Road;
- Wicks Road;
- Proposed new roads (Numbers 17, 18, 19, 20, 21, and 26) in accordance with the *Ryde Bicycle Strategy 2014*

Cycleways are to be located adjacent to property boundary to minimise conflict with street trees, lighting, signage, and other public domain elements.



Figure 3.3.1 Macquarie Park Corridor Cycle Network Plan



### 3.4 Access Network - Street Trees

Macquarie Park was once filled with a series of market gardens and orchards with vast tracts of bushland prior to the establishment of the Macquarie University and its development as an employment area in the mid-1960s. Although there are currently no extant remains of this market gardens and orchards heritage, Macquarie Park's existing landscape character remains that of a green environment, which features trees and large lots with no front fencing that simulate an appearance of extensive open spaces.

Macquarie Park's landscape character is defined by its predominant native tree canopy with minimal exotic tree canopy, its strong links to Lane Cove National Park, and its creeks and riparian corridors.

#### *Native Tree Canopy and links to Lane Cove National Park*

Macquarie Park has strong links to Lane Cove National Park, which is represented by native tree planting throughout its parks and streetscapes.

The existing native tree canopy is predominantly smooth-barked species such as Spotted Gum, with some rough-barked species such as Brush Box in residential character streets.

Native tree species are also planted within private property setbacks (1.5-metre setback from the property boundary), which create clear sightlines along pedestrian paths that are bounded by vegetation.

#### *Minimal Exotic Tree Canopy*

There are some existing exotic trees planted within private property setbacks, median strips, and within the verge along certain sections of Herring Road and Julius Avenue. The exotic tree canopy are predominantly Plane and Liquidambar trees, which create a shaded green ambience particularly during the summer season.

Understorey planting is also characterised by a combination of formal exotic shrub planting and native grass planting such as Orange Jessamine, Lilly Pilly, Duranta Sheena's Gold, and Spiny-head Mat Rush.

#### *Creeks and Riparian Corridors*

There were four creeks that traversed the Corridor (Industrial, Porters, Shrimptons, and University creeks), most of which have been piped and are not currently visible. Open space generally surrounds certain sections of creeks that remain open. Whilst the vegetation in some sections of the riparian corridors and creeks are comprised of exotic weeds, there are areas such as sections of Shrimptons Creek (near Waterloo Road) where revegetation of Sydney Turpentine Ironbark Forest species is evident.

### Objectives

- a. Create a desirable streetscape environment by providing a shaded ambience and ensuring pedestrian amenity
- b. Respond to the local environment by utilising species that are of an appropriate scale and type and that complements to the local context and streetscape character
- c. Recognise the role of streets in habitat creation and provide bio-links and connections
- d. Define a unique landscape character for key streets and other street typologies by creating signature gateway large-scale planting on key streets and smaller-scale planting on other street typologies.
- e. Recognise the landscape heritage of market gardens and orchards within Macquarie Park

### Strategy

Based on the existing landscape character of Macquarie Park, the following overall strategy is proposed for street tree planting within the Macquarie Park Corridor:

- Create a shaded ambience and improve pedestrian amenity by promoting a consistent street tree layout (Refer to *Part 6.0 Typical Layouts*) and by providing street tree planting within the verge, where possible.
- Complement existing landscape character by utilising smooth-barked native tree species such as Sydney Red Gum, Spotted Gum, and Forest Red Gum, where existing streetscape is predominantly native tree species while maintaining the minimal exotic tree canopy at certain locations where the existing streetscape has exotic tree species.
- Encourage native tree planting within the 1.5-metre private property setback (where possible) to reinforce the green landscape character of the Corridor and allow for clear sightlines on major roads such as Lane Cove Road and Waterloo Road.
- Encourage native grass understorey to native street tree planting with drought-tolerant species such as *Lomandra Longifolia* ('Tanika' Spiny-head Mat Rush) or *Dianella caerulea* (Blueberry lily or Blue flax-lily).
- Allow for feature native trees, particularly on street corners and private property entry threshold that could contribute to the public domain.

Table 3.4.1 Street Tree Selection Palette for Existing Primary Streets

Streets	Tree Selection Palette
<b>Existing Primary</b>	
Epping Road, Waterloo Road (Lane Cove to Herring Road), Lane Cove Road, and Delhi Road	<p><b><u>Large Smooth-barked Native Trees:</u></b>  <i>Angophora costata</i> (Sydney Red Gum);  <i>Corymbia maculata</i> (Spotted Gum);  <i>Eucalyptus tereticornis</i> (Forest Red Gum)</p> <p><b><u>Trees within the 1.5m private property setback:</u></b>  <i>Angophora costata</i> (Sydney Red Gum);  <i>Angophora floribunda</i> (Rough-barked Apple);  <i>Corymbia maculata</i> (Spotted Gum);  <i>Eucalyptus botryoides</i> (Swamp Mahogany);  <i>Eucalyptus acmenoides</i> (White Mahogany);  <i>Eucalyptus pilularis</i> (Blackbutt);  <i>Eucalyptus piperita</i> (Sydney Peppermint);  <i>Eucalyptus saligna</i> (Sydney Blue Gum);  <i>Eucalyptus resinifera</i> (Red Mahogany);  <i>Eucalyptus tereticornis</i> (Forest Red Gum)</p> <p><b><u>Feature Broad Canopy Trees (on street corners and private property entry threshold, where appropriate):</u></b>  <i>Ficus rubiginosa</i> (Port Jackson Fig);  <i>Platanus digitalis</i> (Plane Tree);  <i>Ulmus parvifolia</i> (Chinese Elm)</p>
Herring Road	<p><b><u>Large Smooth-barked Native Trees:</u></b>  As listed above</p> <p><b><u>Small Trees:</u></b>  <i>Buckinghamia celsissima</i> (Ivory Curl);  <i>Eleocarpus eumundii</i> (Smooth Quandong);  <i>Eleocarpus reticulatus</i> (Blueberry Ash);  <i>Flindersia australis</i> (Crow's Ash);  <i>Harpullia pendula</i> (Tulipwood);  <i>Syncarpia glomulifera</i> (Turpentine);  <i>Tristaniopsis laurina</i> (Water Gum);  <i>Tristaniopsis laurina</i> ('Luscious' Water Gum);  <i>Waterhousia floribunda</i> (Weeping Lilly Pilly)</p> <p><b><u>Trees within the 1.5m private property setback and feature Broad Canopy Trees (on street corners and private property entry threshold, where appropriate):</u></b>  As listed above</p>

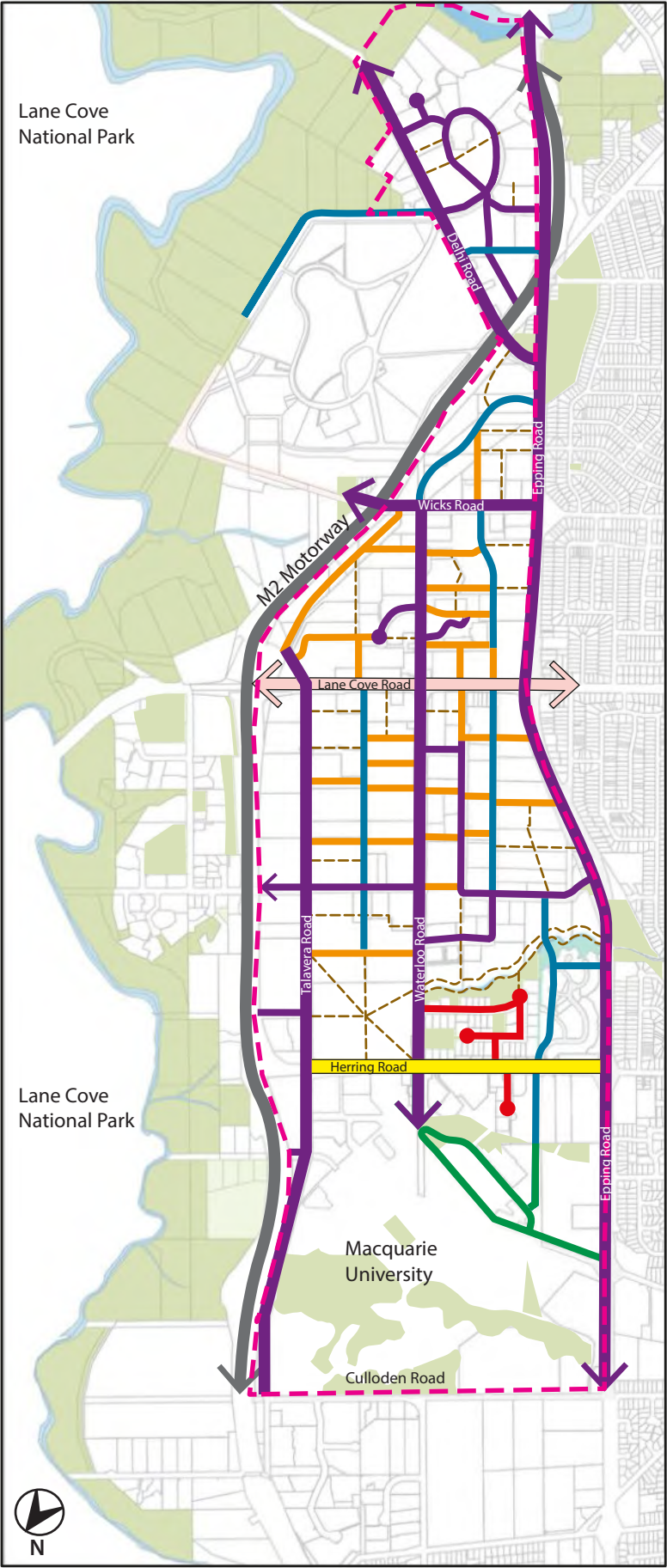


Table 3.4.2 Street Tree Selection Palette for Existing Secondary and Tertiary Streets

Existing Secondary	
Talavera Road, Khartoum Road (Talavera Rd to M2), Khartoum Road (Talavera Rd to Waterloo Rd), Byfield, Lyon Park Road, Wicks Road, and Waterloo Road (Lane Cove Rd to Wicks Rd)	<p><b><u>Large Smooth-barked Native Trees:</u></b>  <i>Angophora costata</i> (Sydney Red Gum);  <i>Corymbia maculata</i> (Spotted Gum);  <i>Eucalyptus tereticornis</i> (Forest Red Gum)</p> <p><b><u>Trees within the 1.5m private property setback:</u></b>  <i>Angophora costata</i> (Sydney Red Gum);  <i>Angophora floribunda</i> (Rough-barked Apple);  <i>Corymbia maculata</i> (Spotted Gum);  <i>Eucalyptus botryoides</i> (Swamp Mahogany);  <i>Eucalyptus acmenoides</i> (White Mahogany);  <i>Eucalyptus pilularis</i> (Blackbutt);  <i>Eucalyptus piperita</i> (Sydney Peppermint);  <i>Eucalyptus saligna</i> (Sydney Blue Gum);  <i>Eucalyptus resinifera</i> (Red Mahogany);  <i>Eucalyptus tereticornis</i> (Forest Red Gum)</p>
Eden Park Drive, Coolinga Street, Giffnock Avenue, and Julius Avenue	<p><b><u>Small Trees:</u></b>  <i>Angophora costata</i> (Sydney Red Gum);  <i>Buckinghamia celsissima</i> (Ivory Curl);  <i>Corymbia maculata</i> (Spotted Gum);  <i>Eleocarpus eumundii</i> (Smooth Quandong);  <i>Eleocarpus reticulatus</i> (Blueberry Ash);  <i>Flindersia australis</i> (Crow's Ash);  <i>Harpullia pendula</i> (Tulipwood);  <i>Pittosporum revolutum</i> (Yellow Pittosporum);  <i>Syncarpia glomulifera</i> (Turpentine);  <i>Tristaniopsis laurina</i> (Water Gum);  <i>Tristaniopsis laurina</i> ('Luscious' Water Gum);  <i>Waterhousia floribunda</i> (Weeping Lilly Pilly)</p> <p><b><u>Trees within the 1.5m private property setback:</u></b>  As listed above</p>
Existing Tertiary	
Rivett Road and Richardson Place	<p><b><u>Large Smooth-barked Native Trees:</u></b>  As listed above</p> <p><b><u>Trees within the 1.5m private property setback:</u></b>  As listed above</p>

Table 3.4.3 Street Tree Selection Palette for Existing Residential Character and New Streets

Existing Residential Character Streets	
Windsor Drive, Peachtree Road, Lachlan Avenue, and Cottonwood Crescent	<p><b>Small Trees:</b>  <i>Buckinghamia celsissima</i> (Ivory Curl);  <i>Eleocarpus eumundii</i> (Smooth Quandong);  <i>Eleocarpus reticulatus</i> (Blueberry Ash);  <i>Flindersia australis</i> (Crow's Ash);  <i>Harpullia pendula</i> (Tulipwood);  <i>Pittosporum revolutum</i> (Yellow Pittosporum);  <i>Syncarpia glomulifera</i> (Turpentine);  <i>Tristanopsis laurina</i> (Water Gum);  <i>Tristanopsis laurina</i> ('Luscious' Water Gum);  <i>Waterhousia floribunda</i> (Weeping Lilly Pilly)</p> <p><b><u>Trees within the 1.5m private property setback:</u></b>  <i>Angophora costata</i> (Sydney Red Gum);  <i>Angophora floribunda</i> (Rough-barked Apple);  <i>Corymbia maculata</i> (Spotted Gum);  <i>Eucalyptus botryoides</i> (Swamp Mahogany);  <i>Eucalyptus acmenoides</i> (White Mahogany);  <i>Eucalyptus pilularis</i> (Blackbutt);  <i>Eucalyptus piperita</i> (Sydney Peppermint);  <i>Eucalyptus saligna</i> (Sydney Blue Gum);  <i>Eucalyptus resinifera</i> (Red Mahogany);  <i>Eucalyptus tereticornis</i> (Forest Red Gum)</p>
New Streets	
Proposed new 20-metre wide street	<p><b><u>Large Smooth-barked Native Trees (entries and blister planting):</u></b>  <i>Angophora costata</i> (Sydney Red Gum);  <i>Corymbia maculata</i> (Spotted Gum);  <i>Eucalyptus tereticornis</i> (Forest Red Gum)</p> <p><b><u>Small Trees (for tree pits) and trees within the 1.5m private property setback:</u></b>  As listed above</p> <p><b><u>Feature Broad Canopy Trees:</u></b>  <i>Ficus rubiginosa</i> (Port Jackson Fig);  <i>Platanus digitalis</i> (Plane Tree);  <i>Ulmus parvifolia</i> (Chinese Elm)</p>
Proposed new 16-metre and 14.5-metre wide street	<p><b><u>Small Trees (for tree pits):</u></b>  As listed above</p> <p><b><u>Trees within the 1.5m private property setback and feature Broad Canopy Native Trees:</u></b>  As listed above</p>



LEGEND

- M2
- Large smooth-barked native trees (with feature broad-canopy trees on street corners and entries, where possible)
- No street trees (tree planting within 1.5m private property setback only)
- Small native street trees in path
- Large smooth-barked in entries and blisters, small native trees in pits, and feature broad-canopy trees
- Small native trees in path and feature broad-canopy trees
- Large smooth-barked in entries and median and small native trees in continuous garden beds
- Pedestrian Ways with small trees in naturestrip
- Refer to Macquarie University Masterplan
- Macquarie Park Corridor Boundary

Note: Refer to *Street Tree Selection Palette* (Tables 3.4.1, 3.4.2, and 3.4.3) for further details

Figure 3.4.1 Macquarie Park Corridor Street Trees Plan



### 3.5 Access Network - Street Lighting

#### Objectives

- a. Provide an access network with high pedestrian, cycling, and vehicular amenity
- b. Reinforce a sense of safety for the community by providing appropriate street lighting
- c. Provide a street lighting network that complements the local context, surrounding developments, streetscape character, and other public domain elements

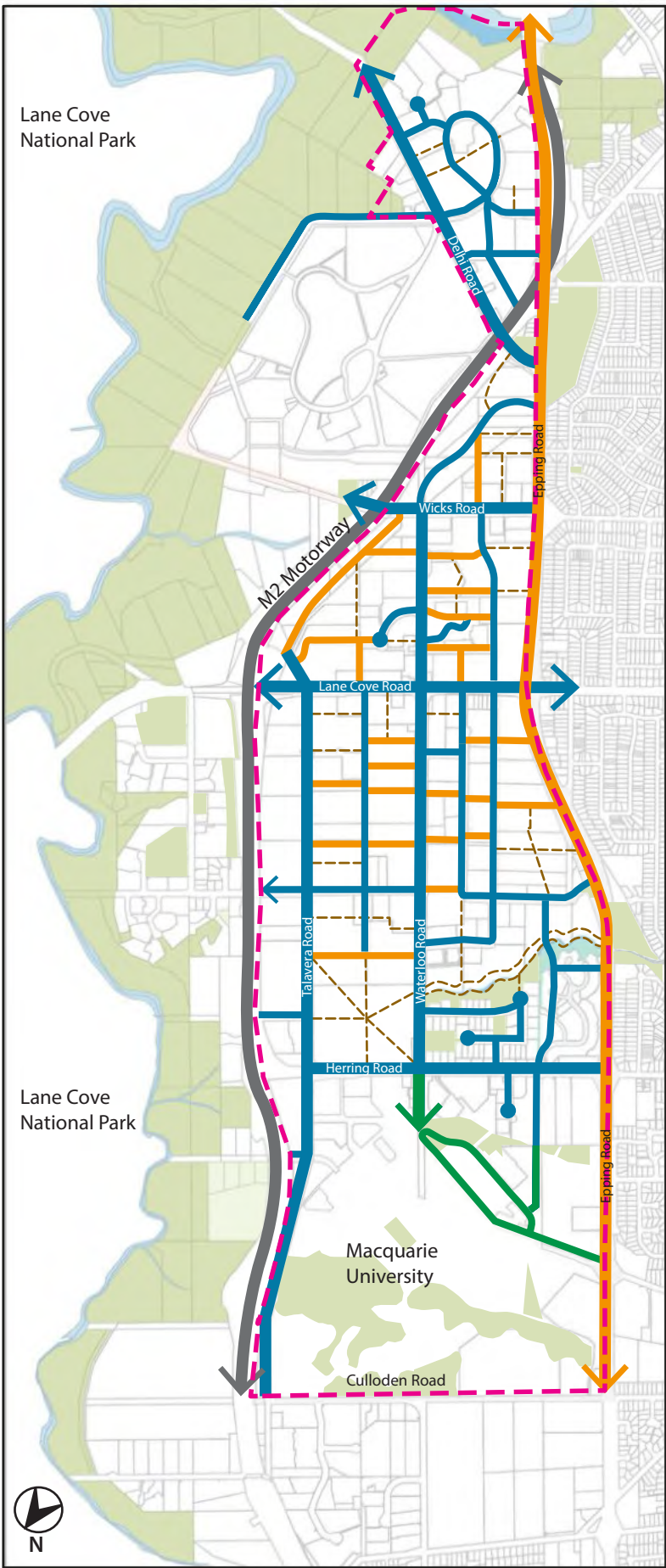
#### Strategy

The overall strategy is to deliver an enhanced street lighting network for the Macquarie Park Corridor through the development process and City of Ryde's program of works.

Street lighting on multi-function poles (MFPs) must be installed as part of public domain works required for developments within the corridor. Following the final completion of the development and the public domain works, Council takes over the ownership and long-term maintenance responsibility of the street lighting.

Please refer to Figure 3.5.1 for the Street Lighting Network Plan and the *Design Guide for Council-owned Street Lighting* for further details.

**Note:** Certain developments that propose street lighting designs that vary from the Street Lighting Network Plan due to site-specific constraints and/or road configurations must be referred to Manager Asset Systems.



LEGEND

- M2
- Staggered multi-function poles on both sides (refer to the Street Lighting Schema for the Macquarie Park Business Area)
- Multi-function poles on one side (refer to the Street Lighting Schema for the Macquarie Park Business Area)
- Standard pedestrian lights
- Macquarie University roads (refer to Macquarie University Masterplan)
- Macquarie Park Corridor Boundary

Note: Street lighting design and specifications are subject to Council's approval. Reference should also be made to the *Design Guide for Council-owned Street Lighting*.

Figure 3.5.1 Macquarie Park Corridor Street Lighting Plan

## 4.0 OPEN SPACE NETWORK

### 4.1 Open Space Network - Function and Character

#### Objectives

- Provide a well-connected public open space network with a range of open space typologies to accommodate active and passive recreational uses
- Create bio-links and canopy connections to existing vegetation communities and contribute to stormwater and ecological management
- Maximise the accessibility of useable open space and contribute to the pedestrian and cycle network

#### Strategy

A new open space network will be provided by identifying new public space and augmenting existing public open spaces within the corridor. The new open space network will comprise of a range of typologies such as multi-function parklands, informal gathering and recreational areas, urban plazas, and rehabilitated and new vegetation reserves that are integrated with the access network to maximise pedestrian and cycle access.

Table 4.1.1 Proposed New Open Space and Desired Function and Character

Park Name/Address	Minimum Area/ Dimensions	Function and Indicative Character
Central Park (43-61 Waterloo Road)	1 hectare 75 x 100 metres (if dimensions are altered, a minimum of 65 metres is required in any direction)	Multi-function Active recreation (informal sport) Passive recreation Community events (cinemas, expos, etc.) Children's play
Shrimptons Creek Parklands and Riparian Corridor	3.8 hectares Dimensions vary	Multi-function Active transport (fitness trail, walking, and cycling) Active recreation (informal sport and skateboarding) Passive recreation Children's play Riparian Corridor/ native habitat
Riverside Park (Julius Avenue)	0.35 hectare 60 x 60 metres	Multi-function Passive recreation Children's play Outdoor dining Neighbourhood meeting place
Thomas Holt Park (Thomas Holt Drive)	0.40 hectare approx. 100 x 40 metres	Multi-function Passive recreation Children's play Neighbourhood meeting place





LEGEND

- Existing Open Space
- Proposed Open Space
- Core Riparian Corridor (20 metres from either side of the creek)
- Proposed Urban Plaza
- Macquarie Park Corridor Boundary

Proposed Open Space

- ① Riverside Park
- ② North Ryde Station Urban Plaza
- ③ North Ryde Station Precinct Park
- ④ Thomas Holt Drive Park
- ⑤ Macquarie Park Station Urban Plaza
- ⑥ Central Park
- ⑦ Shrimptons Creek Core Riparian Corridor
- ⑧ Shrimptons Creek Park
- ⑨ Macquarie University Station Urban Plaza
- ⑩ Macquarie University Station Precinct Urban Plaza

Figure 4.1.1 Macquarie Park Corridor Open Space Network Plan

## 4.2 Open Space Network - Paving and Finishes

### Objectives

- Create a public domain that is visually seamless and unified
- Provide a variety of open space typologies with appropriate facilities
- Utilise robust and durable materials with a consistent colour and finish to create a visually consistent ground plane
- Achieve environmental objectives in accordance with the RDCP 2014

### Strategy

Finishes in parks and public open spaces should be consistent with streetscape materials. Continuity in materials is critical for internal park pathways that connect to external footpaths. Specific treatments for public open space can be incorporated using porous and permeable materials where possible, with an emphasis on durability and simplicity.

**Note:** The final designs including paving treatment, finishes, and fixtures for new open space are subject to Council's approval (Manager Strategic City and/or Manager Asset Systems) and should be consistent with the provisions of the RDCP 2014 as indicated below.

Table 4.2.1 New Open Space Paving and Finishes Key Specifications

Park Name/Address	Minimum fixtures to be provided	Paving treatment for internal park pathways
Central Park (43-61 Waterloo Road)	10 x park benches; 10 x bicycle parking spaces	Full granite paving
Shrimptons Creek Parklands and Core Riparian Corridor	6 x park benches; 6 x bicycle parking spaces; and pedestrian footbridge across Shrimptons Creek to improve connectivity between the existing residential precincts and Lyon Park Road	Concrete footpath with granite banding; full granite at key locations and street frontages
Riverside Park (Julius Avenue)	6 x park benches; 6 x bicycle parking spaces	Concrete footpath with granite banding; full granite at key locations and street frontages
Thomas Holt Park (Thomas Holt Drive)	6 x park benches; 6 x bicycle parking spaces; and landscaping that responds to natural topography of the park (e.g. terraced seating, etc.)	Concrete footpath with granite banding; full granite at key locations and street frontages

## 4.3 Open Space Network - Trees and Landscaping

### Objectives

- Reflect and respond to the local environment through the use of endemic species
- Utilise species that are of an appropriate scale and type and that respond to the local context
- Recognise the role of streets in habitat creation and provide bio-links and connections
- Utilise native and exotic species that are of an appropriate scale and type to reinforce the street hierarchy and provide colour and variety to the streetscape

### Strategy

The overall strategy for trees and landscaping in public open spaces promotes a vegetation palette that provides a “reading” of the underlying geology and topography. This strategy assists in minimising weed infestation and allows for the creation of bio-links and canopy connections to Lane Cove National Park and existing vegetation communities.

Table 4.3.1 Recommended Trees Schedule for Open Space

Recommended Trees	
Sydney Turpentine-Ironbark Forest (for bushlands and corridors within parks)	
<i>Angophora costata</i> (Sydney Red Gum); <i>Angophora floribunda</i> (Rough-barked Apple); <i>Corymbia gummifera</i> (Red Bloodwood); <i>Corymbia maculata</i> (Spotted Gum);	<i>Eucalyptus globoidea</i> (White Stringybark); <i>Eucalyptus saligna</i> (Sydney Blue Gum); <i>Syncarpia glomulifera</i> (Turpentine); <i>Tristaniopsis laurina</i> (Water Gum)
Native Park Tree Species	
<i>Angophora costata</i> (Sydney Red Gum); <i>Angophora floribunda</i> (Rough-barked Apple); <i>Buckinghamia celsissima</i> (Ivory Curl); <i>Corymbia gummifera</i> (Red Bloodwood); <i>Corymbia maculata</i> (Spotted Gum); <i>Eleocarpus eumundii</i> (Smooth Quandong); <i>Eleocarpus reticulatus</i> (Blueberry Ash); <i>Eucalyptus botryoides</i> (Swamp Mahogany); <i>Eucalyptus acmenoides</i> (White Mahogany); <i>Eucalyptus pilularis</i> (Blackbutt); <i>Eucalyptus piperita</i> (Sydney Peppermint);	<i>Eucalyptus saligna</i> (Sydney Blue Gum); <i>Eucalyptus resinifera</i> (Red Mahogany); <i>Eucalyptus tereticornis</i> (Forest Red Gum); <i>Flindersia australis</i> (Crow's Ash); <i>Harpullia pendula</i> (Tulipwood); <i>Melaleuca linariifolia</i> (Flax-leaved Paperbark); <i>Pittosporum revolutum</i> (Yellow Pittosporum); <i>Syncarpia glomulifera</i> (Turpentine); <i>Tristaniopsis laurina</i> (Water Gum); <i>Tristaniopsis laurina</i> ('Luscious' Water Gum); <i>Waterhousia floribunda</i> (Weeping Lilly Pilly)
Broad Canopy Trees (for shade and as specimen species)	
<i>Ficus rubiginosa</i> (Port Jackson Fig); <i>Platanus digitalis</i> (Plane Tree); <i>Ulmus parvifolia</i> (Chinese Elm)	



Table 4.3.2 below indicates the landscaping specifications for new public open space identified in the *Part 4.5 Macquarie Park Corridor* of the RDCP 2014.

Table 4.3.2 New Open Space Tree and Landscaping Palette

Park Name/Address	Landscaping to be provided	Tree and Landscaping palette
Central Park (43-61 Waterloo Road)	Turf detention basin to minimum 50% of park area as the Central Park is on the overland flow alignment; subject to Council's approval	Refer to Table 4.3.1
Shrimptons Creek Parklands and Core Riparian Corridor	Continuous core riparian zone (20 metres wide measured either side of the creek); subject to Council's approval	Only endemic species may be used in the core riparian zone  Refer to Table 4.3.1 and NSW Office of Water's <i>Guidelines for Riparian Corridors on Waterfront Land</i> (2012)
Riverside Park (Julius Avenue)	Subject to design and Council's approval	Refer to Table 4.3.1
Thomas Holt Park (Thomas Holt Drive)	Subject to design and Council's approval	Refer to Table 4.3.1

## 4.4 Open Space Network - Rail Station Plazas

### Objectives

- Support the provision of rail station plazas with active building frontages and clear unimpeded views and access
- Utilise robust and durable materials with a consistent colour and finish to create a visually unified and seamless public domain
- Provide wayfinding and reinforce a sense of safety for the community by providing appropriate lighting and directional signage
- Provide sufficient furniture such as bins, seats, lighting, and bicycle parking in appropriate locations

### Strategy

The overall strategy is to integrate rail station plazas with the streetscape and other elements of the public domain by providing appropriate responses to level changes and continuity in materials. This would assist in promoting good design that provides opportunities for outdoor dining and social interaction and ensuring amenity and access for people of all ages and abilities.

**Note:** The final designs including paving treatment, finishes, and fixtures for new open space are subject to Council's approval (Manager Strategic City) and should be consistent with the provisions of the RDCP 2014 as indicated below.

Table 4.4.1 Rail Station Plazas Minimum Required Fixtures

Rail Station Plaza	Area/ Dimensions	Minimum fixtures to be provided
Macquarie Park Station Plaza (West)	0.35 hectare min. 88 x 40 metres	10 x park benches; 10 x bicycle parking spaces
Macquarie Park Station Plaza (East)	0.35 hectare min. 88 x 40 metres	Minimum 10 x park benches; 10 x bicycle parking spaces
Macquarie University Station Plaza (East)	0.67 hectare min. 80 x 80 metres	Minimum 10 x park benches; 10 x bicycle parking spaces

Detailed descriptions of paving types and finishes for rail station plazas are provided below:

Table 4.4.2 Rail Station Plazas Paving and Finishes Specifications

Elements	Specifications
Paving Treatment	Full-granite equal to black - flame exfoliated 300 mm x 300 mm x 60 mm
Lighting	Subject to design and Council's approval
Seats	Botton and Gardiner Urban Seat (US11ALX) Finish: Cast aluminium; powder coat
Bike Rack	Model: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40NB(48.26) x 2.77mm Grade 304 Stainless Steel Pipe/ Linished

Figure 4.4.1 Rail Station/Urban Plazas Precedent Images



New York, New York, USA - Bryant Park (42nd Street and Fifth Avenue Subway Station)



Melbourne, VIC - Federation Square (Flinders Street Station)



Detroit, Colorado, USA - Wynkoop Plaza (Union Station)



Perth, WA - City Square (The Esplanade)



## 5.0 ART IN PUBLIC SPACES

Art can make urban spaces attractive and welcoming, promote local identity, evoke business confidence, and attract investment. Artworks can enhance streetscapes and other spaces by creating interesting and distinctive urban environments, assisting navigation and wayfinding, and attracting pedestrian activity. High-quality and accessible public artworks can also be a destination in itself and create vital and engaging spaces that connect the private and public domains.

Artworks in public spaces within Macquarie Park can serve the following purposes:

- Create emblems or symbols that depict the identity of the place;
- Establish a strong identity or signature to distinguish a special place or site;
- Signify entry points;
- Strengthen the character of the place; and
- Stimulate the interaction of ideas that is central to the vision for the Corridor.

### Objectives

- a. Include site-specific in the public domain in the Macquarie Park Corridor
- b. To create a distinctive urban environment and sense of place
- c. To reflect Macquarie Park's local character, cultural identity, and natural environment
- d. To create spaces (whether publicly or privately owned), which incorporate art that is at the forefront of new ideas, original, creative, and innovative in its design and use of form, technique, and materials.

### Strategy

- a. Art must be publicly accessible (i.e. in locations where it can be viewed or experienced from the public domain).
- b. Art must be integrated into the landscape and other elements of the site and public domain (including paving, lighting design, tree planting and landscaping, outdoor seating, play equipment, etc).
- c. Art in publicly accessible places must have regard to public safety and use materials that are robust, durable, and low maintenance.
- d. The following themes provide a spectrum of ideas that can be explored and expressed through public art, arising from the Corridor's history and desired future character:
  - Innovation and technology
  - Transport (train, bus, or car) and people movement
  - Macquarie Park's landscape heritage (e.g. market gardens and orchards)
  - Future of Macquarie Park
  - Natural Environment (e.g. water theme relating to the the Corridor's creeks and riparian corridors).
- e. Reference should be made to City of Ryde's *Public Art Policy*.

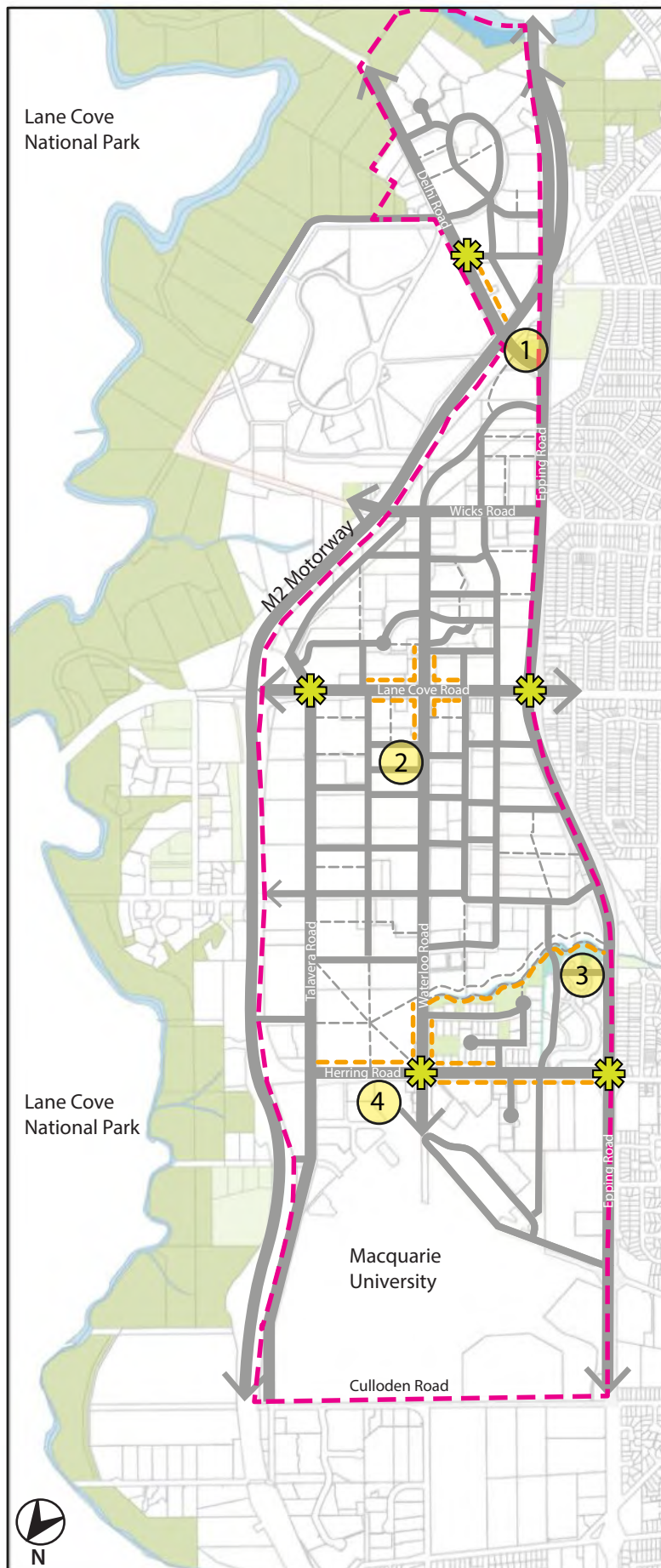


Figure 5.0.1 Macquarie Park Corridor Public Art Plan

## ART IN PUBLIC SPACES

### ICONIC ARTWORKS (approximate locations)

- ① Delhi Road
- ② Central Park
- ③ Shrimptons Creek Parkland
- ④ Herring Road Urban Plaza
- ✱ Medium-sized Feature Artworks at nodal points
- Intimate scale artworks at high pedestrian activity areas

#### Iconic

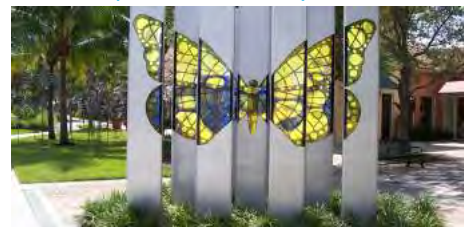


Munich, Germany - Olafur Eliasson



Ryde Wharf Reserve, Ryde NSW - Jane Cavanaugh

#### Feature (medium-sized)



Palm Beach, Florida, USA - Hilton Garden Inn

#### Intimate scale



Wuganmagulya Farm Cove Sydney Sculpture Walk, Royal Botanic Gardens, Sydney NSW - Brenda L.

## 6.0 TYPICAL LAYOUTS

This section provides guidelines for the correct placement and configuration of public domain elements such as street trees, furniture, lighting, and footpaths in the access network.

*Note: Street tree spacing, street lighting location and design, and paving layout may vary depending on site-specific constraints and are subject to Council's approval.*

### 6.1 New 20-metre wide streets

The following typical specifications apply to new 20-metre wide streets within the Corridor as indicated in Fig. 3.1.1 *Macquarie Park Corridor Road Types Plan*.

**Table 6.1.1 Typical Specifications for new 20-metre wide streets**

Elements	Typical specification
<b>Footpath</b>	
Paved footpath	4.5 metres on both sides (Refer to typical layout diagram)
Materials	Granite paving equal to black - flame exfoliated 600 mm x 300 mm x 60 mm (Refer to Fig. 3.2.1 <i>Macquarie Park Corridor Paving Plan</i> )
Kerb	Integral cast-in-situ concrete kerb and gutter; kerb ramp to be designed in accordance with desire lines, safety, and conflict zones (subject to Council's approval)
Driveway	Granite paving equal to black - flame exfoliated 150 mm x 100 mm x 60 mm
<b>Street Trees</b>	
Layout	In accordance with typical layout diagram
Spacing	Trees planted in 1.5 x 3 m pits spaced approximately 8 metres apart; trees planted in 2.5 x 3 m kerb blisters spaced approximately 24 metres apart (subject to lighting design and other site-specific constraints)
<b>Street Furniture</b>	
Seats	Typically 1 per city block and at certain locations such as near train stations, bus stops, and/or corners. Specification: Botton and Gardiner Urban Seat (US11ALX) Finish: Cast aluminium; powder coat
Bins	In regular intervals, typically 1 per city block and at certain locations such as near train stations, bus stops, and outside food shops. Specification: City of Ryde model dual rubbish bins with butt bin Dimensions: 2 x 120 L; 15 mm perforations Finish: Marine-grade stainless steel; mounts to Council specification
Bollards	Installed as necessary Specification: LEDA SSP80F Fixed Bollard 80NB (900mm high) Finish: Linished or brushed Grade 304 Stainless Steel
Bike rack	2 per city block located close to intersections Specification: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40 NB(48.26) x 2.77 mm Grade 304 Stainless Steel Pipe/Linished
<b>Street Lighting</b>	
Layout	Spaced 40 metres apart; staggered configuration (subject to design)
Multi-function light pole	12 metres in height; refer to <i>Design Guide for Council-owned Street Lighting</i> for specification



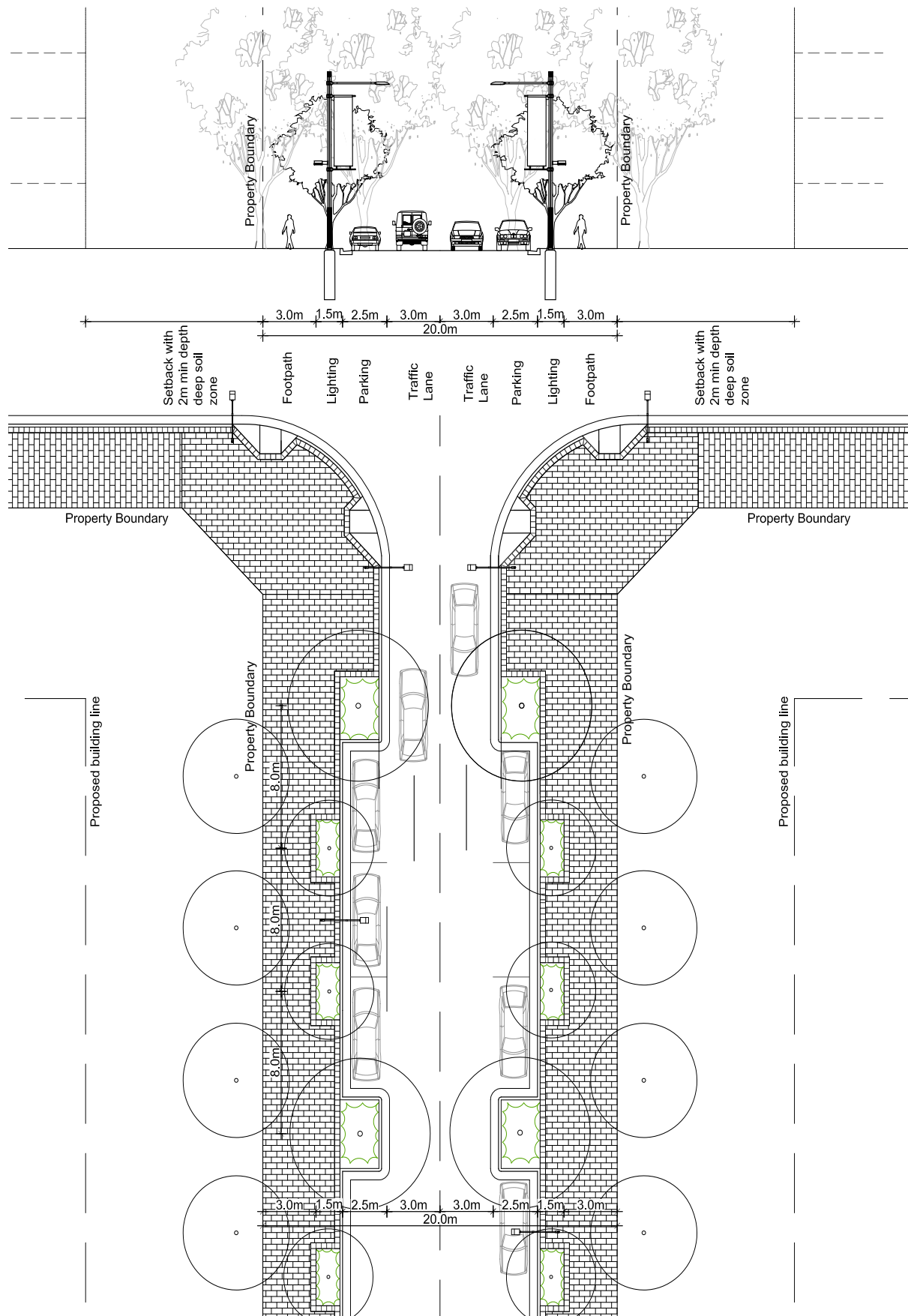


Figure 6.1.1 Typical Layout - New 20-metre wide Streets (NOT TO SCALE)

## 6.2 New 16-metre wide streets

The following typical specifications apply to new 16-metre wide streets within the Corridor as indicated in Fig. 3.1.1 *Macquarie Park Corridor Road Types Plan*.

Table 6.2.1 Typical Specifications for new 16-metre wide streets

Elements	Typical Specifications
<b>Footpath</b>	
Paved footpath	Full width: 2.5 metres on one side and 3.5 metres on the other side (Refer to typical layout diagram)
Materials	Granite paving equal to black - flame exfoliated 600 mm x 300 mm x 60 mm (Refer to Fig. 3.2.1 <i>Macquarie Park Corridor Paving Plan</i> )
Kerb	Integral cast-in-situ concrete kerb and gutter; kerb ramp to be designed in accordance with desire lines, safety, and conflict zones (subject to Council's approval)
Driveway	Granite paving equal to black - flame exfoliated 150 mm x 100 mm x 60 mm
<b>Street Trees</b>	
Layout	In accordance with typical layout diagram
Spacing	Trees planted in 2.5 x 3 m kerb blisters spaced approximately 16 metres apart (subject to lighting design and other site-specific constraints)
<b>Street Furniture</b>	
Bollards	Installed as necessary Specification: LEDA SSP80F Fixed Bollard 80NB (900mm high) Finish: Linished or brushed Grade 304 Stainless Steel
Bike rack	2 per city block located close to intersections Specification: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40NB(48.26) x 2.77mm Grade 304 Stainless Steel Pipe/Linished
<b>Street Lighting</b>	
Layout	Spaced 40 metres apart; staggered configuration (subject to design)
Multi-function light pole	9 metres in height; refer to <i>Design Guide for Council-owned Street Lighting</i> for specification

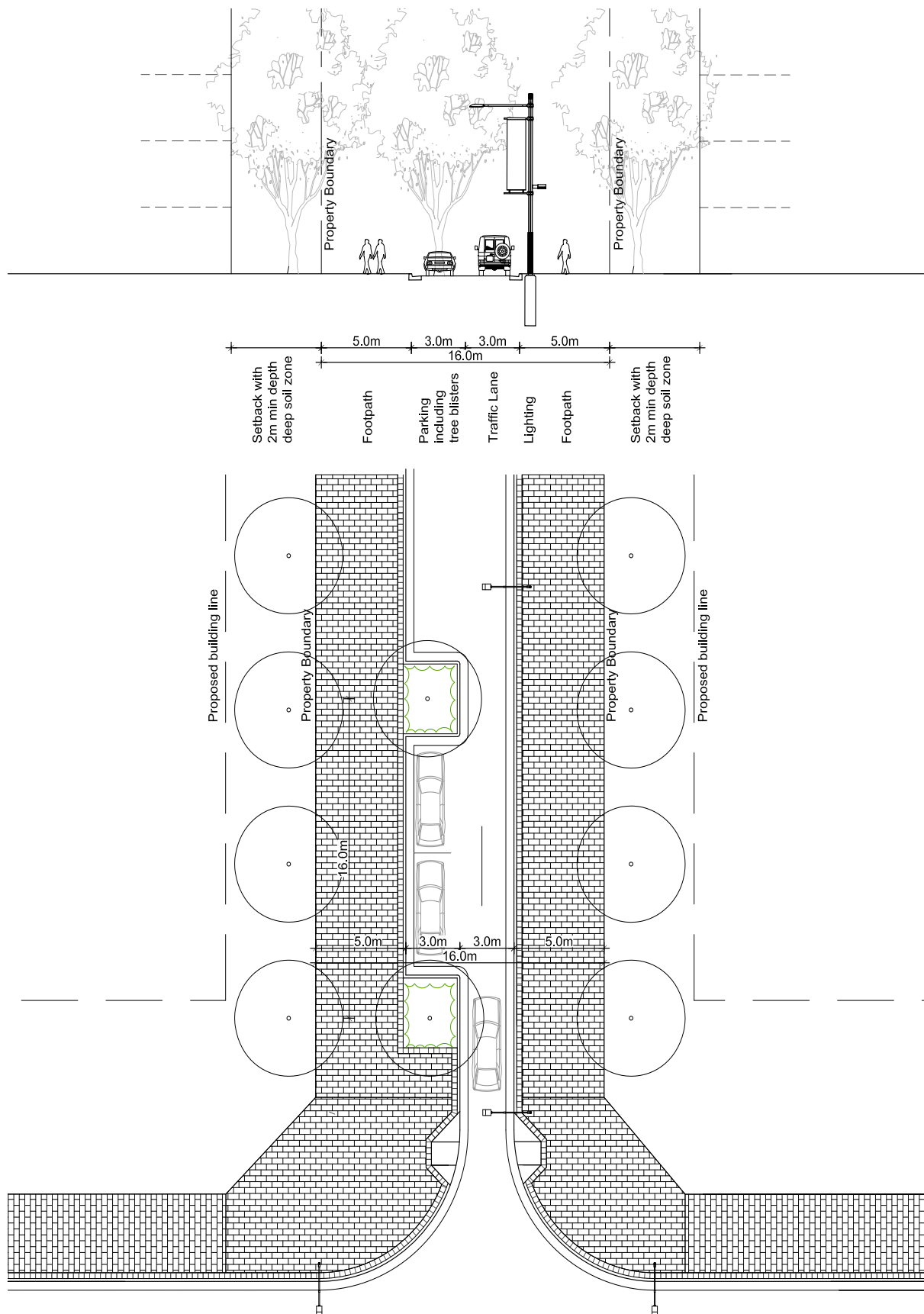


Figure 6.2.1 Typical Layout - New 16-metre wide Streets (NOT TO SCALE)



### 6.3 New 14.5-metre wide streets

The following typical specifications apply to new 14.5-metre wide streets within the Corridor as indicated in Fig. 3.1.1 *Macquarie Park Corridor Road Types Plan*.

Table 6.3.1 Typical Specifications for new 14.5-metre wide streets

Elements	Typical Specifications
<b>Footpath</b>	
Paved footpath	Full width: 2.5 metres on one side and 3.5 metres on the other side (Refer to typical layout diagram)
Materials	Granite paving equal to black - flame exfoliated 600 mm x 300 mm x 60 mm (Refer to Fig. 3.2.1 <i>Macquarie Park Corridor Paving Plan</i> )
Kerb	Integral cast-in-situ concrete kerb and gutter; kerb ramp to be designed in accordance with desire lines, safety, and conflict zones (subject to Council's approval)
Driveway	Granite paving equal to black - flame exfoliated 150 mm x 100 mm x 60 mm
<b>Street Trees</b>	
Layout	In accordance with typical layout diagram
Spacing	Trees planted in 1.5 x 3 m pits spaced approximately 8 metres apart; trees planted in 2.5 x 3 m kerb blisters spaced approximately 16 metres apart (subject to lighting design and other site-specific constraints)
<b>Street Furniture</b>	
Bollards	Installed as necessary Specification: LEDA SSP80F Fixed Bollard 80NB (900mm high) Finish: Linished or brushed Grade 304 Stainless Steel
Bike rack	2 per city block located close to intersections Specification: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40NB(48.26) x 2.77mm Grade 304 Stainless Steel Pipe/Linished
<b>Street Lighting</b>	
Layout	Spaced 40 metres apart; staggered configuration (subject to design)
Multi-function light pole	9 metres in height; refer to <i>Design Guide for Council-owned Street Lighting</i> for specification

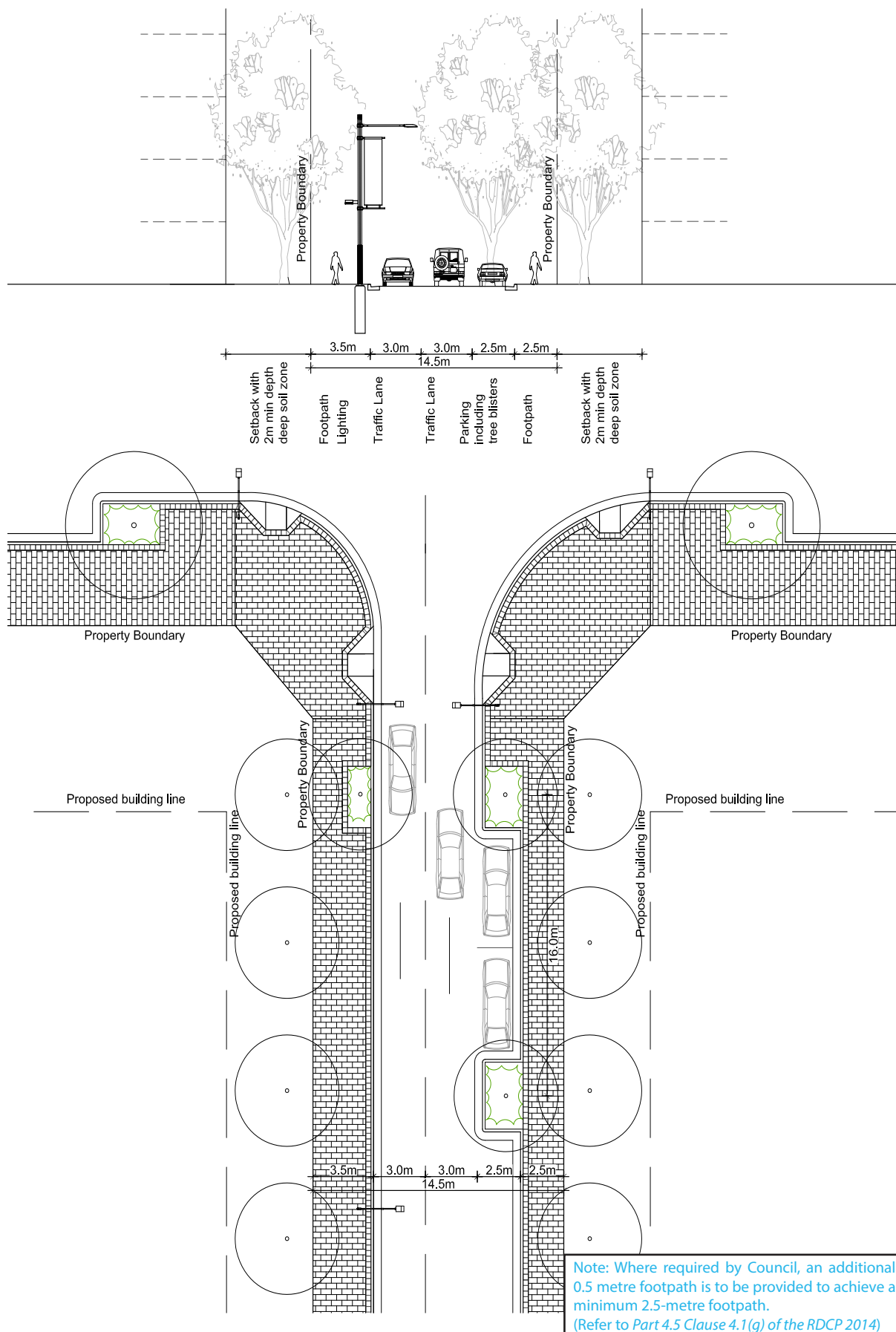


Figure 6.3.1 Typical Layout - New 14.5-metre wide Streets (NOT TO SCALE)

6.4 Intersections

The following diagrams show typical layouts for intersections of various existing and new street typologies within the Corridor, as indicated in Fig. 3.1.1 *Macquarie Park Corridor Road Types Plan*.

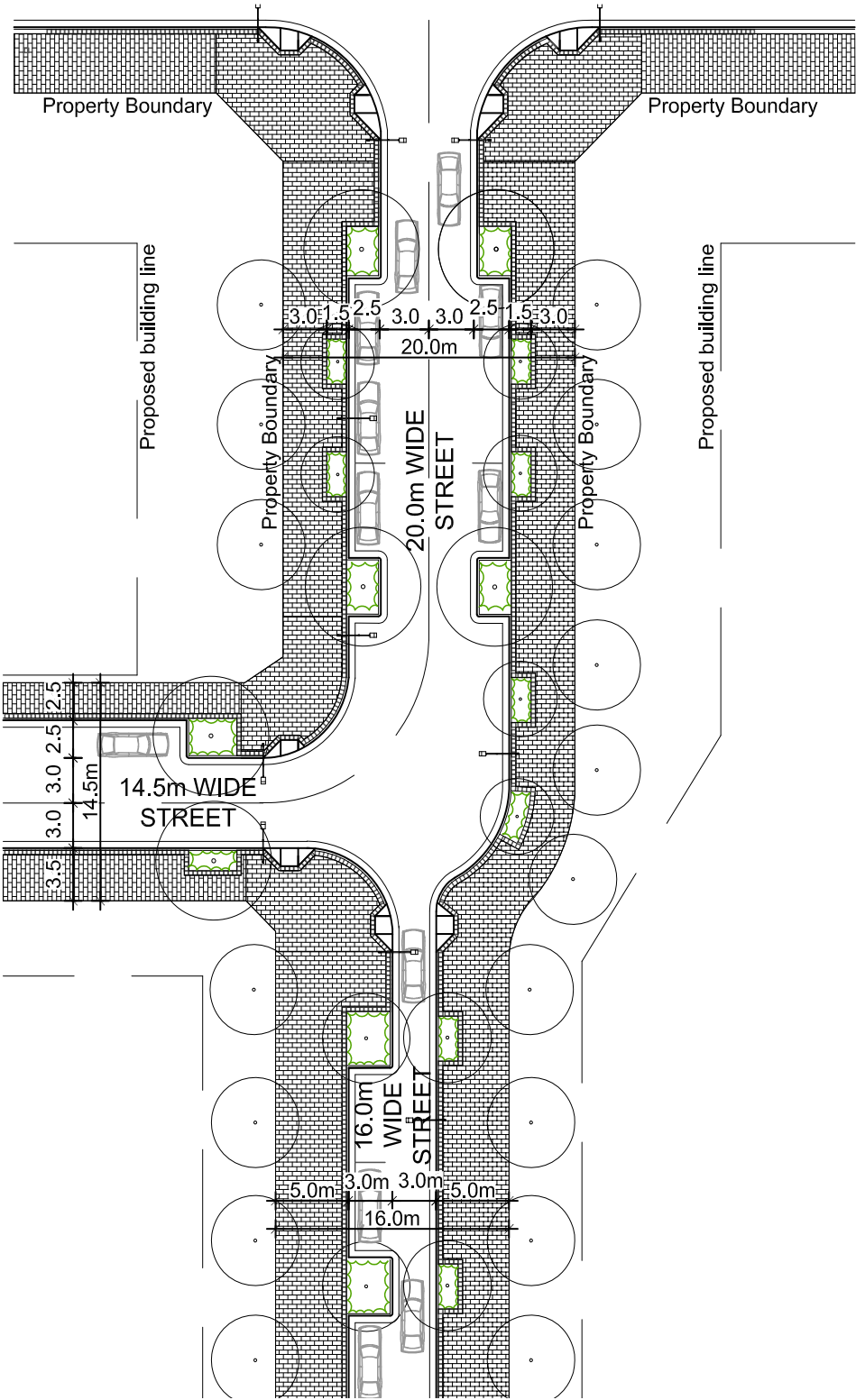


Figure 6.4.1 New 20-metre, 16-metre, and 14.5-metre wide street intersection (NOT TO SCALE)



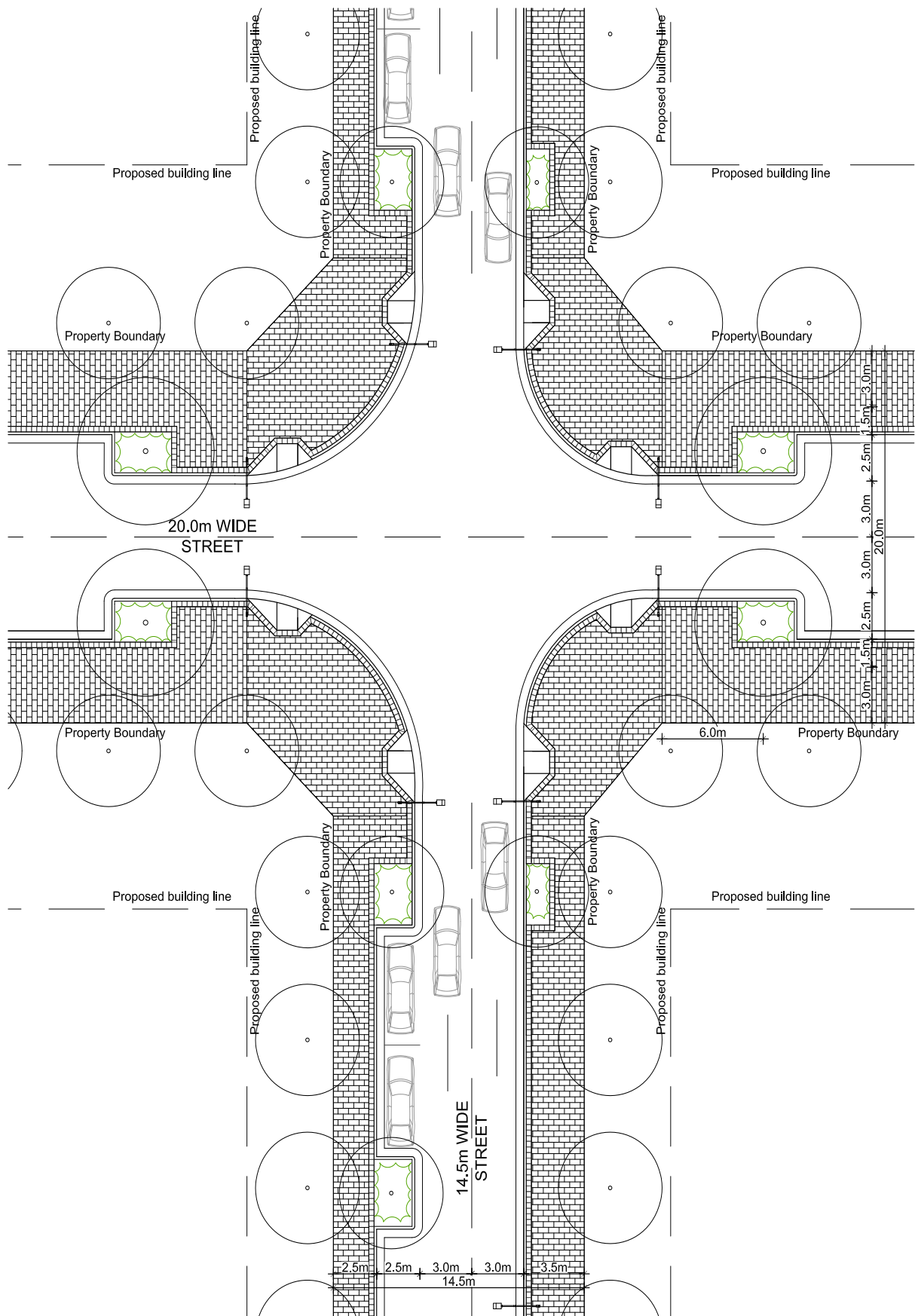


Figure 6.4.2 Typical Layout - 14.5-metre and 20-metre wide street intersection (NOT TO SCALE)

## 6.5 New Pedestrian Ways

The following typical specifications apply to new pedestrian ways within the Corridor as indicated in Fig. 3.1.1 *Macquarie Park Corridor Road Types Plan*.

**Note:** The overall width of the pedestrian way is approximately 8 metres, in accordance with Safer by Design Principles.

Table 6.5.1 Typical Specifications for new pedestrian ways

Elements	Typical Specifications
<b>Footpath</b>	
Paving width	4 metres
Materials	Concrete paving with granite banding equal to black - flame exfoliated (every 6 metres) 600 mm x 300 mm x 60 mm (Refer to Fig. 3.2.1 <i>Macquarie Park Corridor Paving Plan</i> )
<b>Landscaping</b>	
Trees	Planted in 2-metre wide soft landscaping on either side of the path spaced approximately 4 metres apart (subject to lighting design and other site-specific constraints)
<b>Street Furniture</b>	
Bollards	Installed as necessary Specification: LEDA SSP80F Fixed Bollard 80NB (900mm high) Finish: Linished or brushed Grade 304 Stainless Steel
Bike rack	2 per city block located close to intersections Specification: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40NB(48.26) x 2.77mm Grade 304 Stainless Steel Pipe/Linished
<b>Street Lighting</b>	
Layout	Aligned and evenly spaced along length of pedestrian way if possible (subject to design and Council's approval)
Light Pole	5 metres in height; specification subject to Council's approval

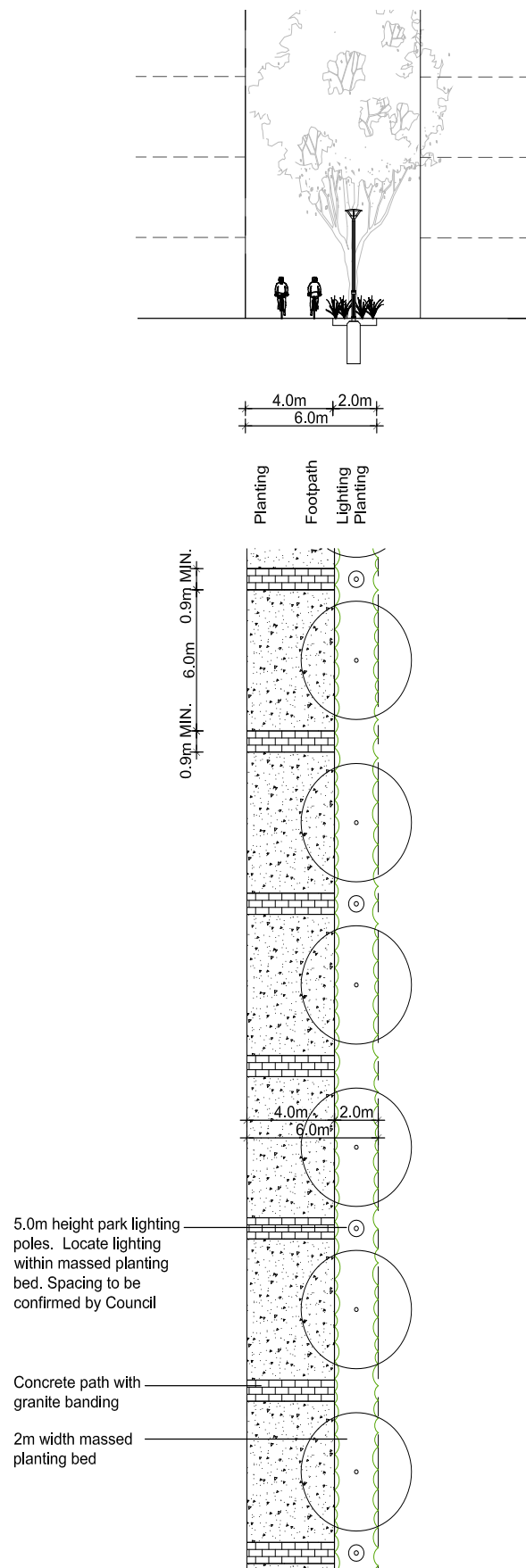


Figure 6.5.1 Typical Layout - New Pedestrian Ways (NOT TO SCALE)



### 6.6.1 Existing Streets - Delhi Road and Epping Road

Epping Road and Delhi Road are characterised by very wide verges in some locations and are both part of regional cycle routes. A consistent footpath width of at least 2.4 metres is required, with wider paved areas at bus stops and intersections in order to accommodate commuters and pedestrians waiting for traffic signals. This will also provide sufficient space for street furniture, kerb ramps, lighting, and bicycle racks.

Table 6.6.1.1 Typical Specifications for Delhi Road and Epping Road

Elements	Typical Specifications
<b>Footpath</b>	
Paved footpath	Full width at intersections and bus stops; 2.4-metre wide paving
Materials	Granite paving equal to black - flame exfoliated 600 mm x 300 mm x 60 mm (Refer to Fig. 3.2.1 <i>Macquarie Park Corridor Paving Plan</i> )
Kerb	Integral cast-in-situ concrete kerb and gutter; kerb ramp to be designed in accordance with desire lines, safety, and conflict zones (subject to Council's approval)
Driveway	Granite paving equal to black - flame exfoliated 150 mm x 100 mm x 60 mm
<b>Landscaping</b>	
Street Trees	<p><u>Epping Road</u> Native grass species in nature strip and trees within the 1.5-metre private property setback</p> <p><u>Delhi Road</u> Trees planted approximately 4 metres apart in continuous garden beds</p> <p>(Refer to Fig. 3.4.1 <i>Macquarie Park Corridor Street Trees Plan</i> and typical layout diagram)</p>
Nature Strip/ Garden beds	Native grass species such as <i>Lomandra Longifolia</i> ('Tanika' Spiny-head Mat Rush) or <i>Dianella caerulea</i> (Blueberry lily) (subject to Council and RMS approval)
<b>Street Furniture</b>	
Bollards	Installed as necessary Specification: LEDA SSP80F Fixed Bollard 80NB (900mm high) Finish: Linished or brushed Grade 304 Stainless Steel
Bike rack	2 per city block located close to intersections Specification: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40NB(48.26) x 2.77mm Grade 304 Stainless Steel Pipe/Linished
<b>Street Lighting</b>	
Layout	Refer to <i>Street Lighting Schema for the Macquarie Park Business Area</i> (to be confirmed by Council)
Multi-function light pole	12 metres in height; refer to <i>Design Guide for Council-owned Street Lighting</i> for specification

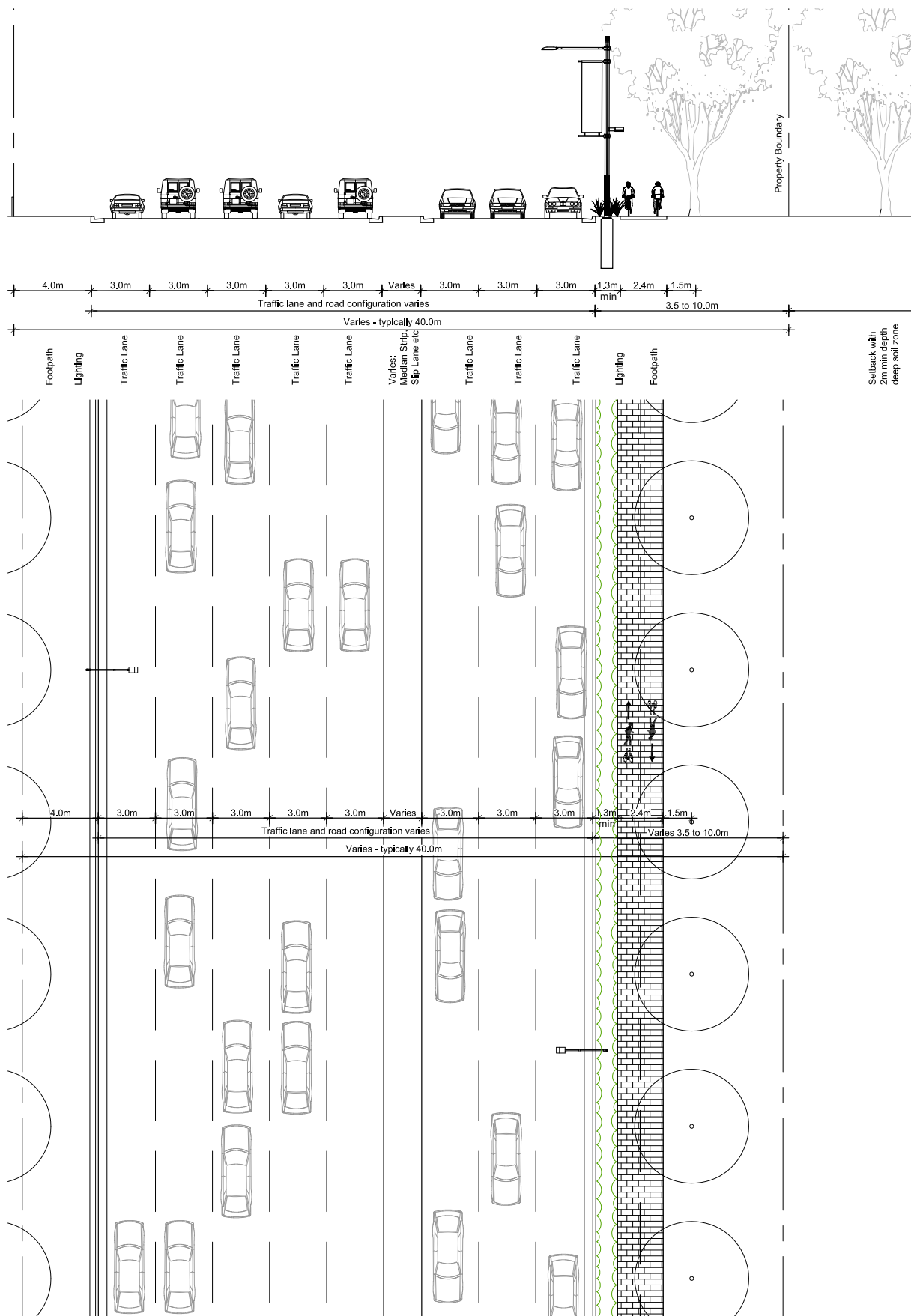


Figure 6.6.1.1 Epping Road Layout (northern side) - typically 40 metres wide (NOT TO SCALE)

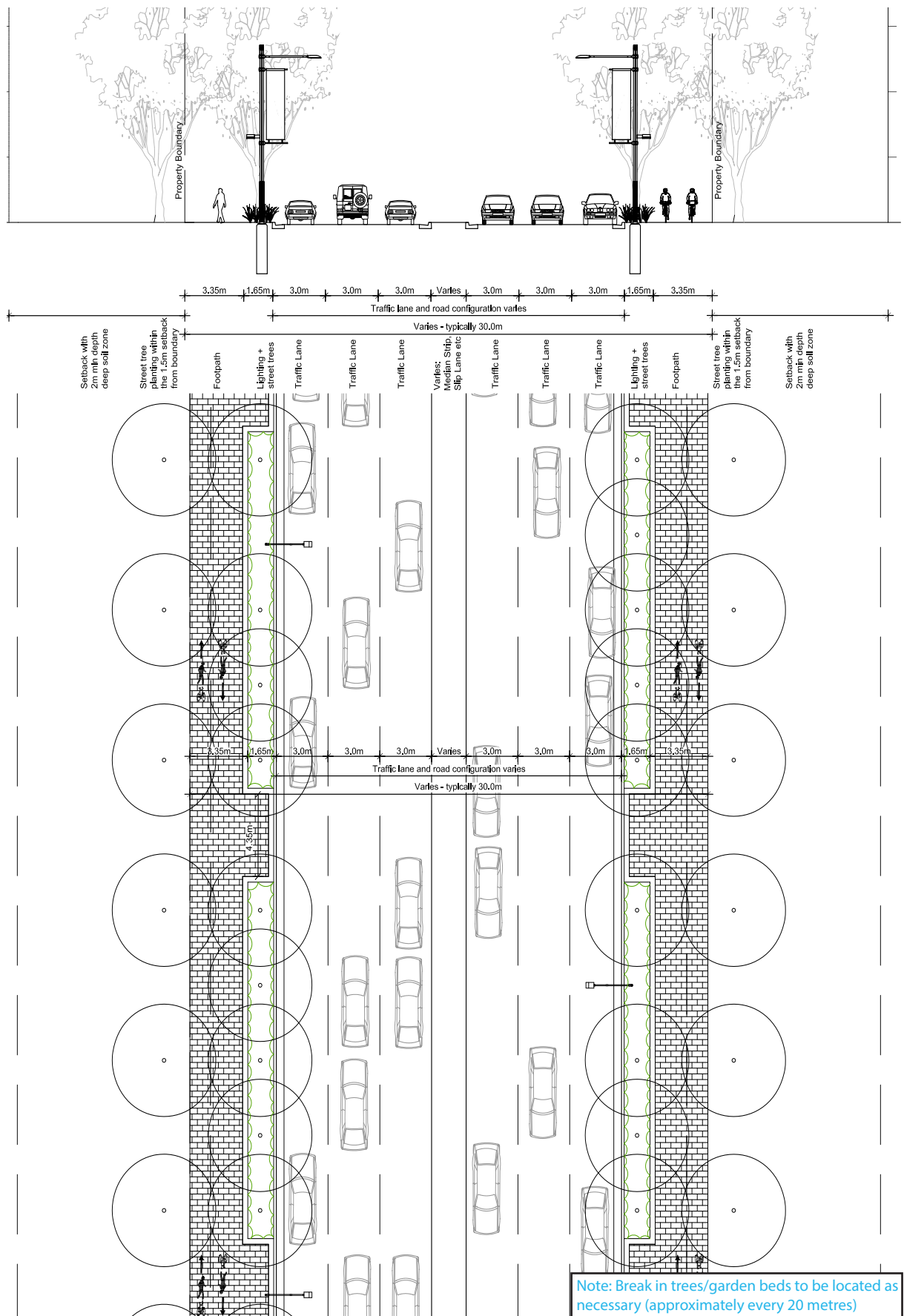


Figure 6.6.1.2 Delhi Road Layout - typically 30 metres wide (NOT TO SCALE)



## 6.6.2 Existing Streets - Mixed Use, Commercial, or Business Character

The following typical specifications apply to existing streets in the Mixed Use, Commercial Core, and Business Park zones within the Corridor.

Table 6.6.2.1 Typical Specifications for Existing Mixed Use/Commercial/Business Character Streets

Elements	Typical Specifications
<b>Footpath</b>	
Paved footpath	Full width: varies between approximately 2.4 to 5 metres
Materials	Granite paving equal to black - flame exfoliated 600 mm x 300 mm x 60 mm (Refer to Fig. 3.2.1 <i>Macquarie Park Corridor Paving Plan</i> )
Kerb	Integral cast-in-situ concrete kerb and gutter; kerb ramp to be designed in accordance with desire lines, safety, and conflict zones (subject to Council's approval)
Driveway	Granite paving equal to black - flame exfoliated 150 mm x 100 mm x 60 mm
<b>Street Trees and Landscaping</b>	
Herring Road	Trees planted in nature strip (approximately 8 metres apart ) with native grass understorey such as <i>Lomandra Longifolia</i> ('Tanika' Spiny-head Mat Rush) or <i>Dianella caerulea</i> (Blueberry lily) (subject to lighting design and Council's approval)
Lane Cove Road	Trees within 1.5 metre private property setback only (subject to lighting design and other site-specific constraints)
Waterloo/ Secondary Roads	Trees planted in 1.5 x 3 m pits spaced approximately 8 metres apart (subject to lighting design and other site-specific constraints)
Tertiary Roads	Trees planted in nature strip (approximately 8 metres apart ) with native grass understorey (subject to lighting design and Council's approval)
<b>Street Furniture</b>	
Seats	1 per city block; located near a point of interest Specification: Botton and Gardiner Urban Seat (US11ALX) Finish: Cast aluminium; powder coat
Bins	In regular intervals, typically 1 per city block and at certain locations such as outside train stations, bus stops, and outside shops (particularly food shops). Specification: City of Ryde model dual rubbish bins with butt bin Dimensions: 2 x 120 L; 15 mm perforations Finish: Marine-grade stainless steel; mounts to Council specification
Bollards	Installed as necessary Specification: LEDA SSP80F Fixed Bollard 80NB (900mm high) Finish: Linished or brushed Grade 304 Stainless Steel
Bike rack	2 per city block located close to intersections or bus stops Specification: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40NB(48.26) x 2.77mm Grade 304 Stainless Steel Pipe/Linished
<b>Street Lighting</b>	
Layout	Spaced 40 metres apart; staggered configuration (subject to design)
Multi-function light pole	12 metres in height; refer to <i>Design Guide for Council-owned Street Lighting</i> for specification

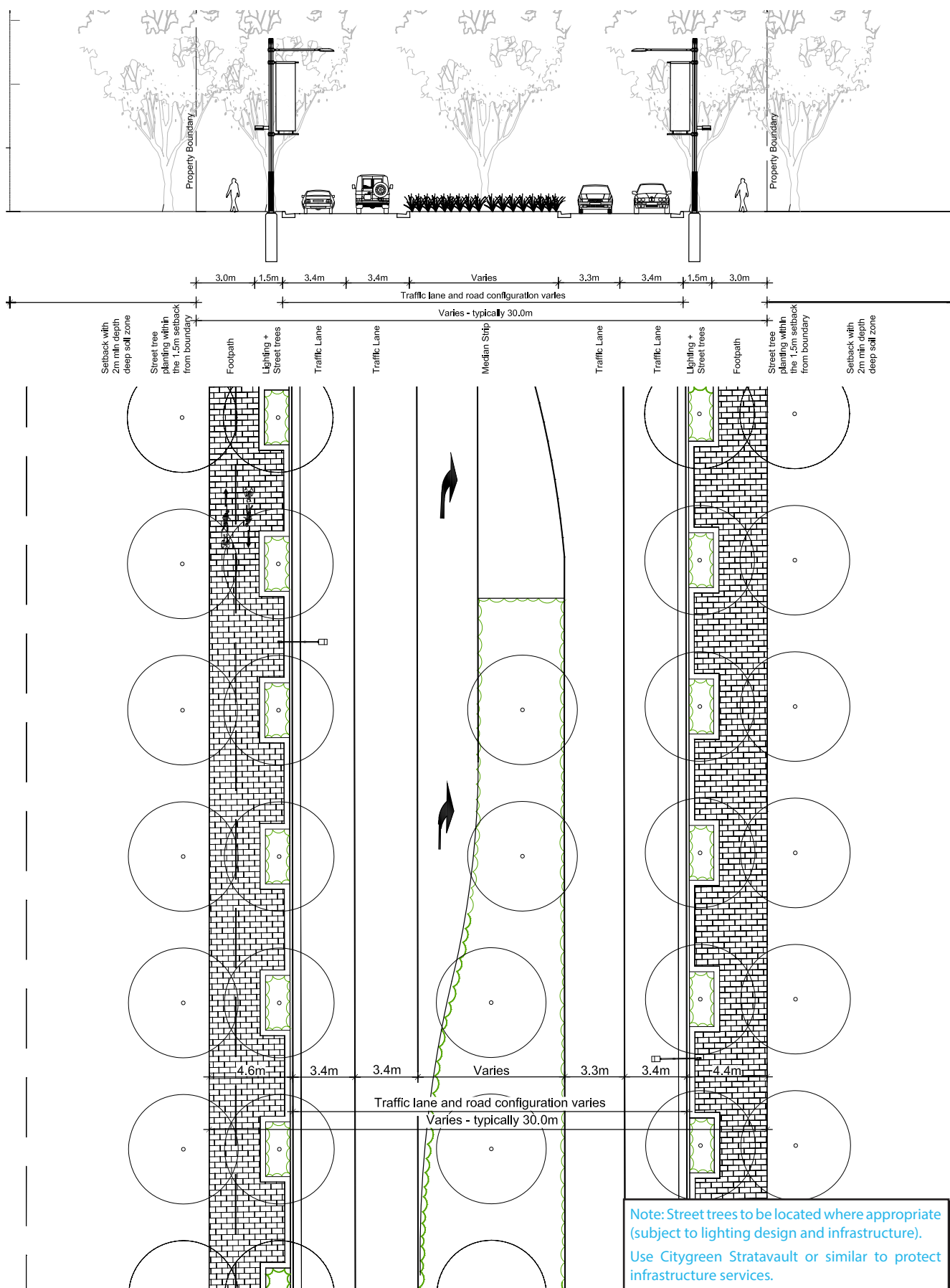


Figure 6.6.2.1 Typical Layout - Mixed Use, Commercial, or Business Character Street (NOT TO SCALE)  
e.g. Waterloo Road - Typically 30 metres wide

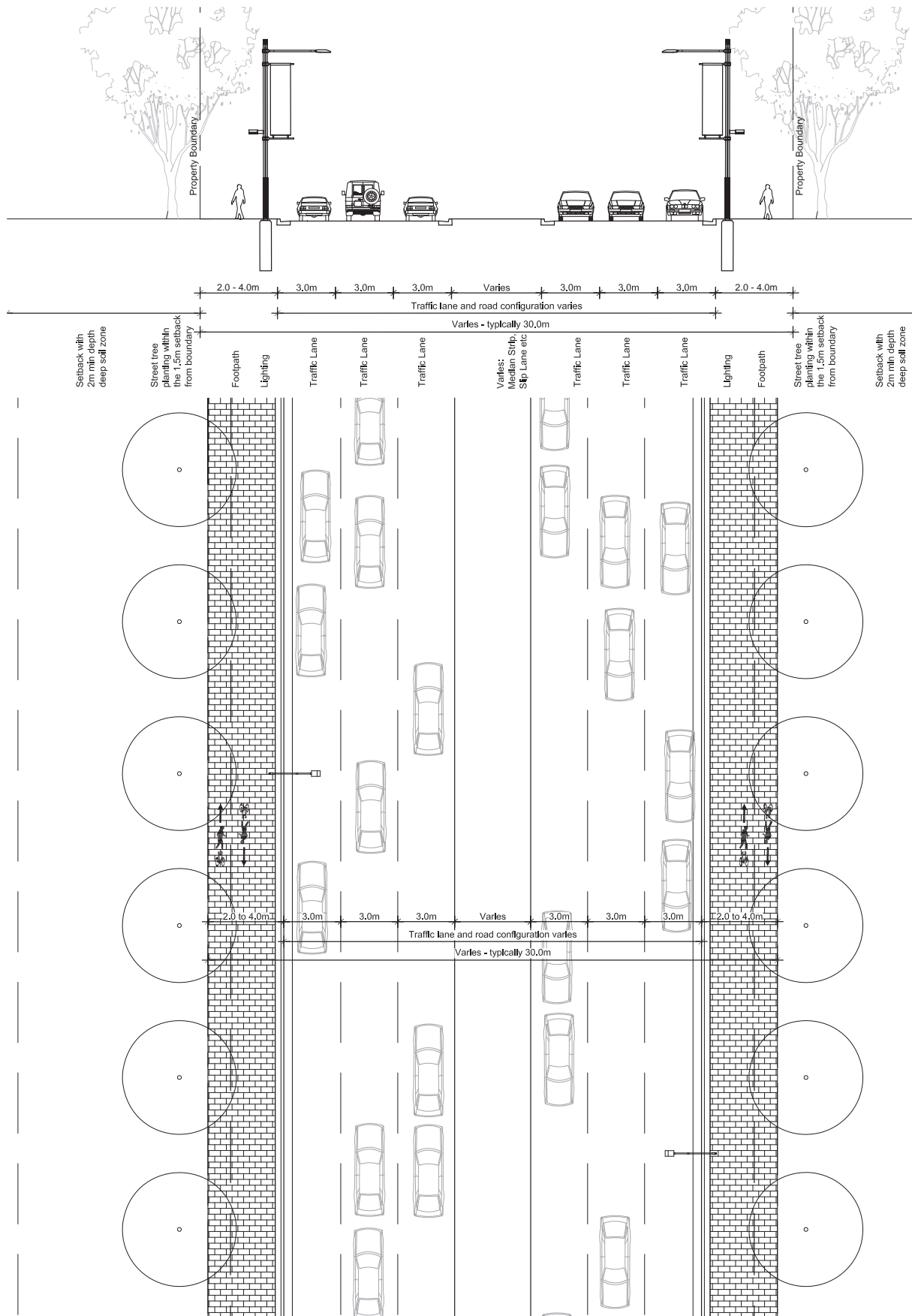


Figure 6.6.2.2 Typical Layout - Mixed Use, Commercial, or Business Character Street (NOT TO SCALE)  
e.g. Lane Cove Road - Typically 30 metres wide



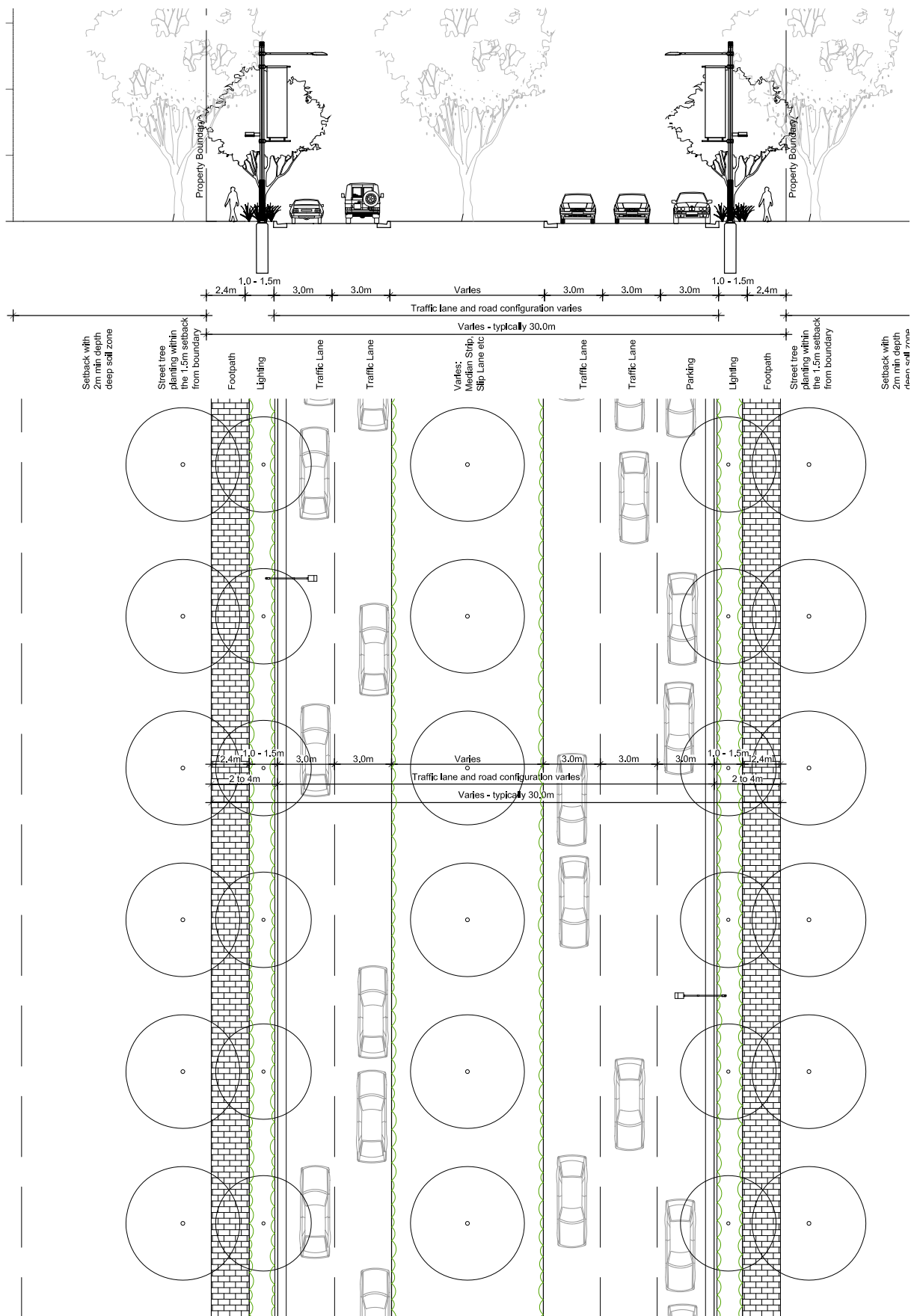


Figure 6.6.2.3 Typical Layout - Mixed Use, Commercial, or Business Character Street (NOT TO SCALE)  
e.g. Herring Road - Typically 30 metres wide

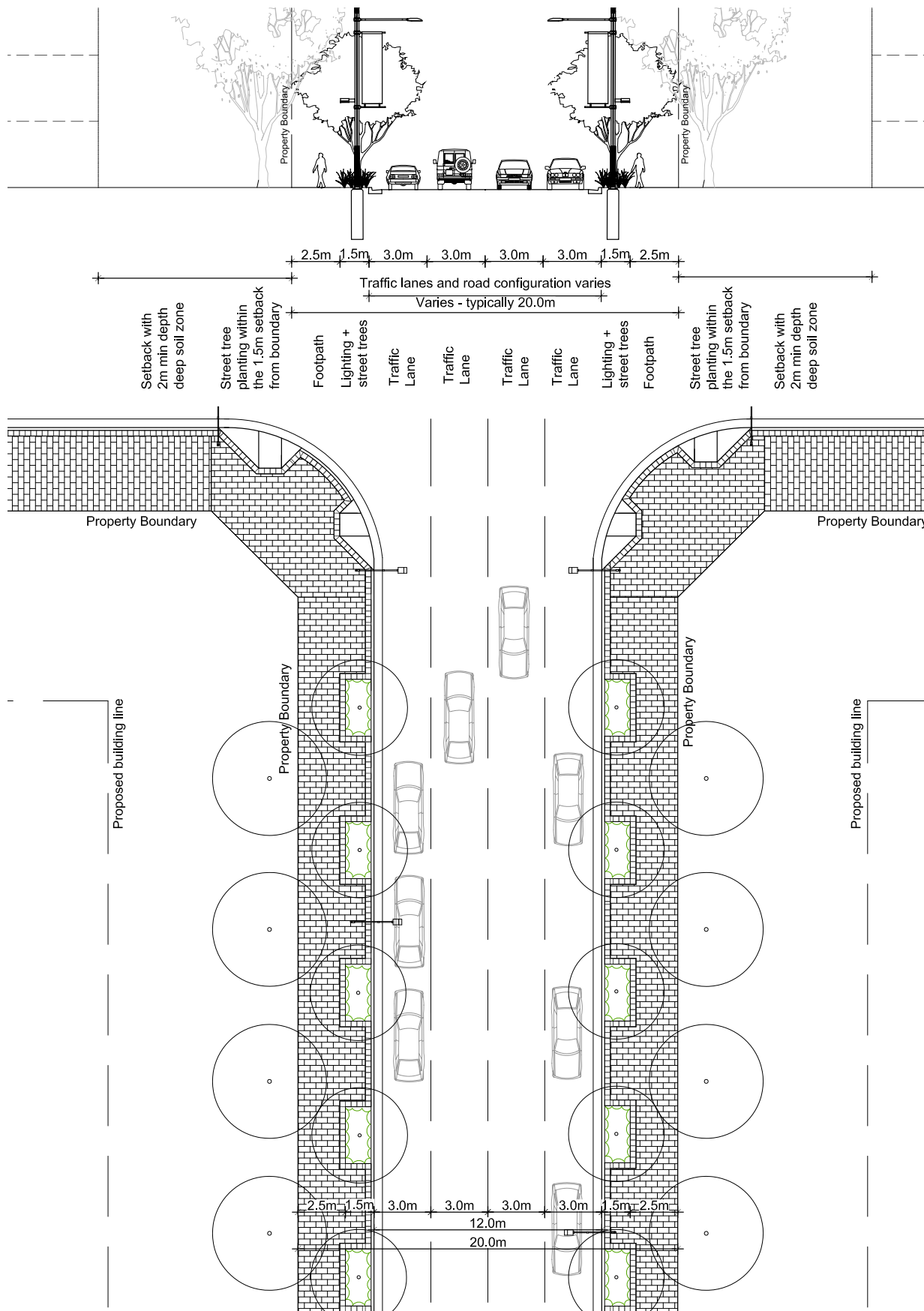


Figure 6.6.2.4 Typical Layout - Mixed Use, Commercial, or Business Character Street (NOT TO SCALE)  
Secondary Road - Typically 20 metres wide

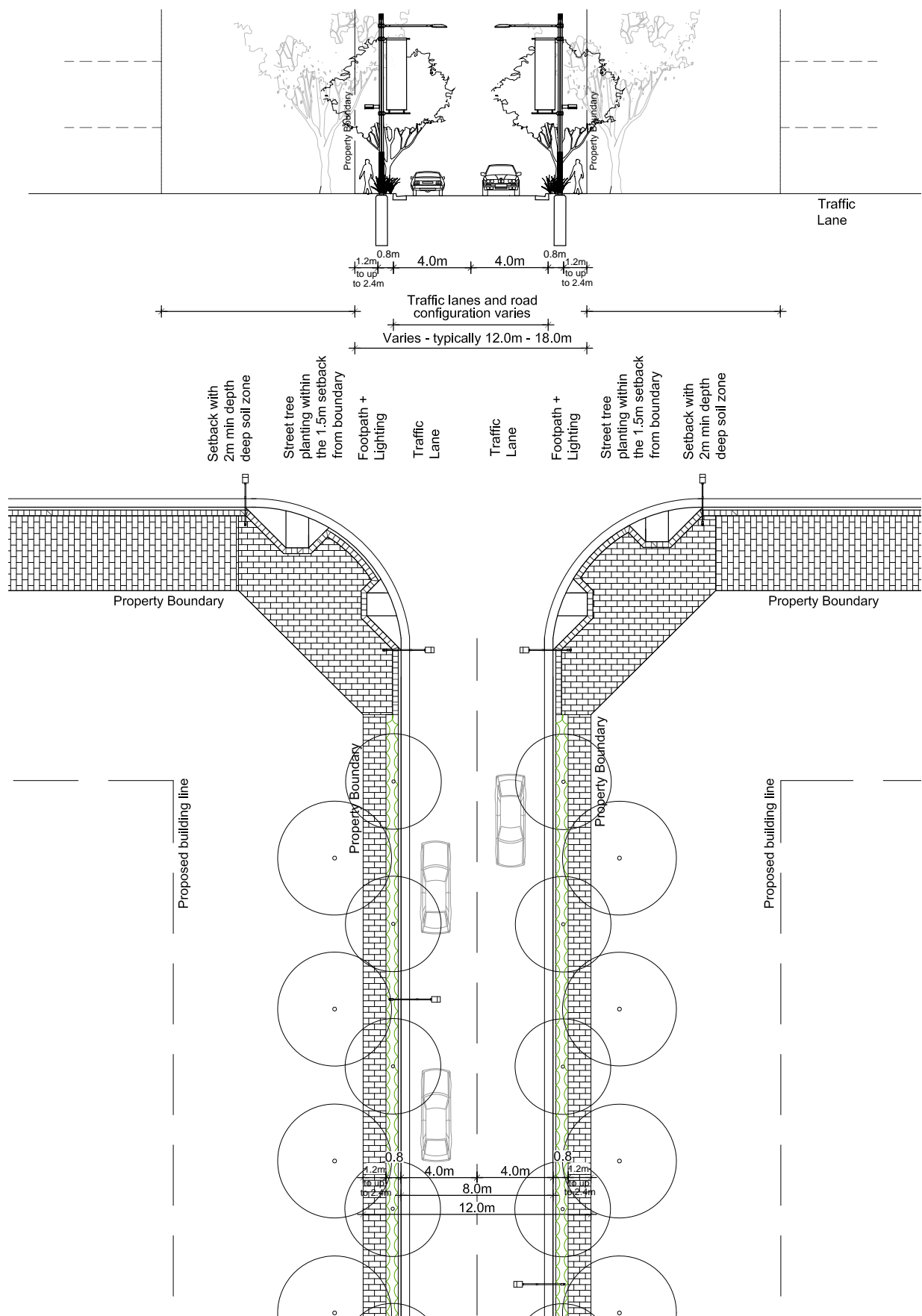


Figure 6.6.2.5 Typical Layout - Mixed Use, Commercial, or Business Character Street (NOT TO SCALE)  
e.g. Tertiary Road - Julius Avenue - typically 12 to 18 metres in wide



### 6.6.3 Existing Streets - Residential Character

The following typical specifications apply to existing streets with a predominantly residential character within the Corridor.

Table 6.6.3.1 Typical Specifications for Existing Residential Character Streets

Elements	Typical Specifications
<b>Footpath</b>	
Paved footpath	Up to 2.4 metres (varies) (Refer to typical layout diagram)
Materials	Granite paving equal to black - flame exfoliated 600 mm x 300 mm x 60 mm (Refer to Fig. 3.2.1 <i>Macquarie Park Corridor Paving Plan</i> )
Kerb	Integral cast-in-situ concrete kerb and gutter; kerb ramp to be designed in accordance with desire lines, safety, and conflict zones (subject to Council's approval)
Driveway	Granite paving equal to black - flame exfoliated 150 mm x 100 mm x 60 mm
<b>Landscaping</b>	
Street Trees	To match existing; trees planted in 1.5 x 3 m pits spaced approximately 8 metres apart (subject to lighting design and other site-specific constraints)
Nature Strip	Native grass species such as <i>Lomandra Longifolia</i> ('Tanika' Spiny-head Mat Rush) or <i>Dianella caerulea</i> (Blueberry lily) (subject to Council's approval)
<b>Street Furniture</b>	
Bollards	Installed as necessary Specification: LEDA SSP80F Fixed Bollard 80NB (900mm high) Finish: Linished or brushed Grade 304 Stainless Steel
Bike rack	2 per city block located close to intersections or bus stops Specification: LEDA BR85F Bike Rail Dimensions: 850 x 1000 mm (200 mm in ground) Finish: 40NB(48.26) x 2.77mm Grade 304 Stainless Steel Pipe/Linished
<b>Street Lighting</b>	
Layout	Staggered configuration; refer to <i>Street Lighting Schema for the Macquarie Park Business Area</i> (subject to design and to be confirmed by Council)
Multi-function light pole	9 metres in height; refer to <i>Design Guide for Council-owned Street Lighting</i> for specification



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City of Ryde  
Civic Centre  
1 Devlin Street  
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# Ryde Town Centre

**PUBLIC DOMAIN PLAN - FINAL** (January 2006)



City of Ryde



Prepared by



In association with

Milne and Stonehouse  
Public Artists and Planners

Tar Technologies Transport Planners

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*This section defines the purpose, scope and structure of the City of Ryde Public Domain Plan, and its relationship to other relevant instruments and policies.*

## 1.1 Purpose

This plan serves as a manual to guide the future planning and design of the Ryde Town Centre public domain. It provides an overall direction for the design and implementation of hard and soft landscaping elements within the public domain, and its interface with the private realm.

The plan has been commissioned by the City of Ryde to complement Ryde Town Centre, Local Environmental Plan 143: and Development Control Plan 56, to promote the revitalisation of key centres within the Ryde Local Government Area (LGA). This *Public Domain Plan* is part of a suite of planning documents that will guide the redevelopment of Ryde Town Centre as the focus for residential, retail, commercial and civic services within the City of Ryde.

The *Public Domain Plan* is intended for use by the Council, developers and consultants involved in the development of Ryde Town Centre.

## 1.2 What is the Public Domain?

Within this plan, the public domain represents all urban and natural elements, structures and spaces that exist within the publicly-owned areas of Ryde Town Centre, and the relationship between them. The public domain is also considered to include many privately-owned arcades, building forecourts, internal streets and other semi-public spaces, given that they also influence the overall character of the public domain.

Ryde Town Centre (also referred to as Top Ryde) is located in the north-west region of Sydney, approximately fourteen kilometres from Sydney CBD and ten kilometres from Parramatta.

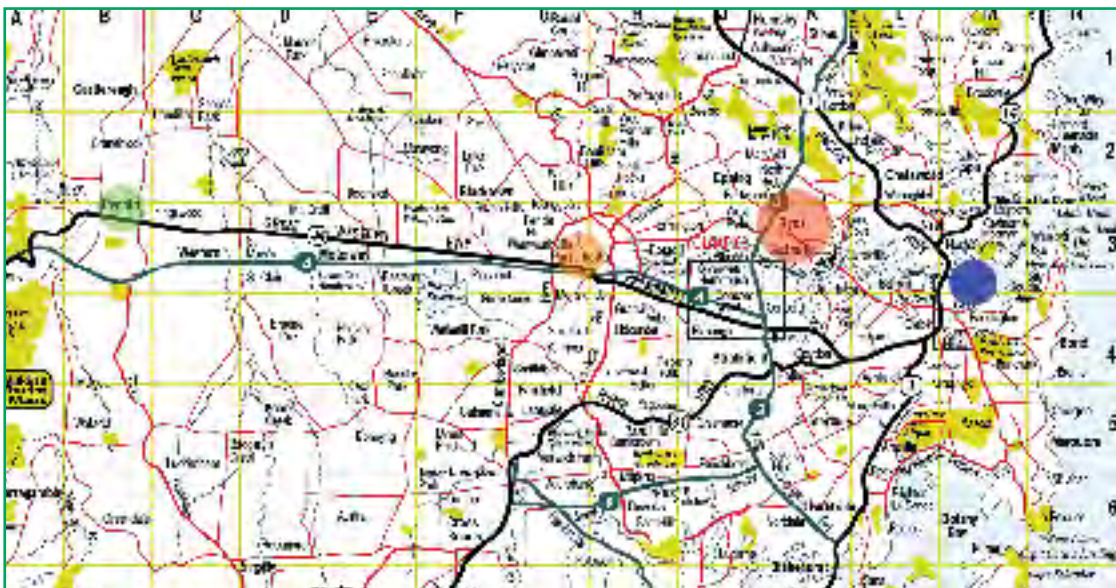


Figure 1.1 - Locational Plan



The area specifically governed by the City of Ryde Public Domain Plan is bound by Curzon Street, Ryde Public School and Argyle Street to the north; Victoria Road to the South; Blaxland Road and Belmore Street to the west; and Princes Street to the east. Refer to Figure 1.2 below.



Figure 1.2 - Public Domain Plan Boundaries

The *City of Ryde Public Domain Plan* provides:

- An outline of the historical background of the subject area,
- Analysis of its existing character and condition; including the built environment and materials; the open space and street tree network; existing natural systems; transport and pedestrian movements; and existing social and cultural precincts,
- An urban design framework providing design direction for future development; including recommended design approaches, treatments and details,
- An estimate of costs for suggested treatments, and
- A proposed implementation plan for these recommendations.

## 1.3 Relationship to Other Instruments & Policies

The City of Ryde Public Domain Plan is a guideline that represents one of several planning initiatives governing the development of Ryde Town Centre. Legislative instruments that also apply to the region are the Ryde Town Centre Local Environment Plan (LEP) 143, and Development Control Plan (DCP) 56, which have been prepared in parallel with this plan. These documents contain strategies and provisions to:

- Establish a new residential community within the Ryde Town Centre,
- Enhance the pedestrian experience and encourage increased public transport use,
- Protect and enhance heritage significance within the area, in particular, the heritage precinct around St Anne's Church,
- Enhance the character of the retail shopping strips in Church Street and Blaxland Road and develop 2000 square metres of civic open space within the existing shopping centre site.

## 1.4 How to Use this Plan

This Public Domain Plan should be read in conjunction with the Ryde Town Centre Local Environment Plan 143, and Development Control Plan 56. Specifically, it should be used to underpin the analysis and inform the design of proposed development sites / works in order to:

- Influence the understanding of the site context, including its existing infrastructure and character,
- Guide design and development decisions to ensure a seamless integration of the public and private domain,
- Inform the choice of materials, furniture, plants and other elements to be used within the Town Centre, and
- Provide physical solutions for recommendations contained within the various planning instruments and strategies that apply to the Ryde Town Centre.

Like all planning documents, this plan should be considered within an optimum timeframe of applicability to ensure its continued relevance and usability to the Ryde Town Centre as it develops over time. It is recommended that the lifespan of this document should be a period of five to ten years, with a review of key provisions occurring every one to two years.

***This section establishes a guiding vision for the revitalisation of Ryde Town Centre and describes the key objectives of the Public Domain Plan.***

## 2.1 Vision for Ryde Town Centre

***“Ryde Town Centre will be a vibrant, attractive and safe place to live, work, visit and invest, with a diversity of public spaces and opportunities for leisure, learning, shopping and business that reinforce the role of Ryde Town Centre as the civic and commercial hub of the City of Ryde. The centre will be enlivened by outdoor dining, civic events and festivals that are set within a high quality public domain.”***

This vision will be achieved through the implementation of significant public domain improvements that are in turn supported by a range of site-specific objectives as outlined in the following section.

## 2.2 Objectives of this Plan

This plan aims to establish a framework of themes, strategies and design principles to guide the implementation of public domain improvements within Ryde Town Centre.

Key objectives of the plan are to:

- Promote design solutions and finishes that enhance the local identity of the area and provide a distinctive, unified and memorable character for Ryde Town Centre,
- Encourage the development of places that are safe and desirable for all users,
- Address existing divisions within the centre by promoting linkages,
- Facilitate the creation of places of high design quality that encourage social interaction, that allow for the expression of cultural differences, and that provide a range of sensory experiences,
- Accommodate change over time through the creation of urban spaces that are flexible and allow uses that address a diverse range of user needs,
- Provide for improved pedestrian and vehicular networks, ensuring equitable access in and around Ryde Town Centre and promoting an urban layout that is easy to understand,
- Promote the visual and physical integration of the public and private domains,
- Protect heritage elements and precincts within Ryde Town Centre, and
- Provide an achievable, cost-effective plan for the staged implementation and maintenance of public domain works.





*This section provides a brief overview of the historical development of Ryde Town Centre, and presents an analysis of the existing character, condition and accessibility of the built and natural elements within the City. Key opportunities and potential constraints related to these factors are also identified.*

## 3.1 Historical Overview

An understanding of the historical development of a region is an important process in the identification of patterns, precincts and features that can be preserved, enhanced or interpreted in the modern-day setting. The following section briefly outlines key periods and events that have shaped the development of the City of Ryde.

### 3.1.1 Aboriginal History & Impacts of Early European Settlement

Before the arrival of European settlers, the present-day Sydney region was inhabited by several groups of Aboriginal people that held claim over distinct areas of land. The clan that occupied the modern-day area of Ryde was the Wallumede, which formed part of a larger group known as the Dharug (Dharuk / Darug). This region, a plateau surrounded by rounded hills and valleys and bounded by the Parramatta and Lane Cove Rivers, was well-suited for habitation, providing reliable supplies of food, water and shelter to the Dharug peoples.

The arrival of European settlers to Port Jackson in 1788 had a devastating impact on the Aboriginal populations of the Sydney region. The introduction of diseases, such as Smallpox, and the forceful alienation of the indigenous people from their traditional lands as the colony expanded, resulted in the deaths and displacement of thousands of Aboriginal people.

Driven by a need to produce food for the colony at Port Jackson, the European settlers undertook a number of expeditions along the Parramatta River in search of fertile land for farming. The modern-day area of Parramatta was settled by Governor Arthur Phillip in November 1788 and following this, a number of land grants were issued for the purposes of farming along the Parramatta River.

The first land grants to be made within the present-day Local Government Area (LGA) of Ryde occurred in January 1792 on the northern shores of the Parramatta River. This land formed part of a larger collective of grant lands known as the Field of Mars. The remaining areas of the present-day LGA were comprised primarily of 25 acre and 30 acre land grants to ex-convicts and army servicemen. Refer to Figure 3.1.1.



### 3.1.2 Establishment & Growth of the Village of Ryde: 1792 - 1900

**Agricultural Production:** By 1803, the Ryde district had become established as an important agricultural region, although the small size of individual land grants impeded farming potential. In response to this, the government of the day formed the Field of Mars Common in 1804, which was a collective of public lands to be used by the local people.

An additional constraint on farming production was the cost of transporting goods to Sydney. The Parramatta River formed a natural barrier between the Ryde area and Sydney, preventing direct road access. The initial transport route between the colonies at this time was therefore via ferry along the Parramatta River. It was not until 1880 that the southern portion of the Field of Mars Common was sold to fund the construction of the Gladesville and Iron Cove Bridges, which significantly increased the accessibility of the Ryde district. The remainder of the Field of Mars Common eventually became Lane Cove National Park.

**Emergence of a Town Centre:** The early development of the village at Ryde grew around the Kissing Point Church constructed in 1826. This church, later to become St Anne's Church as it is now called, was built on the ridgeline of the Ryde area, overlooking the Parramatta River. It formed a prominent landmark for the region and quickly became the focal point for the establishment of a town centre.

The construction of several main roads followed in the 1830s, located within close proximity to the church. These included Glebe Street that followed the present-day alignment of Victoria Road, and the connection of the Great North Road running between Sydney and Wisemans Ferry. By the early 1840s, the name 'Ryde' (named after a town on the Isle of Wight) was increasingly used to refer to the district. This name was given by Mary Turner, the wife of Reverend George Turner, who was Minister of St Anne's Church.

**Subdivision & Commercial Development:** Between 1840 and 1860, the farmlands of Ryde were heavily subdivided for residential and commercial development. By the end of the 1850s the majority of the commercial and civic buildings of Ryde were clustered along the main streets of Glebe Street (Victoria Road) and Church Street. The Municipality of Ryde was officially proclaimed by the government on 12 November 1870. The new Council assumed responsibility for the further development of several roads, including Church, Glebe, Parkes, Princes and Belmore Streets. In the early 1900s, Parkes Street became the main commercial street, following the erection of the Town Hall in 1903 and the relocation of the Post Office in 1907. Refer to Figure 3.1.2, 1895 Village of Ryde. Despite the increase in civic and commercial buildings within the town centre however, the character of the Ryde district in the early 1900s remained predominantly rural.





Figure 3.1.2 - Village of Ryde 1895 (Johnson et al 1992)

**Transport and infrastructure** developments in the Ryde district in the late 1800s and early 1900s greatly influenced the growth of the town. Major transport improvements included the construction of the Gladesville and Iron Cove Bridges in 1880, the extension of the Sydney railway line to Ryde in the 1886 and the connection of tramway lines in 1910 which increased the accessibility of the region.

Key infrastructure improvements led to a significant increase in development of the Ryde town centre. Expansion of residential development in the region was further aided by the supply of water. In 1888 a large reservoir was built on the corner of St Anne's Street and Little Church Street, and in 1892 pipes were connected between the reservoir and a pumping station that was built near the railway station.

Ryde railway station (later to become West Ryde station) was opened on 17 September 1886. This single track rail line joined Strathfield to Hornsby and required the construction of an iron lattice girder bridge connecting Meadowbank to Rhodes across the Parramatta River.

### 3.1.3 Establishment & Growth of the Village of Ryde: 1900 - 2005

In 1907 the government approved the extension of the Sydney tramway system (commenced in 1879) from Gladesville Bridge to Hatton's Flat, Ryde, which was located at the intersection of modern-day Pope Street and Lane Cove Road. This line was completed in 1910.

- The completion of the tramway line resulted in Ryde becoming a tourist destination, with visitors experiencing a scenic trip over the Parramatta River through the farmlands of the first growing district of New South Wales. In 1914 the tram line was extended to Ryde Station. Refer to Figure 3.1.3, 1919 Town Map.
- This tramline also encouraged the expansion of retail precincts within the City of Ryde. Retail centres grew along Parkes Street (now Blaxland Road) in close proximity to the former Post Office at the intersection of Blaxland Road and Church Street.
- The trams were most heavily used between the 1920s and 1930s with the expansion of residential subdivision after the First World War. At the end of the Second World War however, the trams were gradually replaced by buses. The last tram travelled to Ryde in December 1949.
- In the early 1900's Ryde Council had been looking for land in the vicinity of the town centre to create a park. Salters Paddock was chosen and Ryde Park was opened in 1908.
- Ryde Bowling Club was established within the grounds of Ryde Park in 1909.
- Ryde Park was extended in 1925.
- Between 1945 and 1970 the population of Ryde doubled from 35,460 to 87,000 residents.
- During the 1920's, commercial centre of Ryde was expanded. Top Ryde Regional Shopping Centre opened in November 1957. It was the first regional shopping centre built in New South Wales and the second built in Australia.
- Ryde Civic Centre opened on 15 August 1964. Modelled on the AMP Building in Circular Quay Sydney.
- By 1964 the traffic flow along Devlin Street was 37,000 vehicles per day.



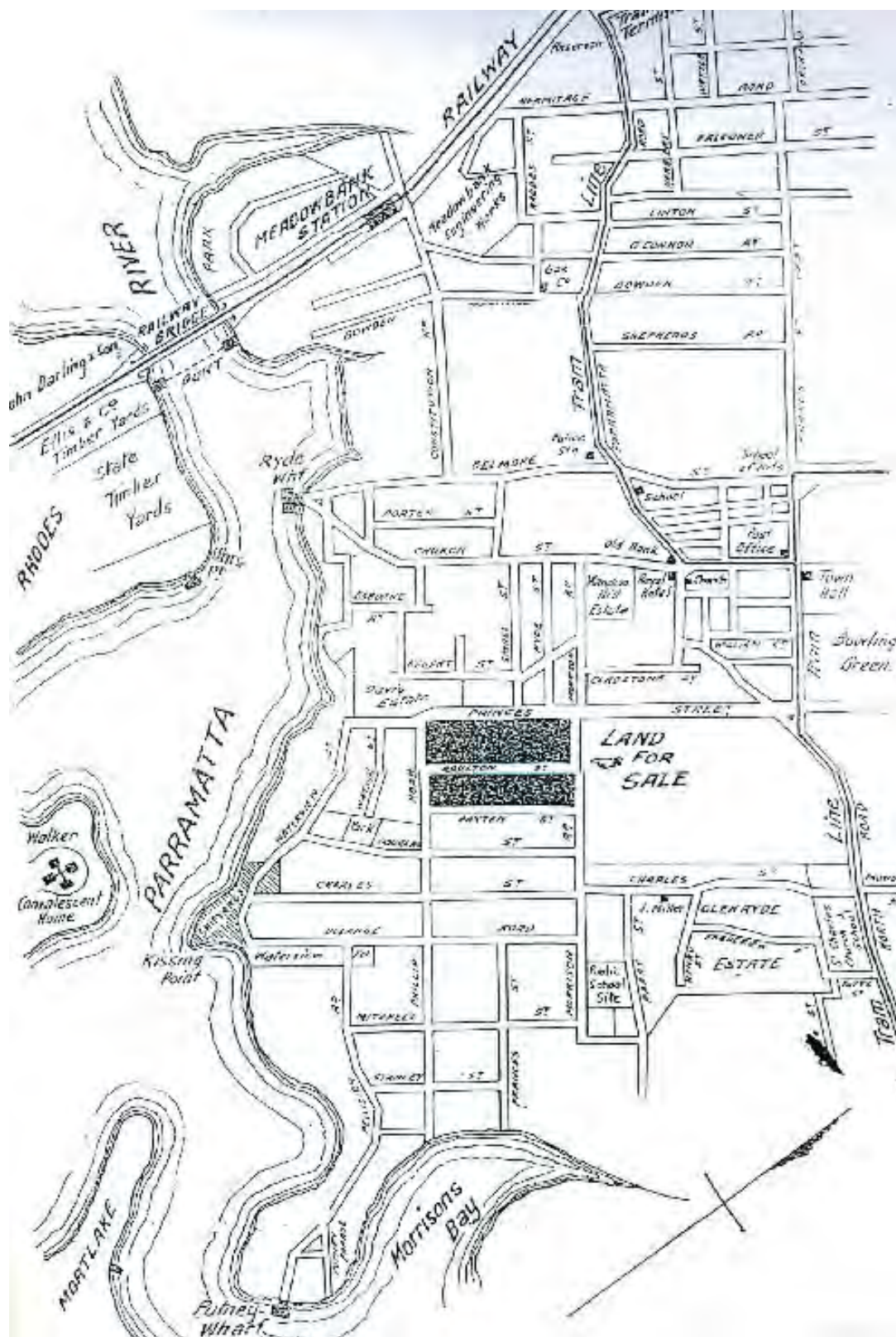


Figure 3.1.3 - Village of Ryde 1919 (Johnson et al 1992)



- The Centenary Library Building was opened in 1970.
- In 1998/99 the Devlin Street underpass was opened. Over 100,000 vehicles per day pass through the intersection of Devlin Street and Victoria Road.
- In 2001, the population of Ryde reached 100,000 residents.

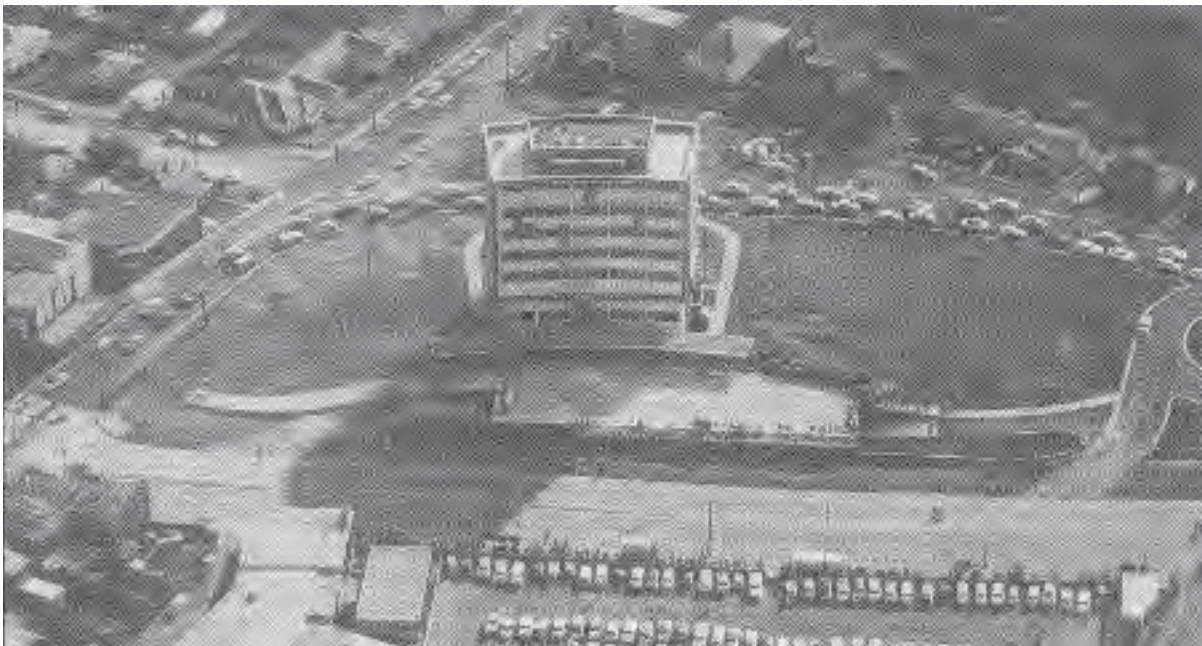


Figure 3.1.4 - Opening Day: Ryde Civic Centre 1964 (Martin 1998 p.61)



Figure 3.1.5 - Opening of the Ryde Tram Line to Hatton's Flat 1910 (Martin 1998 p.117)

## 3.2 Existing Character

An understanding of the historical development of a region is an important process in the identification of patterns, precincts and features that can be preserved, enhanced or interpreted

### 3.2.1 Built Environment

With the exception of the Civic Centre and Council offices, the existing building stock within the Town Centre is relatively low-scale (between one and three storeys in height). The built form of Ryde Town Centre can be roughly grouped into the following key precincts:

**Commercial hub / town core:** comprising Top Ryde Shopping Centre and surrounding on-street retail properties in Church Street and Blaxland Road. Following a shift in focus away from St Anne's Church in the early 1900s, the present-day focal point of Ryde Town Centre is the Top Ryde Shopping Centre, bound by Devlin Street, Blaxland Road, Pope Street, and Tucker Street. Built in the 1950s and modelled on the American shopping mall, this shopping centre was the first of its kind built in New South Wales, and the second in Australia.

The existing centre, which has become run-down, is envisioned to be a mixed-use (commercial, retail and residential) development. The redevelopment of the centre will significantly enhance the public domain, providing pedestrian through links both north-south between Pope Street and Blaxland Road and east-west between Devlin Street and Tucker Street. The commercial and retail components of the development are required to be open-planned, with a vibrant active frontage in contrast to the existing centre, and will provide an alternative to mega-mall shopping centres such as Macquarie Centre and the recently opened Rhodes Shopping Centre at Rhodes in the adjacent Canada Bay LGA. Furthermore, the existing, large open carpark, which contributes poorly to the public domain, will likely be replaced with underground parking.



**Civic precinct:** bound by Blaxland Road, Devlin Street and Belmore Street, and comprising the Council offices, the Council carpark on the corner of Parkes Street and Blaxland Road, Ryde Library and the War Memorial. The existing Civic Centre building, containing the City of Ryde council offices and Ryde public library (Centenary Library), was constructed in 1964 and 1970. Located prominently on a ridgeline and standing as the tallest building in Ryde Town Centre at seven storeys high, the building has become an iconic landmark for Ryde. The Civic Precinct will be subject to a future masterplan.

**Heritage precinct:** St Anne's Church and its surrounds feature several historic buildings of heritage significance. These include the Courthouse building (1863) and former Odd Fellows Hall (1869) in Church Street; Westward Cottage (c1850) and the Parsonage (c1880) in Turner Street; and the Methodist Churches (1849 and 1870) at the intersection of Church Street and Church Lane. Other buildings of historical importance include Ryde Public School (1877) designed by the government Architect.

**Residential precinct:** on the fringes of the civic and commercial core is an awkward mix of detached single storey houses, older-style apartment blocks and newly built three-storey flat buildings with little urban design merit. In the main street of Blaxland Road, the presence of several vacant buildings and allotments contribute to the run-down appearance of many of the built elements within the centre.

### **Key Issues**

The existing built form within Ryde Town Centre is highly disjointed with a generally poor relationship between individual buildings, development precincts and the public domain. The overall impression is one of a neglected town centre in need of revitalisation.

There are several historic buildings whose quality and character can be interpreted and enhanced within the centre.

### **Opportunities**

Activate building frontages to create vibrant streetscapes.

Acknowledge and interpret architectural and cultural heritage through artworks, landscape treatments, furniture and finishes and active participation, such as the promotion of walking tours within the Town Centre.

Address poor building quality of existing building stock through upgrades and maintenance, and promote better urban design for new buildings that will engage the public domain.

Preserve access to natural sunlight within the streets and urban spaces of the Town Centre through the implementation of building height and massing controls.



**Figure 3.2.1 - Existing Built Environment**



**LEGEND**

 One storey Residential	 One storey Commercial	 Two storey Retail	 Civic/community buildings
 Two storey Residential	 Two storey Commercial	 Three storey Retail	 Schools
 Three storey Residential	 Three+ storey Commercial	 Heritage	 Ryde Park
 Four storey Residential	 One storey Retail	 Churches	 Ryde Bowling Club

### 3.2.2 Paving Types

The main pavement treatments existing within the public domain for Ryde Town Centre are summarised below:

**Commercial hub / town core:** Predominantly concrete paving in Pope and Tucker Streets, with a mix of concrete unit pavers, insitu concrete and bitumen pavement along Blaxland Road, Devlin Street and Church Street. Clay brick pavers occur along the frontages to the Top Ryde Shopping Centre.



**Civic precinct:** Primarily 1.2 metre wide plain concrete pathways with granite paving surrounding the existing War Memorial and exposed aggregate/ decorative pebble finish concrete to the forecourt of the Council offices.

**Heritage precinct:** Existing sandstone kerbstone with new sandstone banding and bitumen infill in Church Street. Concrete pavers and concrete paving



**Residential precinct:** Primarily 1.2 metre wide plain concrete pathways with grass verges / nature strip. In the narrow laneways such as Blaxland and Belmore Lanes there are no footpaths.

#### Key Issues

The current pavement treatments within the Town Centre are highly varied and generally degraded. This variety of paving treatments contributes to a low sense of local identity and pedestrian readability throughout the centre, and should be rationalised.

#### Opportunities

The selection of an attractive and coherent palette of paving materials for the Town Centre will create unity and a strong local identity within, and between, individual precincts. The chosen paving treatment should:







- be attractive and durable,
- be reliable in supply and easily replaceable in the event of damage,
- reflect the character of Ryde,
- have low maintenance requirements, and
- be adaptable for use in various applications. This may be achieved, for example, through the availability of a several unit sizes within the one treatment.



**Figure 3.2.2 - Existing Paving Types**



**LEGEND**

	No paving/footpath		Concrete full extent		Bitumen paving full extent
	1.2m wide concrete path and grass verge		Concrete clay pavers full extent		Bitumen paving with sandstone trim



### 3.2.3 Open Space & Street Tree Network

**Open Space:** The need for significant public open space to be preserved within the town centre was recognised by Ryde Council in the early 1900s. Ryde Park, located at the intersection of Blaxland Road and Princes Street was subsequently opened as a park in 1908. Today, the major open spaces existing within Ryde Town Centre are Ryde Park and adjacent Burrows Park, the grounds associated with Ryde Public School in Tucker Street, and the open space area surrounding the Civic Centre building and War Memorial along Blaxland Road. Other important areas of open space include the St Anne's Church grounds, the corridor of open space along Devlin Street at the frontage of the Parsonage and the open space around Blaxland House in Church Lane.

#### **Key Issues**

Lack of connection / poor accessibility of Ryde Park.

The various forms of open space are currently disjointed within the Town Centre.

Some areas, such as those associated with Ryde Public School and the historic buildings are semi-public in nature and are not utilised widely by the broader community.

There is currently no major open space / urban plaza within the heart of the Town Centre.

The narrow frontages in the main streets of Blaxland Road and Church Street constrict major activity in the public domain.

#### **Opportunities**

Promote pedestrian links to connect open spaces. A Landscape Masterplan prepared by Clouston in early 2005 has identified opportunities to link Ryde Park to the adjoining Buffalo Creek bushland corridor. This will also enable larger linkages to the Lane Cove River corridor via Buffalo Creek and associated reserves and parks. The Masterplan also identifies opportunities to provide connections from Ryde Public School through to the Top Ryde Shopping Centre.

Create visual connections between open space areas and precincts through the use of coherent street tree planting.

Address the isolation of the War Memorial open space and explore opportunities for passive uses within the grounds.

Extend and enhance the public domain in all new developments. Measures such as the pedestrian through-links proposed within the Top Ryde Shopping Centre redevelopment should be encouraged.

Encourage the creation of public spaces for cultural events and festivals.

**Street Tree Network:** The dominant street trees existing with the Town Centre include:

*Rainforest trees:* such as *Stenocarpus sinuatus* (Firewheel Tree), *Buckinghamia celcissima* (Ivory Curl Flower), *Elaeocarpus reticulatus* (Blueberry Ash), *Acmena smithii* (Lilly Pilly), *Backhousia citriodora* (Lemon Scented Myrtle), *Araucaria cunninghamiana* (Hoop Pine), *Lophostemon confertus* (Brush Box), *Ficus rubiginosa* (Port Jackson Fig), *Araucaria bidwilli* (Bunya Pine),

*Eucalyptus species:* including *Eucalyptus microcorys* (Tallowood), *Eucalyptus citriodora* (Lemon Scented Gum), *Eucalyptus maculata* (Spotted Gum), *Eucalyptus saligna* (Blue Gum).

*Other street trees:* include *Callistemon* sp. (Bottlebrush), *Jacaranda mimosifolia* (Jacaranda), *Fraxinus raywoodii* (Claret Ash), *Liquidambar styraciflua* (Liquidamber), *Melaleuca quinquenervia* (Broad-leaved Paperbark), *Alnus jourulensis* (Evergreen Alder).

The location of street trees is relatively unstructured, with clusters of remnant, indigenous vegetation being mixed with individual specimen plants and a variety of native and exotic species.

### Key Issues

The existing street tree network in the Ryde Town Centre is comprised of a broad mix of different planting styles and species.

There is little structure to the current organisation / location of street trees.

Small-scale, localised planter boxes and shrub plantings contribute to an unkept appearance in some areas of the Town Centre.

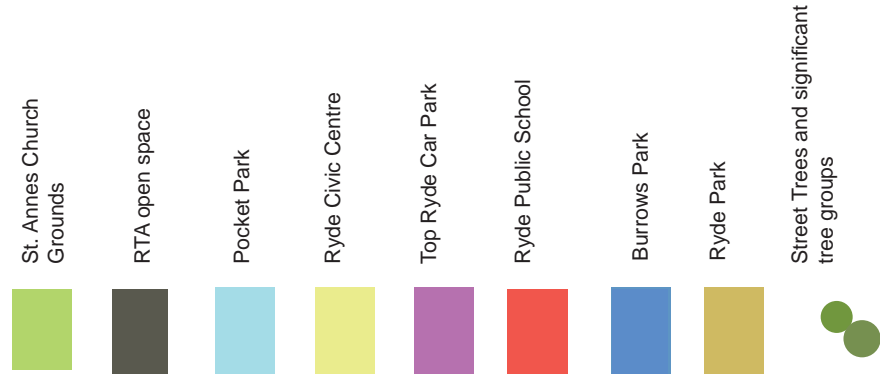
### Opportunities

Rationalise the street tree planting within the Town Centre through the development and staged implementation of a Street Tree Masterplan.

Use street trees to reflect the historical and environmental qualities of the Town Centre, and to promote visual linkages between various precincts and their environs.

**Figure 3.2.3 - Existing Open Space areas & Tree Network**

**LEGEND**



Ryde Bowling Club

Buffalo Creek bushland corridor  
- Vegetation connection to Lane Cove River



### 3.2.4 Natural Systems Topography & Significant Views

**Topography:** Ryde Town Centre is located on a plateau that was historically known as 'Hattons Flat'. With high points at St Anne's Church and the ridgeline housing the Civic Centre, the land falls gently to a shallow valley mid-way along the length of the Top Ryde Shopping Centre.

**Creeks and drainage lines:** To the west and south of the centre, the natural drainage line flows to the catchment of the Parramatta River, with the north and east sectors draining to the Lane Cove River catchment. The closest water course to the Town Centre is a modified tributary of Buffalo Creek which flows through Burrows Park, located diagonally opposite Ryde Park off Princes Street.

**Vegetation:** The existing vegetation within Ryde Town Centre is a mix of native and exotic street tree plantings, tree and shrub plantings within the private domain (front and back yards), and pockets of remnant, indigenous vegetation.

The largest stand of native vegetation in close proximity to Ryde Town Centre is the Lane Cove River bushland corridor. It includes an almost continuous bushland corridor that intersects with Ryde Town Centre along Buffalo Creek. While the Parramatta River is physically closer to the Town Centre, its historical value for agricultural production resulted in extensive clearance of its native vegetation in the early years of European settlement. Other pockets of mature native trees are concentrated on the fringes of Ryde Public School and Ryde Park.

**Views:** The most expansive views from Ryde Town Centre occur to the south-west of the ridgeline in the vicinity of St Anne's Church. From this location, and the adjacent Parsonage building in Turner Street, it is possible to gain clear views over the Parramatta River and Sydney Olympic Park at Homebush Bay. From the higher levels of the Civic Centre building, it is also possible to see the Parramatta Central Business District. From these high points, good views are also obtained of major streetscapes in Ryde, including Tucker Street adjoining the Ryde Public School open space and its native vegetation, mature trees in the heritage precinct along Church and Little Church Streets and the main street of Blaxland Road between Ryde Park and Devlin Street. Within the core of the Town Centre, views are generally confined to the immediate streetscapes, with key focal points being the Civic Centre building and Ryde Park.

#### **Key Issues**

The most prominent views to and from Ryde Town Centre are concentrated at the high point around St Anne's Church.

Views within the city centre are comprised mainly of linear, street-scale vistas.

#### **Opportunities**






Protect and enhance key vistas through the use of street trees to frame views, and through the implementation of building height and massing controls.

Promote the strategic use of artworks and architectural features to direct views and create new focal points within the public domain.

**Figure 3.2.4 - Existing Topography & Significant Views**



**LEGEND**

-  Historical views over Parramatta River / scenic views of Sydney Olympic Park
-  Landmark view to St. Annes Church
-  Elevated view
-  Contours- 5m intervals
-  Ryde Park

### 3.2.5 Transport Networks

**Existing Roadways:** The Ryde Town Centre vehicular transport network includes both local circulation routes and regional linkages (major roads). Key roads include:

#### *Regional links*

- Devlin Street - An arterial road that forms part of a major north-south corridor, connecting with Lane Cove Road at the junction with Blaxland Road to the north, and to Concord Road south of the Parramatta River. In the vicinity of the Top Ryde Shopping Centre, Devlin Street is currently a dual carriageway with between three and four travel lanes in each direction. As part of the shopping centre redevelopment, Devlin Street is proposed to be widened to accommodate access underneath the road to an underground parking station.
- Blaxland Road - An arterial road that provides an important east-west connection. The road may be categorised in two main sections due to a right angle bend in the road occurring within the Ryde Town Centre. North of Devlin Street and Lane Cove Road, Blaxland Road is a four-lane, undivided carriageway, with peak period parking bans to assist traffic flow. East of Devlin Street as far as Victoria Road, Blaxland Road is an undivided carriageway with sufficient width to accommodate four travel lanes. This section passes through the commercial / retail precinct of the Town Centre and provides a vehicular connection to Victoria Road for travel to and from the east, as an alternative to the major intersection of Victoria Road and Devlin Street. In this capacity, Blaxland Road is a principle support road to Victoria Road.
- Victoria Road - An arterial road that provides a major north-west through connection. In the vicinity of Ryde Town Centre, Victoria Road is a six-lane, dual carriageway. An interchange providing access to Victoria Road from Devlin and Church Streets was constructed in May 1999.

#### *Local roads*






Ryde Town Centre contains a number of local, circulation roads that provide connections throughout the residential and retail precincts. These are primarily two-way streets with some one-way routes, such as Belmore Street to the west of the Civic Centre. Pope, Smith and Tucker Streets are key local roads that provide access to the Top Ryde Shopping Centre and adjacent land uses.



**Figure 3.2.5 - Existing Road Hierarchy / Network**



**LEGEND**

- |   |  |   |
|---|--|---|
|  Major arterial road             |  Major intersection         |  Existing Council Carpark |
|  Access roads to Shopping Centre |  Existing Public Carparking |   |

**Previous Studies & Existing Road Network Capacity:** A number of traffic and transport studies have been prepared for various roads and projects within Ryde Town Centre. The *Top Ryde Retail Precinct Transport Study* and associated *Masterplan* was prepared in May 1998 by PPK. This study made several traffic management recommendations (including partial road closures, pedestrian crossing facilities and lane widening) intended to improve amenity and promote commercial activity within the centre.

This study was reviewed by Sinclair Knight Merz in 2003 in light of changes in traffic flow and movement patterns following completion of the Victoria Road / Devlin Street intersection; the proposed redevelopment of the Top Ryde Shopping Centre; and the potential impacts on other road infrastructure and local streets if Blaxland Road were to be partially closed as proposed. Based on these considerations, this study found that the following intersections would operate satisfactorily:

- Blaxland Road, Tucker Street and Church Street,
- Church Street and Morrison Road,
- Devlin Street, Lane Cove Road and Blaxland Road,
- Lane Cove Road and Buffalo Road, and
- Victoria Road, Devlin Street and Church Street, but not for all modelled networks.

Additional capacity improvements were required at the intersections of:

- Victoria Road and Bowden Street (left turn bay),
- Devlin Street, Blaxland Road and Parkes Street, and
- Victoria Road, Devlin Street and Church Street.

The main traffic constraints that would be generated by the proposed shopping centre redevelopment were related to intersection operation. A pedestrian overbridge would be required across Devlin Street, north of the intersection of Devlin Street and Blaxland road to maximise the traffic signal phasing in favour of the main traffic stream.

A final traffic impact assessment related to the proposed Top Ryde Shopping Centre redevelopment was prepared in November 2004 by Mark Waugh Pty Ltd. Specifically, this study investigated the impacts of a series of grade separated tunnels proposed to allow direct access to the underground parking area within the redevelopment. The study found that these works would:

- Remove traffic volumes from the critical movements at the Devlin Street / Blaxland Road intersection, and
- Remove traffic from the local system, particularly at Tucker Street and Blaxland Road.

The study concluded that the arterial road network is currently operating near capacity in peak times and that the Devlin Street / Blaxland Road / Parkes Street (pm peak) intersection and Victoria Road / Blaxland Road (am peak) intersection are not operating satisfactorily at present.

**Heavy Vehicles:** Surveys undertaken by Sinclair Knight Merz in 2003 indicate that within Ryde Town Centre, heavy vehicles generally travel on Devlin Street, Lane Cove Road and Victoria Road. Only a few heavy vehicles (between 3 and 14 vehicles per hour) used Blaxland Road. The volumes of heavy vehicles in local roads, such as Church Street, Tucker Street, Buffalo Road and Bowden Street, range from 2 to 5 vehicles per hour during the morning and afternoon peak hours.

**Public Transport:** Ryde Town Centre, is an important area for both local and regional bus operations utilising the road network. The number of bus routes servicing this area totals 88 during the morning and 95 during the afternoon / evening. These services connect Ryde to Sydney City, Circular Quay, Milsons Point, Macquarie Centre, West Ryde, Strathfield, Burwood, Gladesville Wharf, Eastwood, Denistone and Parramatta. Nearby train stations that are serviced by bus connections to and from Ryde include Meadowbank and West Ryde.

#### **Key Issues**

Devlin Street forms a significant barrier to pedestrian linkages within the city centre.

The narrow frontage of the public domain at the major bus stops in Blaxland Road (opposite the shopping centre), results in pedestrian congestion at the entrance to the shopping centre.

Bus services in the evening (following the afternoon peak) are limited.

#### **Opportunities**

Improve pedestrian connections and amenity within and through the town centre through pavement widening, and improved traffic management.

Traffic and road improvements in Devlin Street associated with the proposed Top Ryde Shopping Centre redevelopment, and the proposed pedestrian footbridge over Devlin is likely to improve traffic flow and pedestrian safety and accessibility.

Widen footpaths and improve amenity for bus patrons/users.



### 3.2.6 Pedestrian Networks

Pedestrian movement within Ryde Town Centre is primarily concentrated around and through the Top Ryde Shopping Centre, and in particular, at the major intersections forming its southern boundary. The intersection of Devlin Street and Blaxland Road is the most heavily used by pedestrians, being in close proximity to the main entrance to the shopping centre, the bus stops on both sides of Blaxland Road, and the Civic Centre and public library to the north-west. Recent counts undertaken during the morning and afternoon weekday peak periods have found that the average number of pedestrians crossing Devlin Street at these times is approximately 250. The majority of these crossings occur on the northern side of Devlin Street, with the main pedestrian destination being the Top Ryde Shopping Centre. Pedestrian activity is also relatively concentrated at the intersection of Blaxland Road and Tucker Street at the south-eastern corner of the shopping centre. The main pedestrian activity around this intersection is distributed between travel to the shopping centre, the bus stops and the retail strip along Church Street.

Other significant pedestrian patterns include:

- Movement between Ryde Public School and surrounds and the northern entrance to the Top Ryde Shopping Centre along Tucker Street,
- Movement between Ryde Public School and surrounds and the bus stops located on Blaxland Road, and
- Movement to and from the bus stops located on the southern frontage to the Top Ryde Shopping Centre along Blaxland Road.

A number of through-site links and major crossing upgrades are required by the LEP for the Top Ryde Shopping centre site. These are intended to both improve pedestrian access through the centre and minimise delays for motorists due to pedestrians crossing at the major intersections. Proposed improvements include:

- Construction of a pedestrian overbridge crossing Devlin Street at Blaxland Road west, connecting the Civic Centre to the first or second level of the proposed Top Ryde Shopping Centre redevelopment.
- Construction of a pedestrian overbridge crossing Devlin Street at the northern end of the Top Ryde Shopping Centre Site adjacent Pope Street.
- Construction of a pedestrian overbridge crossing Victoria Road adjacent Princes Street to allow for safe pedestrian access to the town centre from the redevelopment of the Dalton Gardens Retirement Village.
- **Note:** Final bridge locations to be determined at the detail design phase and with RTA approval.



Footbridge designed by Ed Lippman



- Allowance for a major pedestrian through-way within the shopping centre site, to connect Pope Street and Blaxland Road, and
- Extension of the existing lane way along the eastern side of Devlin Street to meet Victoria Road at Church Street, and across Church Street to St. Anne's Street.

The public domain plan also makes provision for pavement widenings to allow for outdoor dining and improvements to pedestrian amenity. The selection of locations for pavement widening has been based on such factors as existing and anticipated pedestrian movements / needs; location of bus facilities; natural topography; and suitability for outdoor dining.

Possible locations for outdoor dining include:

- Blaxland Road, from Edward Street to the Ryde Bowling Club,
- Sections of Pope Street, and
- The eastern side of Church Street, between Gowrie Street and Blaxland Road.

Blaxland Road between Devlin Street and Tucker Street is considered to be a desirable location for a widened footpath due to the relatively level grades of the footpath and its central location to the shopping centre, bus services and existing restaurants. The existing frontage to Blaxland Road however is narrow, and any proposal for pavement widening in this area will hinge upon the improvement of traffic flow through the intersection of Devlin Street and Blaxland Road. Possible options for improvement include:

1. Removal of one or both at-grade pedestrian crossings in Devlin Street at Blaxland Road, This option may also include provision of an exclusive bus phase (traffic signal) out of Blaxland Road.
2. Prohibit the left turn from Devlin Street into Blaxland Road for private vehicles, or alternatively reduce the length of the 'Green' signal at Blaxland Road and Victoria Road,
3. Ban the right turn from Devlin Street into Blaxland Road north-bound,

**Key Issues**

Pedestrian activity is concentrated around the Top Ryde Shopping Centre and at the major intersection at Devlin Street and Blaxland Road.

Through-site pedestrian linkages are limited at present.

Existing pavement frontages are relatively narrow and outdoor dining is limited at present.

**Opportunities**

Include through-site links within new developments to improve pedestrian connections and circulation.

Support construction of pedestrian bridges over major roads to facilitate pedestrian and traffic flow.



Investigate opportunities for pavement widening to reduce pedestrian congestion and provide for outdoor dining.



**Figure 3.2.6 - Existing Pedestrian Network**



**PEDESTRIAN NETWORK LEGEND**

-  Minor pedestrian
-  Areas of major pedestrian activity

### 3.2.7 Public Art, Social & Cultural Precincts

Public art and outdoor cultural elements are limited within Ryde Town Centre. The Queen Victoria fountain, which was originally located within the Town Centre, is now located at the intersection of Blaxland and Victoria Roads, opposite the Brad Garlick Ford dealership. Other items include:

- The memorial stone to denote the extension of the tramline to Hatton's Flat which is located in Devlin Street, and
- The War Memorial located in the roundabout to the north of the Civic Centre.

By contrast, a number of buildings and spaces contribute significantly to the social and cultural life of the public domain within the centre. Many of these buildings are semi-public in nature, such as the school grounds, the internalised shopping centre, function centres and clubs, and these also add to the social and cultural activity within the public realm by providing important meeting places for local residents and by attracting people to the centre.

The Ryde Civic Centre and associated public library are heavily used, both during the day and weekday evenings, by library users and for functions. The Top Ryde Shopping Centre, which occupies the largest area of the centre, contains a variety of shops and restaurants that provide important meeting places and services for the community. At present, the life of the shopping centre is mainly concentrated in business hours, with after-dark activity confined to a small number of restaurants located in its immediate vicinity along Church Street and Blaxland Road.

Similarly, Ryde Public School generates significant activity during morning and afternoon periods of school hours, and the Top Ryde Shopping Centre is a major attraction for people within the vicinity during these times. The school grounds have a limited after-school life for local sporting / training purposes.

Other key buildings that influence the social and cultural life of Ryde's public domain include:

- The Argyle Centre (Town Hall), which is also used for functions and as a polling station for elections,
- The heritage buildings surrounding the St Anne's Church precinct including the Odd Fellows Hall (1869), the Court House Building (1863), Westward Cottage (1850) and The Parsonage (1880),
- Four churches St Anne's Church (c1826), Ryde Wesley Uniting Church (1849 and c1870), Ebenezer Chapel Baptist Church (1862) and the First Church of Christ Scientist, and
- The Masonic Temple (1908).

Ryde Park represents the most significant open space for social activities, providing grounds for sports such as soccer, football and cricket, and has lights for night time use.

#### Key Issues

The cultural history of Ryde is not currently well expressed through public art or built form. The majority of activity within the centre occurs during weekday and weekend business hours.

**Opportunities**

Encourage the use of public art to promote cultural diversity, local identity and the creation of distinctive spaces.

Promote events and developments that encourage after business hours activity within the centre.

Develop opportunities for temporary installations (such as banners) to celebrate festivals, events and important cultural holidays.

Develop linkages between private and public domain cultural facilities (such as galleries and theatres) by including opportunities for public art.



**Figure 3.2.7 - Existing Public Art, Social & Cultural Precincts**



***This section establishes a series of principles and strategies to guide the character, quality and functioning of design proposals within the public domain of Ryde Town Centre. These guidelines will form part of an overall urban design framework for the centre and support the provisions of relevant planning instruments, including the Ryde Town Centre Development Control Plan and Local Environment Plan.***

***The recommendations made within this section will inform the provisions of Sections 5, 6 and 7 of this Plan pertaining to concept design details, costing and staging of infrastructure improvements for Ryde Town Centre.***

## 4.1 Access

This section identifies key provisions for the improvement of vehicular and public transport, and visual access within and around Ryde Town Centre.

### 4.1.1 Transport Network / Recommendations of Previous Studies

A number of measures have been identified within previous traffic studies for the Ryde Town Centre to improve traffic flow and safety in the city streets. Key recommendations include:

- Implementation of capacity improvements to Blaxland Road and Devlin Street through the removal of key right-hand turning movements, thus reducing the number of traffic phases. This would require removal of pedestrian crossing phases north and south across Devlin Street and replacement with a pedestrian overbridge. The pedestrian phase across Devlin Street, south of Blaxland Road would be retained,
- Prohibition of right turning movements from Devlin Street Northbound into Blaxland Road East bound (buses excepted),
- Provision of three eastbound through-lanes and one exclusive right turning lane from Victoria Road into Bowden Street, southbound,
- Provision of two westbound through-lanes and one exclusive right turning lane from Victoria Road into Bowden Street, northbound,
- Closure of Church Street, south of Victoria Road and at Devlin Street,
- Closure of Church Lane (west) at Devlin Street,
- Conversion of Blaxland Lane into one-way from Belmore Lane to Church Street,

## Transport Network

- Closure of access to and from Devlin Street near the memorial garden,
- Control of the left turning movement from Blaxland Road (west) and Devlin Street into Blaxland Road (north) by the provision of traffic signals to control the conflict between these two movements,

### CYCLEWAYS

- Provision of cycleways within the Town Centre will offer an alternative to the use of motor vehicles and public transport. Cycleways will be provided as either signed routes with or without linemarking and off road cycle routes through Ryde Park and linking through the open space of Buffalo Creek bushland corridor.






### Figure 4.1.1 - Transport Network Principles



## LEGEND

- Declassification of Blaxland Road between Devlin Street and Tucker Street and public transport enhancements
- Capacity improvements:
  - removal of key right hand turning movements.
  - removal of pedestrian crossing phases and replacement with pedestrian overbridge across Devlin Street
- Restrict through traffic in Blaxland Road between Devlin Street and Victoria Road

## Cycleways

-  On road cycleways
-  Possible future off road cycleway
-  Off road cycleways

### 4.1.2 Pedestrian Network & Pavement Widening

The following principles are intended to encourage and facilitate pedestrian access and enjoyment when moving around and through the public domain. Key aims are to:






- Enhance and expand existing pedestrian networks and connections between major functions and public spaces,
- Improve public domain finishes and facilities, including pavements and lighting to provide safe pedestrian routes,
- Improve accessibility for pedestrians at key locations through specific works including:
  - Replace the existing at-grade pedestrian crossing in Tucker Street (near Ryde Public School) with a “wombat crossing” (raised threshold),
  - Provide a wombat crossing in Pope Street to match the width of the proposed plaza associated with the shopping centre redevelopment,
  - Provide a wombat crossing in Argyle Avenue providing safe access from Ryde Park into the school grounds,
  - Provision of an “all-pedestrian” traffic phases at the intersections of Blaxland Road with Church and Tucker Streets,
  - Undertake a program of pavement widening to reduce pedestrian congestion and provide spaces for social interaction, dining and gathering,
  - Provision of pedestrian overbridges to facilitate safe movement of pedestrians through the town centre,
- Promote through-site links in all major developments,
- Encourage seamless transition between public and semi-public domains,
- Establish a data collection and monitoring program to facilitate the timely maintenance and repair of unsafe pedestrian routes,
- Provide footpath pavement widening to encourage outdoor dining, in accordance with the Footpath Activity Policy.
- **Note: Final locations of pavement widenings need to be subject to RTA approval and further design investigation.**



**Figure 4.1.2 - Pedestrian Network and Pavement Widening Principles**



## LEGEND

- |   |  |   |  |
|---|--|---|--|
|  | Footpath pavement widening                       |  | Improve public domain finishes including paving upgrade and lighting to provide safe pedestrian routes |
|  | 'All pedestrian' traffic phase to traffic lights |  | Pedestrian overbridges   |
|   |  |  | Wombat crossings (raised crossing)   |

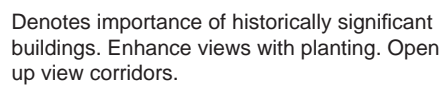
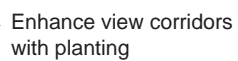
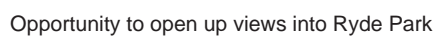
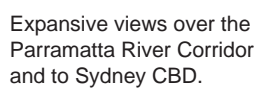


## 4.2 View Corridors

Important views and significant features within Ryde Town Centre should be highlighted and enhanced in order to improve visual connections and promote awareness of major elements of the city. Key strategies are to:

- Denote the importance of significant features and precincts, such as the heritage area in the vicinity of St. Anne's Church. This area affords the highest and best vantage point within the town centre and its importance should be recognised through the use of marker plantings such as Araucaria sp. and Cabbage Tree Palms.
- Facilitate viewing opportunities at specific locations, for example through the establishment of a lookout over the Parramatta River corridor in the vicinity of the Parsonage and Westward Cottage heritage buildings near Tucker Street.
- Maintain and enhance view corridors along major street networks, including the elevated areas of Tucker Street, Church Street and Blaxland Road, through appropriate plantings and building controls.
- Improve existing views through public domain upgrades of elements including paving, seating and street tree planting.
- Incorporate public art to enhance views, create focal points, and aid in orientation within the town centre.
- Connect Ryde Park to the town Centre by opening up views to Blaxland Road and across Ryde Public School to Top Ryde Shopping Centre.

The map shows the Topside Shopping Centre area in Victoria, BC. The shopping centre is highlighted in yellow. The proposed bus stop is marked with a black dot. Blue dashed arrows indicate the proposed bus route, and red arrows indicate the current bus route. A yellow dashed arrow points from the proposed bus stop to the shopping centre.



## 4.3 Paving

This section provides a framework for the implementation of paving treatments throughout the Ryde Town Centre public domain.

### 4.3.1 Paving Plan

Key strategies for paving are to:


- Provide high quality finishes that enhance the presentation and ambience of the public Domain,
- Provide a unified pavement system throughout the public domain that is coordinated through a consistency of design, materials and colours,
- Establish a hierarchy of paving treatments that enables the expression of differences within various precincts in the city (Refer to Figure 4.3.1),
- Select a palette of materials that are durable, hard-wearing, cost-effective, and readily available,
- Adopt a flexible approach to private domain pavements, and where possible, encourage contributions from the private sector to extend unified pavement finishes across the public and private (commercial) domain,
- Undertake a program of pavement replacement within the public domain to address existing unsafe and unattractive treatments,
- Develop effective maintenance regimes to manage potential safety risks and liabilities to Council.



### Figure 4.3.1 - Paving Principles



## LEGEND

-  Major Streets  
 Secondary Streets  
 Heritage Area

## 4.4 Green Links

This section provides strategies to enhance the natural and visual character of Ryde Town Centre, improve integration of natural systems such as the Lane Cove River and Buffalo Creek bushland corridors, strengthen the existing street tree network, and enhance important cultural, open space and recreational zones for public use.

### 4.4.1 Open Space & City Ecology

The open space reserves and natural ecosystems existing within Ryde Town Centre and its environs are important assets that should be preserved and enhanced. This can be achieved through implementation of the following strategies and design principles:





- Protect existing significant native vegetation within the town centre, in particular, the bushland corridor and mature trees that fringe Ryde Park, and the grounds of the Ryde Public School that features two significant Fig trees.
- Enhance green corridors existing along the road network.
- Enhance and expand existing open space reserves, pocket parks and public squares within the centre.
- Foster linkages between these spaces and the city core, both physically and visually.
- Protect the character and useability of important open spaces and heritage landscapes like the St Anne's Church grounds.
- Raise public awareness of the importance of natural environments, including bushland corridors and the Lane Cove River and Parramatta River systems.
- Promote positive relationships between these elements and the city centre through effective environmental education programs, site-specific interpretive elements and artworks, and promoting practices and initiatives that are environmentally sustainable, including:
  - The adoption of management strategies that encourage the use of recyclable, renewable or durable, hard-wearing resources.
  - The promotion of energy efficient practices and resources that will result in a reduction in Greenhouse Gas emissions and improve local air quality.
  - The promotion of total catchment management systems for the treatment of stormwater and the adoption of processes that will facilitate the conservation and recycling of water, improve water quality, restore natural water courses and drainage systems, and manage flood events in an equitable and sustainable manner.
  - The protection and enhancement of ecological processes, including the protection of flora and fauna life cycles.



**Figure 4.4.1 - Open Space and City Ecology Principles**



## LEGEND

- |   |  |   |                             |   |                        |
|---|--|---|-----------------------------|---|------------------------|
|  | City Core (Major Streets) - Add greenery to the city core by street tree planting and potted plants + flower displays under awnings. |  | Cultural/historic landscape |  | Significant vegetation |
|  | Secondary streets - add street tree planting - provides links to parklands, bushland and cultural landscapes                         |  | Parks/open space            |   |                        |



#### 4.4.2 Planting Palettes and Street Tree Planting

Street tree and feature planting will form an important component of the overall open space and city ecology network of Ryde Town Centre. The palette of tree plantings proposed will include both local indigenous species and feature trees selected to reflect the planting styles of the late Nineteenth Century and accent important heritage sites within the centre.

Specific strategies for street tree planting and species selection are to:

- Extend existing vegetation corridors and major plantings into the town centre to create visual links between outer lying parks, bushland reserves and the public domain of the city centre.
- Locate feature plantings to provide focal points to key areas, significant buildings and landmarks; to enhance views and vistas; and to create connections along streetscapes to visually link the open space reserves and civic spaces within and around Ryde Town Centre.
- Enhance the streetscape by planting appropriate street trees to form continuous avenues. This may require modification to the existing urban environment in some locations to maximise the number of trees that can be incorporated.
- Select tree species that appropriately reflect and celebrate the character of individual precincts within the Town Centre, such as the heritage landscapes and buildings around St Annes Church.
- Locate planting, and select species, to inform and define spaces within the public domain.
- Enhance the microclimate of the city centre through the selection of tree species that will control sunlight and shade, reduce radiant pavement heat and light reflection, and modify wind movement.

### Figure 4.4.2 - Planting Pallettes and Street Tree Planting Principles



## LEGEND

- |   |   |   |   |
|---|---|---|---|
|  | Feature planting including potted plants and small feature shrubs under awnings<br><i>Syzygium</i> 'Bush Christmas' |  | <i>Flindersia australis</i> (Australian Teak)       |
|  | <i>Lophostemon confertus</i> (Brush Box)  |  | <i>Elaeocarpus reticulatus</i> (Blueberry Ash)      |
|  | <i>Araucaria cunninghamiana</i> (Hoop Pine)   |  | <i>Elaeocarpus eumundi</i> (Smooth-leaved Quandong) |

## 4.5 Street Furniture

The following strategies refer to the implementation of signage and street furniture within Ryde Town Centre, including design principles that govern styles, colours and locations for these elements.

### 4.5.1 Signage (Refer to Council's Civic Signage Manual 2008)

An effective hierarchy of signage treatments within Ryde Town Centre is necessary to create a unified, interesting, informative, readable and navigable city. An integrated signage strategy should be developed that provides a framework for the establishment of:

- Appropriate directional signage to major recreational, functional and heritage areas, in order to improve the ability to navigate around city.
- Effective interpretation and awareness of important features of the centre, including historical references, environmental issues, important civic places and buildings, significant views and major events.
- High quality signage that is clear, legible and informative, and which is designed specifically to reflect the character of Ryde.
- Signage that incorporates artwork, where appropriate, to communicate important information.

### 4.5.2 Street Furniture

Street furniture, including seats, tables, fencing, bollards, lighting, banner poles and drinking fountains, provides an important functional role in the public domain, but may also be considered as a vehicle to reinforce characteristics of Ryde Town Centre. Key strategies and design principles for the use of street furniture are therefore to:

- Integrate artwork elements into street furniture and select colours, materials and styles that are distinctive to Ryde, and which reflect the character of individual precincts within the centre in accordance with the Draft Public Art Policy.
- Locate street furniture elements to best serve the function of the area that it is in, and to avoid conflicts between different uses.
- Locate lighting to serve major pedestrian accessways and spaces, and create unity within the town centre.
- Provide lighting that is intimate in scale to the streetscape, and which can become a vehicle for artwork or the integration of banners and signage.
- Use feature lighting to highlight significant features, landmarks, buildings and plantings,
- Increase the safety of the town centre.
- Provide banners at key locations in accordance with the Draft Banner Policy.



### 4.5.3 Lighting

Lighting is an important public domain element, contributing to the overall life and safety of spaces at night. The current lighting within the town centre is large-scale highway lighting along the roadway with little pedestrian-scale lighting. A hierarchy of lighting should be established that will:

- Provide intimate-scale lighting for pedestrians to improve community safety.
- Unify the streetscape, define spaces and add to the overall identification of the city centre.
- Use lighting to highlight major features within the town centre, such as important buildings or artworks.
- Allow for the incorporation of artworks and temporary banners into lighting fixtures.
- Be cost effective and coordinated with Council's electricity supplier to ensure that on-going maintenance requirements are addressed.

### Figure 4.5.1 - Street Furniture Principles





## LEGEND

## LIGHTING

- Lighting for heritage buildings and structures
- Park lighting

NOTE: Investigate feature lighting to significant trees

-  Pedestrian overhead lighting to city core
-  Security lighting

## SIGNAGE

-  Gateway
-  Park Identification
-  Information
-  Regulatory
-  Directional

## 4.6 Safety & Amenity

Public amenity and the perception of safety within the public domain will be major influencing factors in the overall functioning and use of Ryde Town Centre. This section presents strategies intended to ensure public safety in the town centre.

The creation of a safe environment and high level of public amenity will be achieved through implementation of the following strategies and design principles:

- Undertake a program of footpath improvements, including pavement widening, new paving and street furniture, to encourage and facilitate pedestrian activity. Pavement widening in particular provides opportunities for meeting places within the public domain, e.g. through use for outdoor dining or public seating.
- Undertake a program of upgrading pedestrian crossings and installing additional traffic calming facilities to encourage safe pedestrian movement within and around the town centre.
- Apply best practice techniques and ‘*Safety by Design*’ principles in the design of outdoor areas.
- Promote activities and developments that encourage after hours use in the core of the town centre, in order to increase pedestrian numbers at night and increase the perception of safety.
- Facilitate use of Ryde Park at night by providing lighting along the major thoroughfares and supporting after hours use of the lit sporting fields for training.
- Provide lighting along major pedestrian networks throughout the town centre.
- Encourage through-site access within new developments where possible, promote mixed use developments (including residential, commercial and retail) to ensure pedestrian activity before, during and after standard business hours.
- Access for people with disabilities.
- Ensure equity and access for all e.g kerb ramps, tactile pavers at change of levels etc, and improved paving.



**Figure 4.6.1 - Safety and Amenity Principles**



**LEGEND**

- Park lighting - denote safe after hours route through park
- Under awning lighting

## 4.7 City Spaces & Street Life

This section provides strategies that will contribute to the creation of effective spaces and places within Ryde Town Centre. It outlines principles for space creation and enhancement, recommendations for activity integration and opportunities for supporting cultural richness.

### 4.7.1 Spaces

The physical layout and look of a city influences the legibility, navigability, safety and usability of the spaces within it. The strategies for creating and enhancing city spaces within Ryde Town Centre are to:

- Provide attractive, interactive spaces within the public domain for people to meet and socialise.
- Provide a variety of spaces that address community needs in an organised manner.
- Establish a palette of public domain finishes, including seating, lighting and paving, that will create a unified image and identity for the town centre.
- Encourage through site connections within new developments where possible, to increase accessibility within and around the city.
- Preserve and enhance existing cultural and natural spaces within and around the town centre.
- Explore opportunities to extend the public domain through the establishment of partnerships with developers and private landholders.
- Promote high quality urban design finishes that contribute to the seamless integration of public and private domain interfaces.
- Implement appropriate landscape treatments and tree planting programs to create visual connections within the town centre, and to improve the microclimate of the city's major spaces.

### 4.7.2 Active Frontages

The creation of active frontages within the public domain at street level will serve to enliven the town centre, improve safety, and enhance business and social life through increased opportunities for human interaction. Strategies for activating the city are to:

- Concentrate active uses along building frontages in the city centre and major pedestrian routes.
- Consider grouping like uses, such as restaurants and cafes, in close proximity to each other to create precincts of lively activity.
- Encourage common setback distances along street frontages to improve accessibility. Wider street frontages should be promoted to allow for outdoor dining opportunities.
- Reduce pedestrian conflicts along street frontages through the control of driveway locations and loading zones.

### 4.7.3 City Events

A sense of place is not only created by physical arrangements of land uses and spaces, but through the interaction of these spaces with recreational and cultural activities. This section presents a number of 'place-making' strategies for Ryde Town Centre that will attract and provide for a diverse range of activities and events. These are to:




- Provide and maintain spaces and infrastructure (including lighting, power and water supply) for outdoor dining, markets, festivals and entertainment.
- Create spaces and infrastructure that are flexible and robust, and which can be adapted for a range of different uses over varying time-frames.
- Recognise and celebrate cultural diversity within the Ryde community through the promotion of events, markets and festivals that express differences in an innovative manner within the spaces and streets of the public domain.
- Improve community safety through the application of 'Safety by Design' principles and establishment of a community crime prevention program.



**Figure 4.7.1 - City Spaces and Street Life Principles**



## LEGEND

- |   |                           |   |                  |
|---|---------------------------|---|------------------|
|  | Events / Festivals        |  | Active frontages |
|  | Outdoor dining encouraged |   |                  |

## 4.8 Public Art, Social & Cultural Facilities

The character of Ryde is strongly influenced and enhanced by the heritage assets within the city centre, particularly St Anne's Church and the surrounding heritage buildings in Church and Turner Streets. At present, cultural and heritage artifacts, including sculptures and other artworks are limited in the town centre. The following strategies will facilitate the creation of a distinctive, memorable, and informative public domain that builds on the unique local identity of Ryde. Key strategies are to:

- Promote awareness of Ryde's heritage through the use of materials and artworks that reflect the history of the area.
- Develop and implement a system of attractive and informative signage to interpret the development of Ryde, for example, communicating the alignment and boundaries of the original town centre, and other key aspects of the city's growth.
- Incorporate elements in the public domain that prompt recognition and reflection on the Aboriginal and European history of the region.
- Reflect and engage with the community of Ryde through the use of public art that embodies the ideals, aspirations and interests of the people, and which promotes awareness of and connections between people, places and the environment.
- Support community involvement in the development and implementation of public art, social and cultural works, for instance, by engaging local artists and historians,
- Identify all items of public art, memorials, monuments and sculptures, etc. within a public register and develop specific strategies for their maintenance and management. These strategies should outline options and proposals for the upgrading and interpretation of these valuable assets.
- Identify opportunities to provide artistic lighting to significant elements such as trees, buildings and monuments.
- Preserve and enhance important centres for social and cultural interaction, including existing open space reserves, recreational and entertainment facilities.



**Figure 4.8.1 - Public Art, Social and Cultural Facilities Principles**



## LEGEND



Top Ryde Entertainment Precinct - Shopping, public plaza



Main Street Precinct - outdoor dining



Public art location - to denote entrance to Ryde Park



Church Street/ Church Lane/ Turner Street Cultural Precinct

- For Public Art refer to section 5.7



*This section presents design solutions and details for the Ryde Town Centre that address a range of specific public domain issues, including intersection treatments, public open space treatment, outdoor dining arrangements, paving heirarchies and materials, street tree planting and street furniture. These design solutions are based on the analysis presented within previous sections of this report and reflect the aims and objectives established throughout.*

## 5.1 Introduction

In order to facilitate the expression of individual characteristics of the various precincts and spaces within the town centre, a heirarchy of treatments has been established.

Ryde Town Centre can be effectively classified into two main types of public domain treatments. These are:

- The major streets - the main town centre area surrounding the Top Ryde Shopping Centre and the Civic centre, as well as Blaxland Road and Church Street, and
- The secondary streets - the remaining areas of the town centre, including residential streets.

The treatments proposed will reflect a unified palette of materials and design solutions across the precincts, with key variations of paving finish and planting to reflect differences between them. The major and secondary streets as established for the town centre are illustrated in Figure 4.3.1.

## 5.2 Paving Types

As examined within Section 3.2.2 of this report, the Ryde Town Centre public domain is currently characterised by a range of mis-matched paving styles that includes concrete, bitumen, heritage paving (sandstone and bitumen) and unit pavers. The application of these paving types reflects a lack of continuity across various areas of the city.

Ryde City Council has investigated, and begun to trial, new paving treatments on the outskirts of the Ryde Town Centre area. The first of these demonstration sites was on the corner of West Parade and Anthony Road, West Ryde. This site was part of a paving and streetscape design prepared by Clouston Landscape Architects (22/05/03) for the redevelopment of the Woolworths site. The materials chosen were granite pavers. The colours for the paving were 'Raven Black' as the main paving areas and 'Sesame Grey' as the feature paving. The paving pattern chosen was 300 x 300mm pavers laid diagonally with smaller, infill pavers.

The second of the trial sites was the Cox's Road neighbourhood shopping centre at North Ryde and was designed by Councils in-house design team. The paving treatment chosen was a combination of 400 x 400mm Granite pavers used for the trim / banding with an infill of clay pavers and 'Bluestone' coloured concrete.

Based on the effective trial of granite pavers at these two sites, it is proposed that the use of granite pavers be continued throughout the Ryde Town Centre Public Domain area. Some of the advantages of granite paving include:

- The dense structure and shape of the stone enables the pavers to be cut to size and shape as required, and also contributes to stain resistance and long-term durability.
- Artworks and patterns, etc. can be readily sandblasted into the paving surface.
- Imports of granite from China have reduced the cost of the supply of granite, making it comparable in cost to reconstituted products such as Pebblecrete and Urbanstone.
- The finish of the product will enhance the character in the public domain.
- Granite is compatible with the use of other products such as concrete, bitumen etc.
- Granite is extremely hard wearing if laid on a firm subbase.

The proposed paving although granite needs to be subject to a trial pilot project which shows the proposed colour and proposed paving pattern. The chosen granite paver should be subject to the following criteria:

- Durability,
- Resistance to staining (influences colour choice),
- Ease of maintenance / cleaning,
- Compatability with other design elements in the public domain such as furniture, signage etc.
- Slip resistance,
- Availability of long term supply,
- Compatibility with heritage areas,
- Suitability to trafficable areas.



## Paving treatments - Project Examples

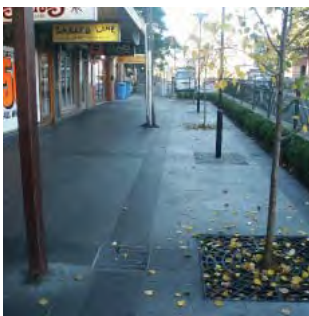


Granite Paving

It is proposed that granite pavers are used in the major streets (town core) of Ryde Town Centre. The paving is to run in a stretcher band pattern across the footpath. Corners can be highlighted using the pavers laid in a diagonal pattern. Refer to 'Details'.

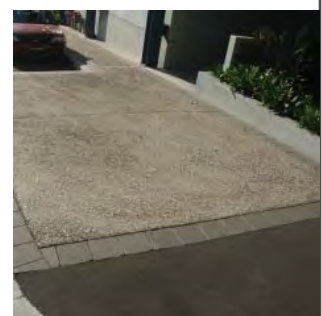


600 x 300mm granite paving

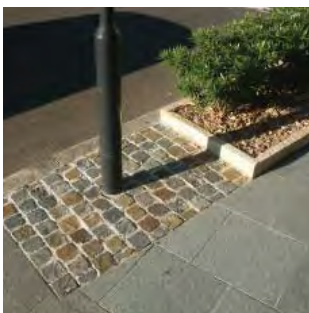


Tree and Shrub Planting along pavement edge

The major streets will be fully paved with granite pavers. This will provide opportunities to incorporate other paving treatments such as granite or porphyry setts to tree surrounds.

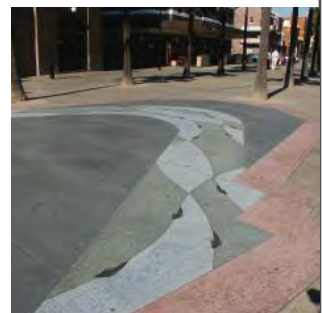


Granite paving with exposed aggregate concrete infill



Combined pavement treatments

In secondary streets the pavement treatment will be either a coloured or textured concrete or bitumen pavement with a granite banding to relate to the major street banding. Maximum distance between banding is proposed to be 6.0metres.



Pavement artworks



Granite paving with textured finish

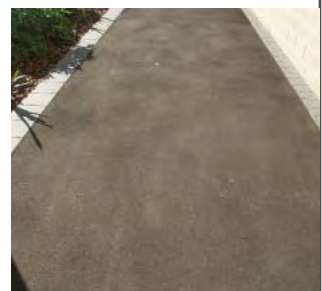
Banding at property boundaries and corners. Where appropriate use banding to denote and give emphasis to interface between public domain and buildings. eg. at entries.



Granite setts

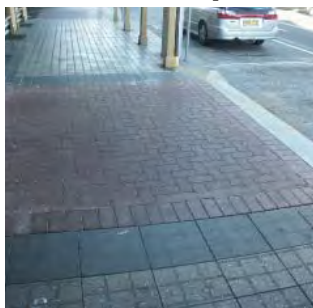


Pebblecrete paving trim + banding with bitumen infill



Granite paving trim

## Road / Footpath intersections treatments - Project Examples



Pavers as a threshold across a laneway



Raised paved threshold



Road/footpath junction



Coloured bitumen threshold



Granite setts as threshold

Paving materials used for footpaths will also need to be coordinated with the treatment of pedestrian crossings and other traffic devices within the town centre.

Due to wear and maintenance implications of pavement surfaces in a vehicular environment, options for pavement materials for thresholds and crossings should be investigated.

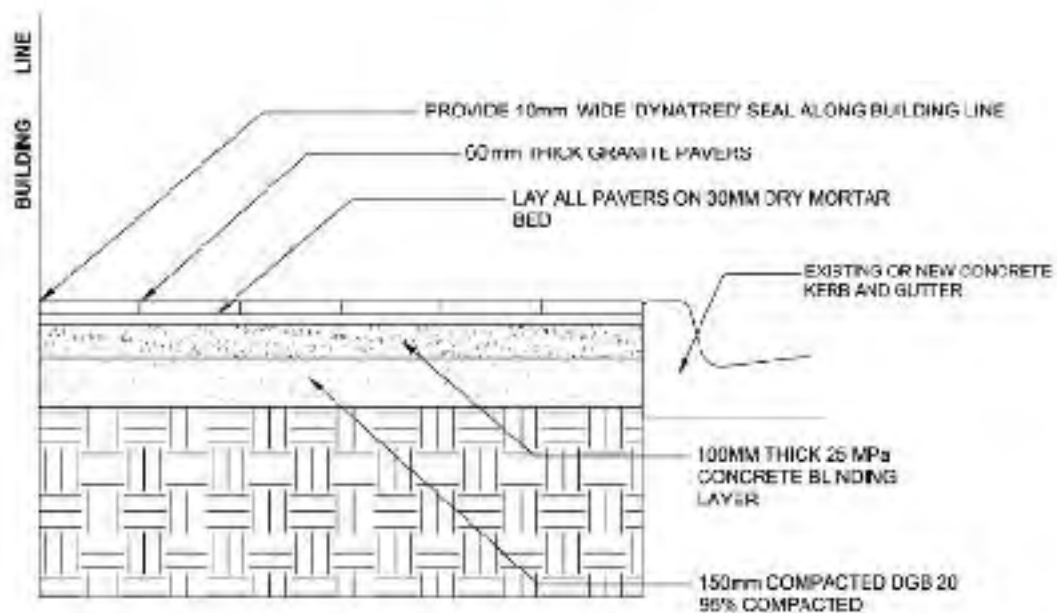
Options include the use of smaller format granite pavers, such as 90 x 90mm setts, or 230 x 110mm granite pavers (or similar) across roadways. Another alternative is the use of coloured asphalt, coloured to match the chosen paving colours.

The use of smaller format pavers can also be used to denote shared pedestrian / vehicle zones within the laneways of the centre.

## Details - Major Street Paving



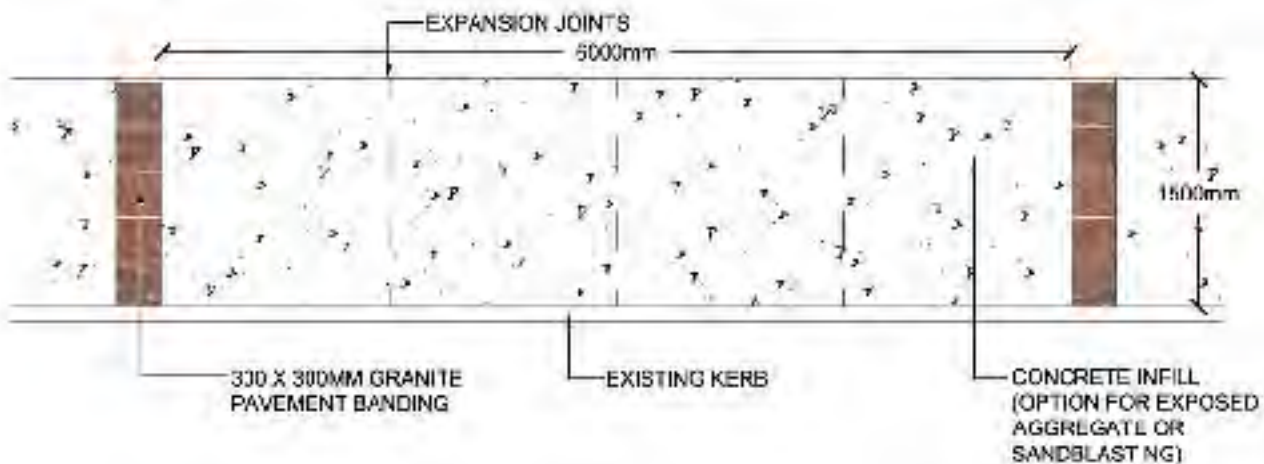
**Plan - Not to scale**



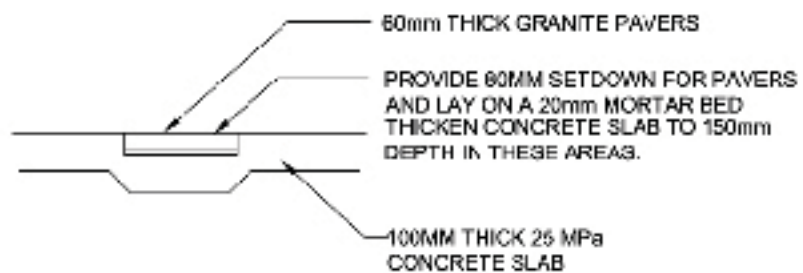
**Section - Not to scale**



## Details - Secondary Street Treatment



**Concrete infill plan**  
N.T.S



**Concrete infill section**  
N.T.S



Major street treatment - Blaxland Road, Ryde Town Centre





Argyle Ave (full width )



Victoria Road (full width)

## Secondary street treatment - Ryde Town Centre



Curzon Street (retaining verge with trees)



corner of Curzon St and Smith St  
(full granite paving at corner)



## 5.3 Street Tree Planting and Treatments

### 5.3.1 Street Tree Planting

Historically the development of the town of Ryde centred on St Anne's Church. The glebe lands surrounding the church were noted for their heavily treed grounds. These trees included a combination of exotic species such as Pines, Oaks and Elms, and native species including Eucalypts and rainforest trees such as Figs and Araucarias. Norfolk Island pines became landmark feature trees on the ridgeline.

It is proposed that the use of street trees in the town centre will be selected from a palette of rainforest trees to reflect the use of these trees at the end of the 19th century. Many of these rainforest trees are still used as street trees today. Species will be selected from the following list:

#### LANDMARK FEATURE TREES

- *Ficus rubiginosa* (Port Jackson Fig)
- *Araucaria cunninghamiana* (Hoop Pine)
- *Agathis robusta* (Kauri Pine)
- *Livistona australis* (Cabbage Tree Palm)

#### STREET TREES - without overhead wires

- *Lophostemon confertus* (Brush Box)
- *Harpullia pendula* (Tulipwood)
- *Flindersia australis* (Australian Teak)
- *Podocarpus elatus* (Plum Pine)
- *Elaeocarpus reticulatus* (Blueberry Ash)
- *Waterhousia floribunda* (Weeping Lilly Pilly)
- *Elaeocarpus eumundi* (Smooth-leaved Quandong)

#### STREET TREES - under wires

- *Buckinghamia celsissiana* (Ivory Curl Flower)
- *Backhousia citriodora* (Lemon Scented Myrtle)
- *Acmena smithii* 'Minor' (Creek Lilly Pilly)
- *Syzygium* and *Acmena* sp. cultivars to 5m tall.
- *Tristanopsis laurina* (Water Gum)
- *Lepiderema pulchella* (Fine-Leaved Tuckeroo)

#### UNDER AWNINGS

Under awnings, smaller growing Lilly Pilly cultivars such as *Syzygium* 'Bush Christmas', *Syzygium* 'Cascade' and *Syzygium leuhmannii* can be shaped to provide greenery.

## STREET TREES

Deciduous trees are to be used where solar access is required and to provide feature trees in key locations:

- *Pyrus ussuriensis* (Manchurian Pear)
- *Pyrus calleryana* 'Bradford' (Ornamental Pear)
- *Koelreuteria paniculata* (Golden - Rain Tree)
- *Lagerstroemia* 'Indian Summer' (Crepe Myrtle)
- *Malus ioensis* 'Plena' (Bechtel Crab)
- *Schinus ariera* (Pepper Tree)
- *Toona australis* (Red Cedar)

## Street tree planting treatments



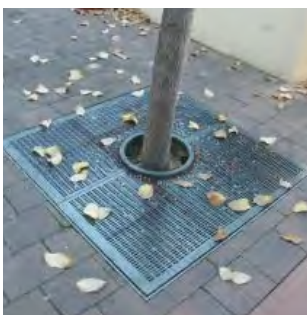
Steel tree guards



Paving surrounds with decomposed granite infill



Paving surrounds with mulch and understory planting



Tree grates



Granite setts terrabond infill

In the major streets (town core) the treatment of the street trees is to include a pink paving surround, grey granite setts and grey terrabond infill to match the colour of the grey granite pavers.

In the secondary streets the street tree planting in some locations will be underplanted with ornamental native grasses such as *Dianella caerulea* or *Lomandra 'Tanika'*.

Refer to Fig 4.3.1 'Paving Principles' on p 41 for more detail.



Street trees with groundcovers under as secondary treatment



Paving surrounds with planting



Paving surrounds with decomposed granite infill



Paving surrounds with decomposed granite infill



Granite setts terrabond infill



[illegible]

STEEL RAILS 75 X 50MM

800 X 800MM PAVERS

TERRABOND

200 X 200MM PAVEMENT BANDING

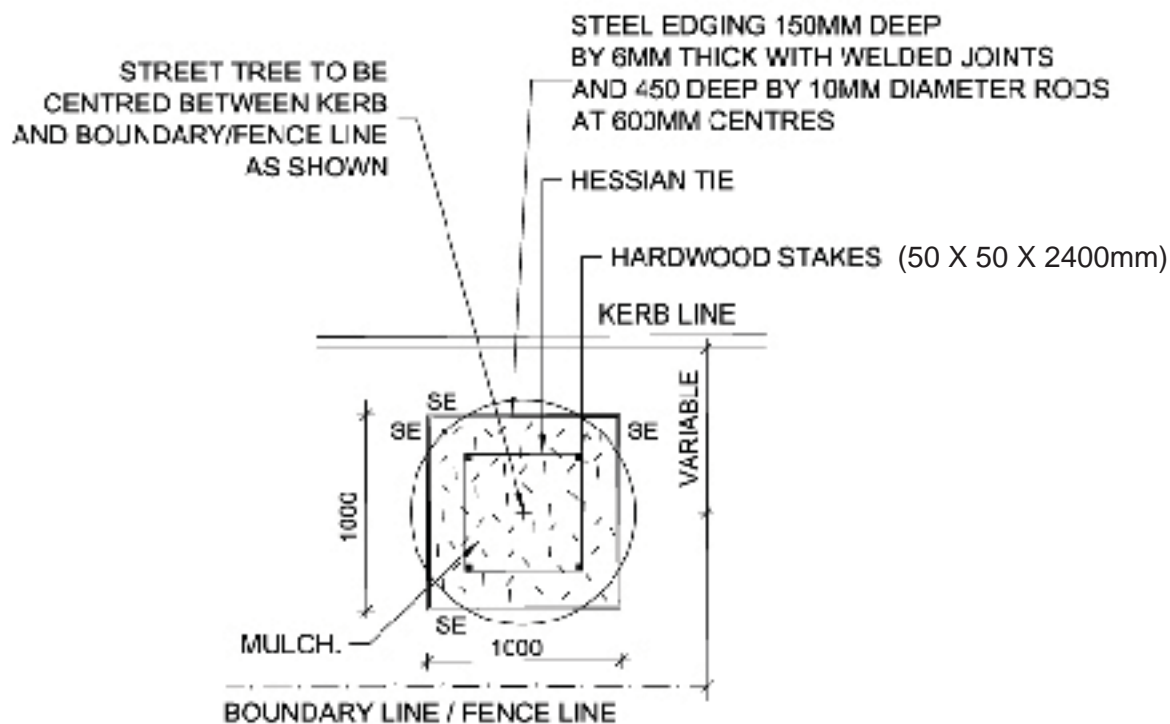
STEEL STAKES 100x100x2400MM

STEEL EDGE

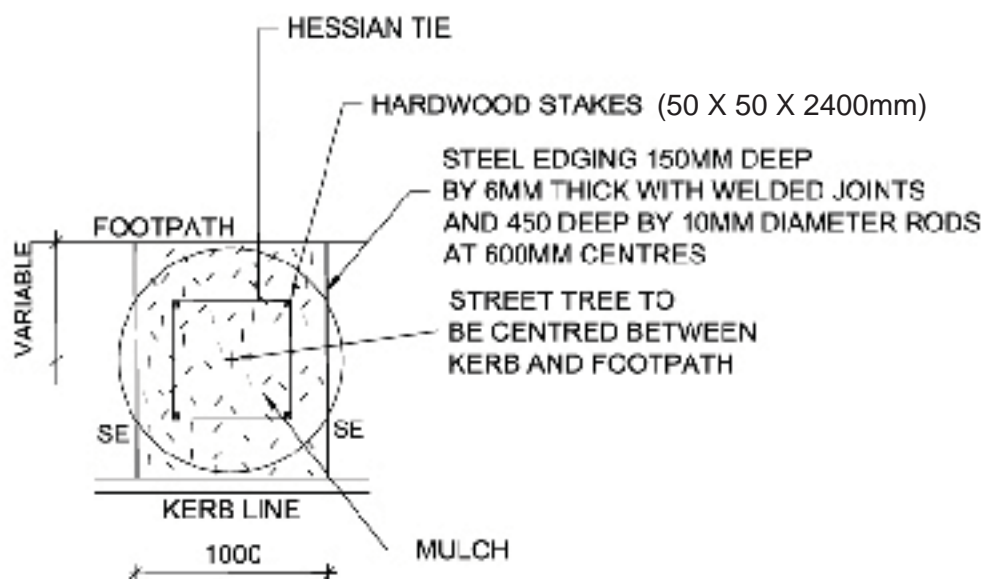
EXISTING KERB

Ryde Town Centre Public Domain Plan  
City of Ryde July 2006

## Typical Tree Treatment Detail in Secondary Streets



**Option 1 Plan**  
No footpath - N.T.S



**Option 2 Plan**  
With footpath - N.T.S

## 5.4 Open Space & Understorey Planting Palette

Further to the proposed street tree species identified in Section 5.3, a range of additional native and exotic species have also been identified for planting within the open space areas of the town centre, such as the Ryde Public School grounds, Ryde Park and the open space adjacent to Devlin Street at the end of Church Lane. Many of the street tree species listed in Section 5.3 will also be suitable for these locations.

The predominant vegetation association indigenous to the Ryde area is Turpentine-Ironbark Forest, supported in soils derived from Wianamatta Shale (Benson & Howell 1990, p.124-129). While the majority of the original bushland vegetation in the region was cleared early in the settlement of Ryde, a good representative example of this vegetation community still exists at Wallumetta Nature Reserve, located at the corner of Twin and Cressy Roads, East Ryde. The area around Buffalo Creek is a transitional vegetation zone with the clay soils of the Wianamatta Shale giving way to outcrops of sandstone. Vegetation here changes from Blue Gum Forest dominated by *Eucalyptus saligna* (Blue Gum) growing in the shale derived soils and giving way to open forest woodland and heathland that is characteristic of sandstone soils.

A number of individual, mature, remnant trees also exist scattered throughout the town centre area. In order to reinforce this natural character, a significant component of the planting used throughout the town centre should reflect the indigenous vegetation of the region. This will be achieved through a combination of tree, shrub and groundcover / grass planting, however due to safety concerns over the potential for shrubs to block sight lines, tree and groundcover species will be the primary planting used in the open space areas.

Tree planting will provide a continuous canopy and habitat in areas such as Ryde Park, Ryde Public School and the adjacent Buffalo Creek Corridor (Burrows Park) on Princes Street, and will also be important in providing shade. More ornamental plantings of native grasses and groundcovers can be used to enhance gateways (such as signage etc.) and around the base of artworks and street tree plantings.

Exotic species will also form an integral part of the overall planting palette, to be used as feature or accent planting to compliment the native species, and in locations where native species are considered to be less suitable, such as traffic islands and roundabouts, and where it is desirable to use deciduous trees that will allow sunlight in winter and shade in summer to important public spaces.

Exotic species also add flower colour which is desirable for the community. The plants chosen are also drought resistant which is important in times of water restrictions and ongoing maintenance.



## Indigenous Species Palette

### TREES

- *Syncarpia glomulifera* (Turpentine)
- *Eucalyptus resinifera* (Red Mahogany)
- *Angophora costata* (Sydney Red Gum)
- *Eucalyptus saligna* (Sydney Blue Gum)
- *Eucalyptus haemostoma* (Scribbly Gum)
- *Eucalyptus pilularis* (Blackbutt)
- *Eucalyptus punctata* (Grey Gum)
- *Glochidion ferdinandi* (Cheese Tree)
- *Elaeocarpus reticulatus* (Blueberry Ash)
- *Leptospermum polygalifolium* (Yellow Tea Tree)
- *Melaleuca decora* (Paperbark)
- *Banksia serrata* (Old Man Banksia)

### GROUNDCOVERS / GRASSES

- *Commelina cyanea* (Scurvy Weed)
- *Dianella caerulea* (Paroo Lilly)
- *Dianella* 'Breeze' (Dianella)
- *Dianella* 'Cassa Blue' (Blue Dianella)
- *Dianella* 'Tas Red' (Red Dianella)
- *Dianella revoluta* (Spreading Flax Lily)
- *Hardenbergia violaceae* (Purple Twining Pea)
- *Imperata cylindrica* (Blady Grass)
- *Kennedia rubicunda* (Dusky Coral Pea)
- *Lomandra longifolia* (Lomandra)
- *Lomandra* 'Tanika' (Fine Leaf Lomandra)
- *Microlaena stipoides* (Weeping Grass)
- *Pandorea pandorana* (Wonga Vine)
- *Themeda australis* (Kangaroo Grass)

## Exotic Species Palette

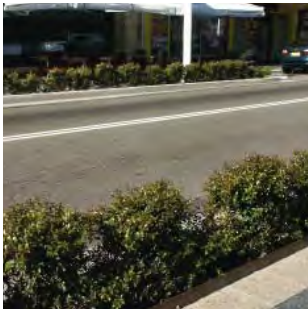
### SHRUBS

- *Nandina* 'Gulf Stream' (Dwarf Nandina)
- *Syzygium* 'Bush Christmas' (Dwarf Lilly Pilly)
- *Syzygium* 'Tiny Trev' (Dwarf Lilly Pilly)
- *Syzygium* 'Lillyput' (Dwarf Lilly Pilly)
- *Syzygium* 'Cascade' (Dwarf Lilly Pilly)
- *Strelitzia parvifolia* (Bird of Paradise)
- *Agapanthus* 'Snowball' (Agapanthus)
- *Hebe* sp. (Hebe)
- *Agave attenuata* (Agave)
- *Nerium* 'Dwarf' (Dwarf Oleander)

### GROUNDCOVERS / GRASSES

- *Convolvulus cneorum* (Silverbush)
- *Convolvulus mauritanicus* (Ground Morning Glory)
- *Liriope* 'Evergreen Giant' (Giant Liriope)
- *Trachelospermum* 'Tricolor' (Variegated Star Jasmine)

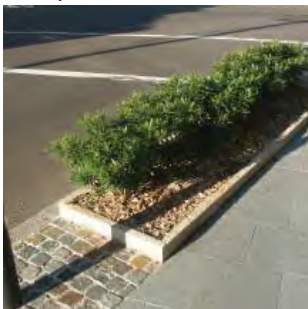
## Planting Styles / Treatments



Hedge planting to dining areas



Planting to separate roadway from pedestrian environment



Planting to separate roadway from pedestrian environment



Feature planting to soften built form



Hedge planting to soften built form

The addition of plants to the public domain can soften the built environment in a number of ways.

This includes:

- Feature plantings to highlight key locations of historic spaces, buildings or events,
- To provide a delineation between dining areas and the adjacent roadway,
- To provide shade and shelter in hard urban environments, and
- To provide a sense of human scale in relation to the built environment.
- Raised kerb surrounds to planting areas.



Feature planting to streetscape



Feature landmark planting



Planter boxes to define the road edge



Feature landmark planting



Planting to separate roadway from dining areas



## 5.5 Furniture

The furniture for the public domain is a coordinated palette of seating, garbage bins, bollards and fencing that complements the public domain finishes such as the paving colours. This has helped to create a unified, distinct character for Ryde Town Centre.

### 5.5.1 Signage

In 2008 Council prepared a Civic Signage Manual that specifies a uniform signage 'family' for the entire City. This manual includes the design for heritage interpretation signs that will be installed in the heritage area around Church Street.

### 5.5.2 Street Furniture

The majority of the street furniture comprises standard, ready-to-order items supplied by Botton and Gardiner Urban Furniture. Items, such as seats, garbage bins and tables can be customised to suit the needs of Ryde Council, for example to incorporate the Council's corporate colours and logo. The proposed furniture models are:

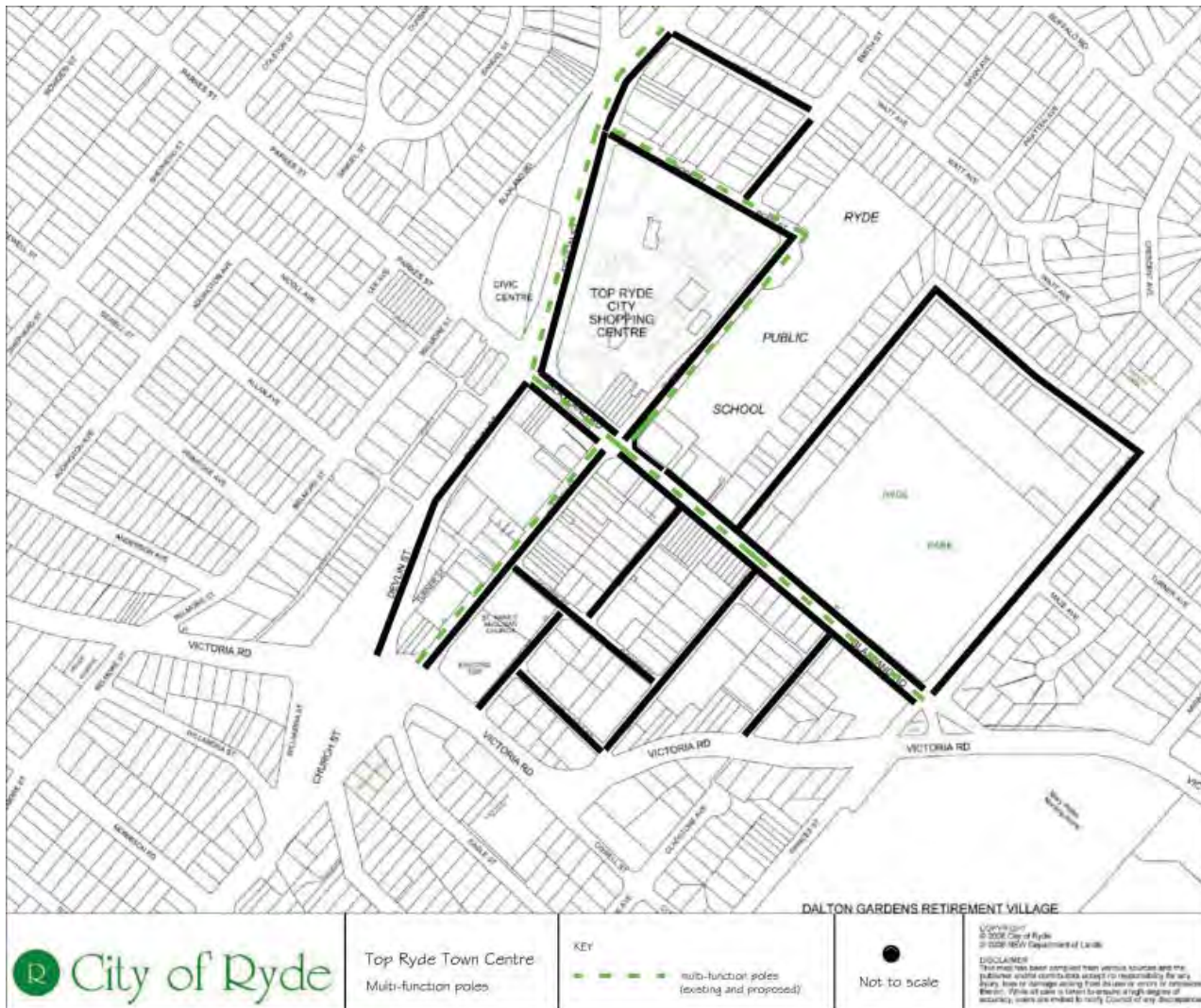
- Urban seat 11 in aluminium (2.1m length)
- Urban picnic table UT11X with 2 x BSIX bench seats. (2.1m length) powder coated frames (colour green) with oiled jarrah timber slats.
- Garbage bins -120 Litre mobile garbage bin housing MGBHHAL powder coated (colour green).

Opportunities also exist to include custom designed, artist-inspired furniture that will celebrate specific aspects of Ryde's character or history. Due to the higher costs involved in the design and production of this type of furniture, it is proposed to limit the use of specialty items to key sites and spaces.

### 5.5.3 Lighting

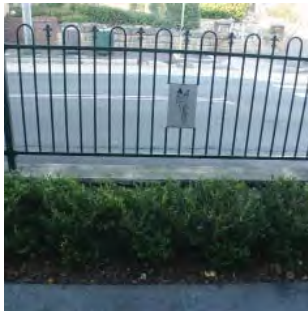
The current lighting within the town centre is highway lighting and needs to be replaced with more intimate pedestrian lighting including underawning lighting where relevant. The chosen fittings should be coordinated with Council's electricity supplier to ensure ongoing maintenance requirements.

Council has installed multi-function poles in the major streets in the Town Centre. These combine street lighting, pedestrian lighting, signage and banners. **See plan below showing the multi-function pole locations.**





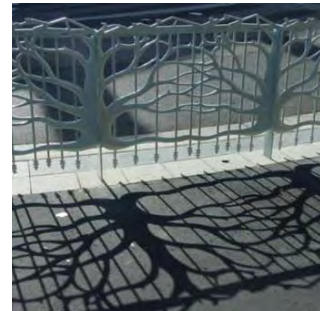
## Furniture treatments



Fencing



Fencing - Artworks by Ruth Downes



Fencing - Artwork by Milne & Stonehouse



Lighting



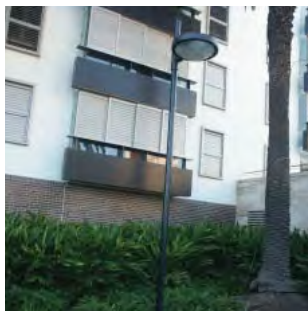
Smart Pole Lighting



Artworks - By Melanie El Mir



Bin



Lighting



Fencing artworks by Marla Guppy



Bin



Artworks - By Nick Bray & Ball  
Sculptural & Architectural



Outdoor furniture



Seat and table



Seat



Planter artwork by Ruth Downes



## 5.6 Outdoor dining

The use of footpaths and public plazas for outdoor dining is encouraged as a means of promoting activity within the city, particularly at night.

Council has an Outdoor Dining Policy (2010) available for viewing on Council's website. Application forms to apply for an outdoor dining permit are available on the website. Fees levied for outdoor dining cover the costs of cleaning the paving and maintaining any plantings used in the outdoor dining areas. Permits outline the type of seating and tables to be used, type of cafe dividers (with/without advertising) and type of umbrellas (with/without advertising) that will be accepted.

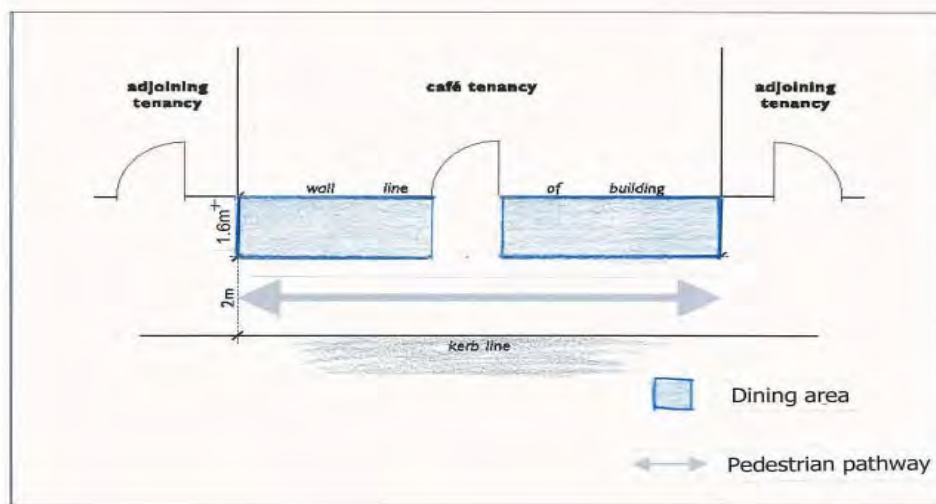
Suitable locations for proposed outdoor dining have been identified in Section 4 of this report. These included Blaxland Road between Edward Street and the Argyle Centre (extending one block into Argyle Street), extending to Ryde Park, Church Street (eastern side) between Blaxland Road and Gowrie Street and Pope Street (southern side) between Devlin Street and Smith Street.

The style and design of each outdoor dining area will be influenced by the topography of the areas where the outdoor dining is to take place. Pavement gradients in dining areas should be approximately 2% (1:50) to provide a relatively flat surface for tables and chairs, but also allow for surface drainage. In Church Street, dining terraces will be constructed by Council because of the steep topography of the street.

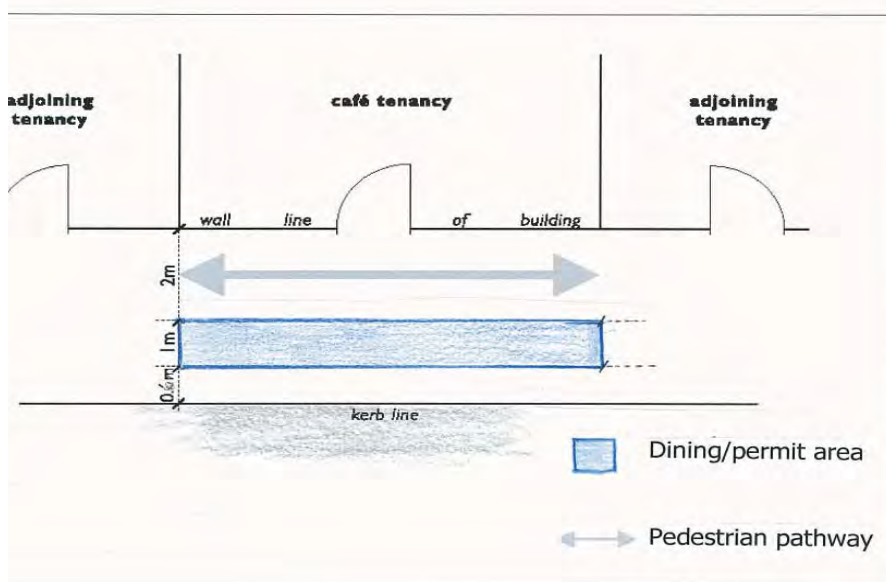
To define outdoor dining areas for leasing purposes Council requires pavement markers or other methods (see section 3.2 of the Outdoor Dining Policy). Dining areas may also require protection from vehicles on the adjacent roadway. This can be achieved by planting (in the form of a low hedge), fencing or bollards inset along the kerb.

It is important to incorporate artworks into the outdoor dining areas to reflect the character of the town centre and provide incentive for community pride in the area. Artworks may be incorporated into fencing, bollard or planter box elements to achieve this. It is also important to allow for public spaces within these outdoor dining areas so that the community can use these spaces.

The diagrams below are from Ryde's *Outdoor Dining Policy 2010*.



The above diagram shows location for a 2m wide pedestrian corridor where outdoor dining is proposed against the shopfront of the premises. The diagram below shows the location of the 2m wide pedestrian corridor where outdoor dining is proposed along the kerb line. Note the 600mm wide clearance from the kerb where there is a parking lane in the adjacent road.



Location of tables and chairs is also an important consideration, with the options being either immediately adjacent to the roadway or against the building. In either scenario, a minimum of 2.0m for pedestrian access is required to be maintained.



## Outdoor dining treatments



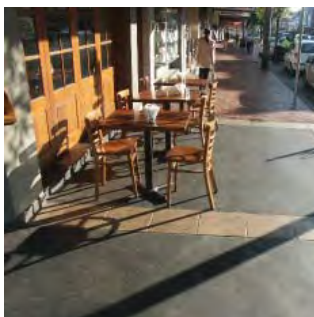
Church Street Parramatta



Church Street Parramatta



Willoughby Rd Crows Nest



Darling Street Balmain



Church Street Parramatta



Willoughby Rd Crows Nest



Norton Street Leichhardt



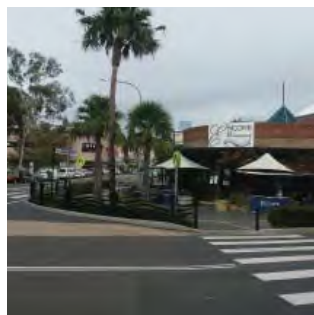
Church Street Parramatta



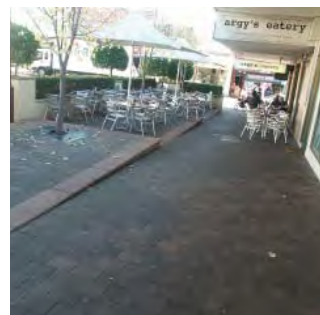
Willoughby Rd Crows Nest



Norton Street Leichhardt



Church Street Parramatta



Willoughby Rd Crows Nest



Cowper Wharf Rd Woolloomooloo Bay



Crows Nest

## 5.7 Public Art

### 5.7.1 Objectives

Creative, meaningful design and placement of artworks within the Ryde Town Centre will:

- Reinforce place making and the special character of Ryde.
- Slow down the pace of movement and reinforce the sense of pause and breath.
- Enhance pedestrian enjoyment.
- Activate memories and stories that connect people to Ryde Town Centre.
- Reconnect disparate areas.
- Strengthen the wealth of cultural identity.
- Inspire community pride in the local area.
- Assist navigation through the town centre.

### 5.7.2 Design Principles

Proposed artworks should be site specific and should respond to the distinctive character of the people, culture and places of Ryde. Artworks should be integrated with built elements or furniture and provide a subtle layer of interpretation or interest, or else, may form a major feature in themselves that acts as an intervention in the site.

The commissioning and implementation of artwork elements is governed by an overall Arts and Cultural Policy prepared by Council. This document provides guidelines on specific locations for artworks and requirements for developer contributions to public art within major commercial developments.

### 5.7.3 Site Specific Themes for Ryde town Centre

#### River Connections

The transport of produce and connection of people to Sydney and Parramatta via the Parramatta River was a vital element in Ryde's development. Historically therefore, the township was heavily focussed on the river, and even its former name of Kissing Point referred to its position on the river. With the growth of the road network, Ryde was separated physically from the river. Artworks should focus on the historical importance of this earlier connection and may also draw on contemporary issues relating to the river, including environmental quality.

#### Fruit Bowl

The Field of Mars area in Ryde's early settlement was referred to as the "Fruit-bowl" of the colony. Orchards of oranges, grapes and various stone fruits supplied markets in Sydney. A legacy of Arthur Phillip's tenure as Governor was the development of a self-sufficient farming industry, thus avoiding reliance on food supplies from overseas.

Kissing Point was an important producer of grain and fruit for the colony in the Nineteenth Century, but for thousands of years prior to European settlement, the fertile lands had also provided an important food source for the indigenous Aboriginal people. The “fruit-bowl” theme allows for the integration of indigenous and post colonial interpretive layers using imagery from “bush tucker” to citrus and stone fruits.



### European heritage

The European heritage of Ryde, contained within its buildings, parks and narratives is a valuable element that can be interpreted through artworks. Architectural elements from historic buildings, such as the ornate fretwork of the bargeboards and finials, the lacework of the balconies, and the pattern of glazing in the churches are a rich source of imagery for artworks. Old advertising signs and the names of pioneering families and street signs can be inserted within artworks as layers within a contemporary refurbishment.



Old Post Office building, Church Street

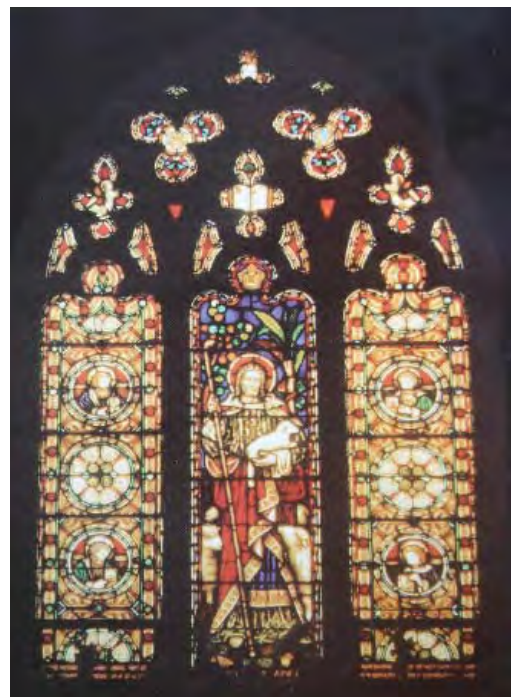


### St Anne's Church

St Anne's Church and associated school appeared in old sketches of the area as a signpost for Ryde. Prior to the expansion of buildings, St Anne's Church was the most visible landmark from every direction within the region. Its continued occupation by the Church makes it the most intact precinct in the Ryde Town Centre area, existing alongside contemporary uses. The influence of the church and its school upon Ryde's identity should be referenced through artworks.



St Annes Church, Church Street



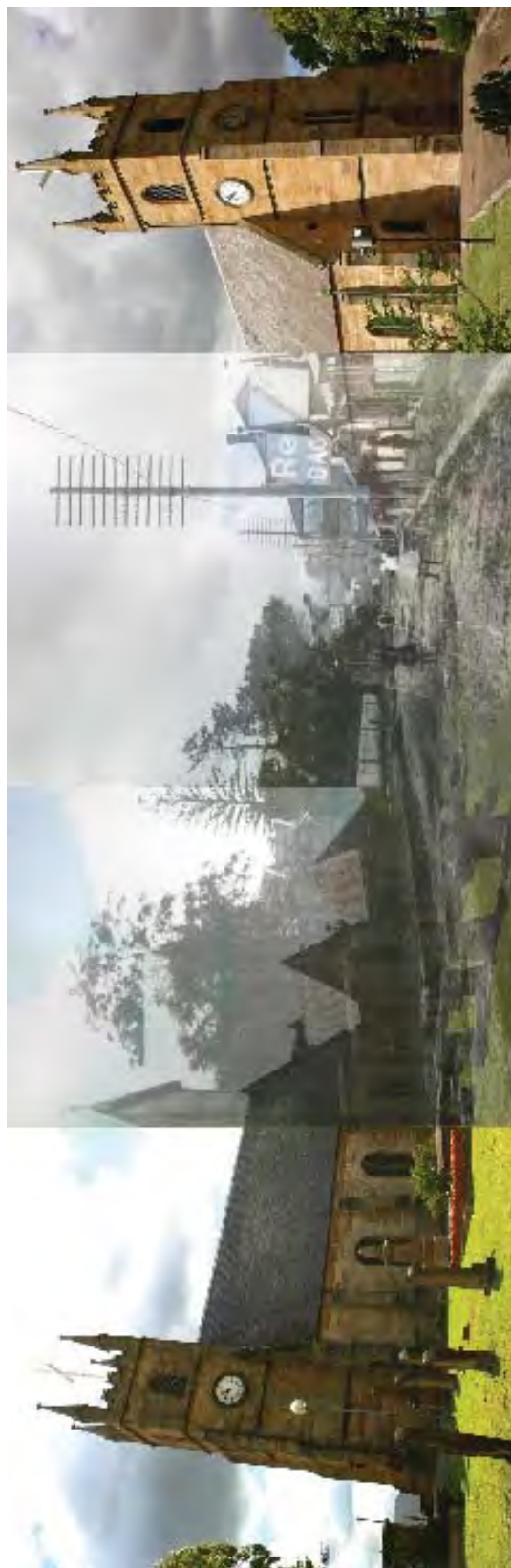


Photo montage; St Annes Church and overlay of historic Church Street photo c1912 by Rex Hazlewood  
By Milne and Stonehouse

### High Point

Located on a ridgeline, Ryde occupies a high point in the landscape. Historically it commanded views over the surrounding landscape and down to the Parramatta River, and was also highly visible from distant regions. While the development of the town has lessened the impact of this high point, significant views from the heritage precinct around St Anne's Church are still available and should be enhanced through artwork and signage for visitors.



Photo montage by Milne and Stonehouse; modern day view of the high point at the junction of Victoria Road and Devlin Street looking towards Parramatta and the Parramatta River.

Historic view of James Devlin's House 'Willandra' as sketched by colonial artist Henry Curzon Allport, August 1841.



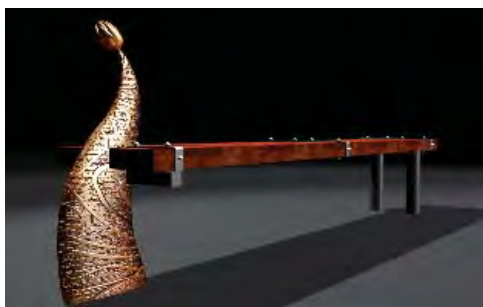
## 5.7.5 Opportunities for Public Artworks

### Integrated Approach

Artworks can be integrated into the fabric of building refurbishments to create an integrated element in the space. The constricted space of Ryde's streetscapes will accept these kinds of artworks readily without impeding the flow of users. The impact of these artworks can be subtle or powerful, small or large in scale, depending upon the materials used and colour palette selected.

### Street Furniture and Bollards

Imagery from the architectural elements of the heritage buildings and churches could be translated into the frames of street furniture, or the shape and texture of bollards. Street furniture as a motif, with individual cast bollards, could give Ryde a signature as a unique place, blending the historical with the contemporary.



Artwork furniture elements designed by Urban Art Projects



## Pavement

Small areas such as cafes, corners and pausing points are suitable for the incorporation of stencilled, terrazzo or sandblasted images and inserted text. These elements can be based on an articulation of the specific themes identified in Section 5.7.3, and may include such possibilities as the insertion of old advertising signs and the names of pioneering families and street signs into the pavement. The colours of the glazing from St Anne's Church may be translated into a bright flourish of terrazzo tiles.



Carrington Place Katoomba - Artist Planners - Milne and Stonehouse



Carrington Place Katoomba - Artist Planners - Milne and Stonehouse

### Lookout

A lookout in the vicinity of the St Anne's Church will reinforce the historically commanding position of Ryde in the colony and capitalise on the sweeping views. Artworks should be interactive and interpret multiple views from early Nineteenth Century sketches to the dense developments of the present day. Such images may take the form of digital interactive screens or result from a collaboration with a landscape architect in the design of the lookout itself.

### Intersections and Access Points

The articulation of key intersections and entries, such as the Blaxland Road and Victoria Road intersection, will create a sense of gateway entrance into the Ryde Town Centre and overcome the current disjointed approaches. This may be achieved through site specific artworks, colourful annual plantings and, in the case of Blaxland and Victoria Roads, refurbishment of the existing, historic fountain.

The overpass at the intersection of Victoria Road and Devlin Street also provides a significant opportunity for creation of a gateway into Ryde, for example through the incorporation of artworks into fencing or as a stand-alone element. This gateway may enhance the sightlines reinforced by the proposed lookout.



Elizabeth Street footbridge Parramatta. Artworks by Milne and Stonehouse



**Figure 5.7 - Proposed Artwork Locations / Opportunities**



## LEGEND

Note: Artworks locations are indicative only and will be finalised during the design process

- |  |   |
|--|---|
| <span style="display: inline-block; width: 20px; height: 10px; background-color: red; border: 1px solid black;"></span> Pavement artworks in areas of major streets (town core). Themes to reflect river connections, fruit bowl, heritage. Possibly street furniture and bollards | <span style="display: inline-block; width: 0; height: 0; border-left: 5px solid transparent; border-right: 5px solid transparent; border-bottom: 8px solid blue;"></span> High points - interpret views to Parramatta River in artworks |
| <span style="display: inline-block; width: 20px; height: 10px; background-color: green; border: 1px solid black;"></span> St Annes Church precinct.  | <span style="display: inline-block; width: 10px; height: 10px; background-color: lightblue; border-radius: 50%; border: 1px solid black;"></span> Major art installation - Urban Plaza, Top Ryde Shopping Centre                        |
| <span style="display: inline-block; width: 20px; height: 10px; background-color: yellow; border: 1px solid black;"></span> Pedestrian overbridges - artwork opportunities  | <span style="display: inline-block; width: 10px; height: 10px; background-color: black; border-radius: 50%; border: 1px solid black;"></span> Lookout - expansive views over the district. Historic interpretation.                     |
|  | <span style="display: inline-block; width: 10px; height: 10px; background-color: pink; border: 1px solid black;"></span> Key entry points - signify with artworks.  |

## 5.8 Streetscape Concept Images

### Planting Treatments



Existing



Proposed

Devlin Street - median strip gateway planting of Cabbage Tree Palms with ornamental native grasses to the base



Existing



Proposed

Smith Street - secondary paving treatment with 1.5m wide footpath and street tree planting

## Paving Treatments



Existing



Proposed

Blaxland Road, Argyle Centre - major paving treatment with allowance for outdoor dining



Existing



Proposed

Blaxland Road - major paving treatment with paving features



## Roadway Treatments



Existing



Proposed

Tucker Street - raised pedestrian threshold and major paving treatment



Existing



Proposed

Belmore Lane - laneway treatment with cobblestone threshold

## Outdoor Dining Treatment



Existing

Church Street - major paving treatment and proposed outdoor dining area with timber decking and raised planter boxes



Proposed

***This section presents the costings for the public domain plan. These costings are based on the Concept Designs developed in Section 5. The costings provide Council with an estimate for the works enabling these estimates to guide developer contributions, section 94 plans, and Councils budget forecasts.***

## 6.1 Introduction

The costings provided with this public domain plan are an opinion of probable cost only. Further detailed design work will need to be undertaken to provide a final figure for the proposed works.

These costings do not include consultants fees, project management, survey, tendering or contract administration. These figures will assist Council to provide budget forecasts for proposed projects, to provide input into the Section 94 plans for the town centre and input into the development application process with developer contributions to the proposed works.

The final figures quoted do not take into account the contribution of private developers in the implementation of the works within the public domain. Developer contributions would reduce these overall figures. For example it would be expected that the developers of Top Ryde Shopping Centre would upgrade and pay for the proposed public domain of the street frontages surrounding the shopping centre, namely Pope Street, Tucker Street, Blaxland Road and Devlin Street.

The council should endeavour to promote the public domain plan improvements by initiating a pilot project that can showcase these improvements. With the impending development of the Top Ryde Shopping Centre Precinct it would be advantageous to develop a retail and dining hub with the redevelopment of Church Street. This would create an active eat street and provide a focal point for the people of Ryde. This would also help to reconnect Ryde to the historical precinct of St. Annes Church.

The costings have been separated into major and secondary streets in accordance with Section 4 of the report, Urban Design Framework. Refer to the tables on pages 92 - 94 for the summary of costs.

For maintenance of the proposed works it is suggested that 3% of the total construction costs will be set aside each year to cover the cost of maintaining the works at a high standard. This maintenance cost could be incorporated into a special rate for the LEP area.



## MAJOR STREETS

## Proposed Works

	Demolition	Pavement work	Paving	Furniture	Arboretum	Pedestrian Crossing	Outdoor seating (patio area)	Lighting	Signage	Furniture	TOTAL
Streets											
POPE ST.	\$20,000	\$1,050,000	\$50,000	\$15,000	\$250,000	\$150,000	\$250,000	\$280,000	\$17,000	-	\$2,002,000
TUCKER ST.	\$20,000	\$1,444,500	\$150,000	\$10,000	-	\$55,000	-	\$350,000	\$0,000	-	\$2,055,500
BLAND RD. (EAST OF DEVILIN ST.)	\$50,000	\$2,745,900	\$310,000	\$30,000	\$150,000	-	\$1,500,000	\$300,000	\$24,000	-	\$5,709,900
CHURCH ST.	\$30,000	\$1,170,000	\$750,000	\$30,000	\$250,000	-	\$700,000	\$400,000	\$5,000	-	\$2,745,000
BLAND RD. (WEST OF DEVILIN ST.)	\$15,000	\$010,000	\$350,000	\$30,000	\$250,000	-	-	\$550,000	\$20,000	-	\$2,025,000
DEVILIN ST. TO POPE ST. TO BLAND RD.)	\$10,000	\$540,000	\$700,000	\$30,000	\$150,000	-	-	\$400,000	\$15,000	\$3,500,000 – less Indebts	\$4,064,000
SUBTOTAL											\$19,463,400
20% CONTINGENCY											\$3,896,680
TOTAL											\$23,360,080

Description	Planting	Arboreta	Therapeutic	Viewing Platform	Furniture	Lighting	Signage	Fountain/Maze	Outdoor string pathway (wintering)	Restroom/ existing	TOTAL
Streets											
CLARKSON ST.	\$3,840	-	-	-	-	\$280,000	-	-	-		\$294,040
SOUTH ST.	\$6,400	-	-	-	-	\$200,000	\$14,000	-	-		\$346,860
WATT AVE.	-	-	-	-	-	-	-	-	-		\$19,000
ARGYLE AVE.	\$23,640	-	-	-	\$10,000	\$760,000	\$29,000	-	\$250,000	\$55,000	\$1,609,640
PRINCES ST.	\$12,240	-	-	-	\$10,000	\$440,000	\$25,000	-	-	-	\$543,240
HATTON ST.	\$3,240	-	-	-	-	\$180,000	-	-	-	-	\$306,440
WILLIAM LANE	-	-	\$12,000	-	-	-	-	-	-	-	\$12,000
WILLIAM ST.	\$11,280	-	-	-	-	\$320,000	-	-	-	-	\$528,480
EDWARD ST.	\$9,600	-	-	-	-	\$200,000	-	-	-	-	\$345,600
ESOMIE ST.	\$8,520	-	-	-	-	\$240,000	-	-	-	-	\$404,320
ST ANNES ST.	\$2,760	-	-	-	-	\$180,000	-	-	-	-	\$291,360
LITTLE CHURCH ST.	\$2,880	-	-	-	-	\$225,000	\$6,000	-	-	-	\$353,880
CHURCH LANE (WEST OF CHURCH ST.)	\$300	-	\$600	-	-	-	\$6,000	-	-	-	\$52,300
CHURCH LANE (VIEWING PLATFORM)	-	-	-	\$250,000	-	\$50,000	\$6,000	-	-	-	\$506,000

SECONDARY STREETS

	Description	Preliminary works	Planting	Artworks	Therapeutic	Viewing platforms	Furniture	Lighting	Signage	Foodstalls	Outdoor Dining (patio area)	Pedestrian crossing	TOTAL
Streets													
TURNER ST.	\$500	\$29,000	\$40,000	-	-	\$40,000	-	-	-	-	-	-	\$ 109,500
VICTORIA RD.	\$16,320	\$293,760	\$50,000	\$150,000	-	-	-	\$240,000	\$45,000	\$1,500,000	-	-	\$2,905,080
BELMORE LANE	-	\$70,000	-	-	\$35,000	-	-	-	-	-	-	-	\$ 105,000
CHURCH LANE (WEST OF DEVLIN ST.)	-	\$16,000	-	-	\$24,000	-	-	-	-	-	-	-	\$40,000
BLAKLAND LANE	-	\$17,000	-	-	\$24,000	-	-	-	-	-	-	-	\$41,000
BELMORE ST.	\$10,000	\$151,000	\$35,000	-	-	-	-	\$200,000	-	-	-	-	\$796,000
DEVLIN ST. (BLAKLAND RD TO VICTORIA RD)	\$22,000	\$310,000	\$40,000	-	-	-	-	\$350,000	\$2,000	-	-	-	\$925,000
SUBTOTAL													\$11,018,620
20% CONTINGENCY													\$2,203,724
TOTAL													\$13,222,344



*This section outlines a proposed program of planning, design and construction works that will support the long-term implementation of the recommendations made within this Public Domain Plan.*

## 7.1 Introduction

The guiding visions and recommendations contained within this Plan form a basis from which detailed design works, public consultation, sponsoring / partnering arrangements, and funding sources may be established for the long-term implementation of public domain improvements.

Achievement of the objectives and measures identified in the Plan will require a coordinated management approach by City of Ryde, and on-going commitment to a program of construction, trialling, maintenance, review and improvement.

The staging and implementation of proposed works will be influenced by:

- availability and allocation of funding for maintenance and new works,
- formation of partnerships with representatives from the corporate sector and / or state and federal government organisations,
- community expectations and engagement,
- political and socio-economic forces at the state and local level,
- Council priorities, for example, the need to address risks and liabilities such as uneven pavement surfaces, and
- major construction works.

## 7.2 Staging

A combination of short and long-term measures should be established for the implementation of public domain improvements. It is envisaged that short-term measures will address immediate priorities and form a basis for on-going or future works. Proposed measures include:

### Short-term (1 - 3 years)

- Coordinate outcomes of this Public Domain Plan with the redevelopment of the Top Ryde Town Centre.
- Undertake a pilot project for paving and the selection of granite colour and finishes.
- Incorporate outcomes of this Public Domain Plan into Council's Local Environment Plan and Development Control Plan for the town centre to ensure consistent approaches and enforceable requirements for developers to contribute to the installation of public domain finishes at the frontage of new developments.
- Investigate traffic improvement and pavement widening options along Blaxland Road and Church Street to enable outdoor dining.
- Improve pedestrian amenity access and equity along key streets including Church Street, Blaxland Road, Tucker Street and Devlin Street by developing a detailed masterplan for the town centre that includes proposals for paving, lighting, outdoor dining and street tree planting.

- Undertake the preparation of a signage manual to guide the design and implementation of future signage throughout the public domain.
- Undertake the preparation of a lighting manual to guide the type and location of proposed lighting within the public domain.
- Undertake a tendering process to select a preferred contractor for the supply, installation and on-going maintenance of paving materials.
- Develop a paving policy for the town centre that establishes detailed locations for various paving treatments and contains details relating to material selection, supply and installation.
- Develop design concepts and documentation of key areas such as the public domain surrounding Top Ryde Shopping Centre and the associated Civic Centre, Blaxland Road and Church Street.
- Develop a public art installation plan and commission a major installation in a prominent area of the public domain, such as the civic precinct proposed to be adjacent to the Top Ryde Shopping Centre redevelopment.

#### **Long-term (3 - 5 years)**

- Design and document improvements to a key public area, such as the surrounds of Ryde Park, introducing secondary paving treatments and street tree planting.
- Implement traffic devices that encourage a pedestrian-friendly environment in Blaxland Road, including provision for outdoor dining.
- Implement measures for pedestrian linkages between Ryde Park, Ryde Public School and the Top Ryde Shopping Centre.
- Redevelop the Argyle Centre as a cultural precinct, including provision for outdoor dining.
- Design and document plans for the historic precinct around St Anne's Church and adjoining historic buildings in Church Lane and Turner Street that will include proposals for interpretive artworks, a lookout facility and paving and lighting works.

## A - Lighting

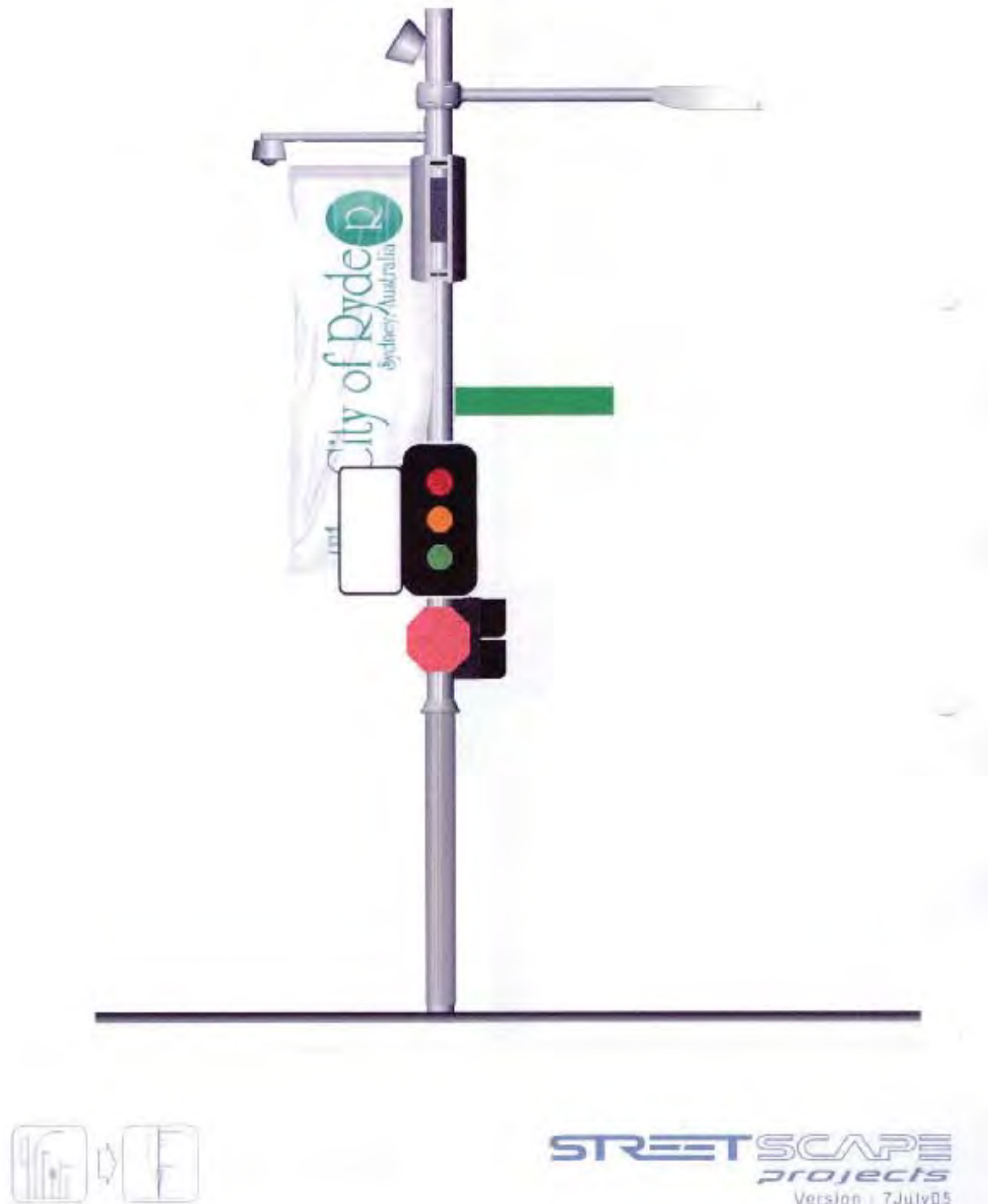
## S1B 9.6m Smartpole



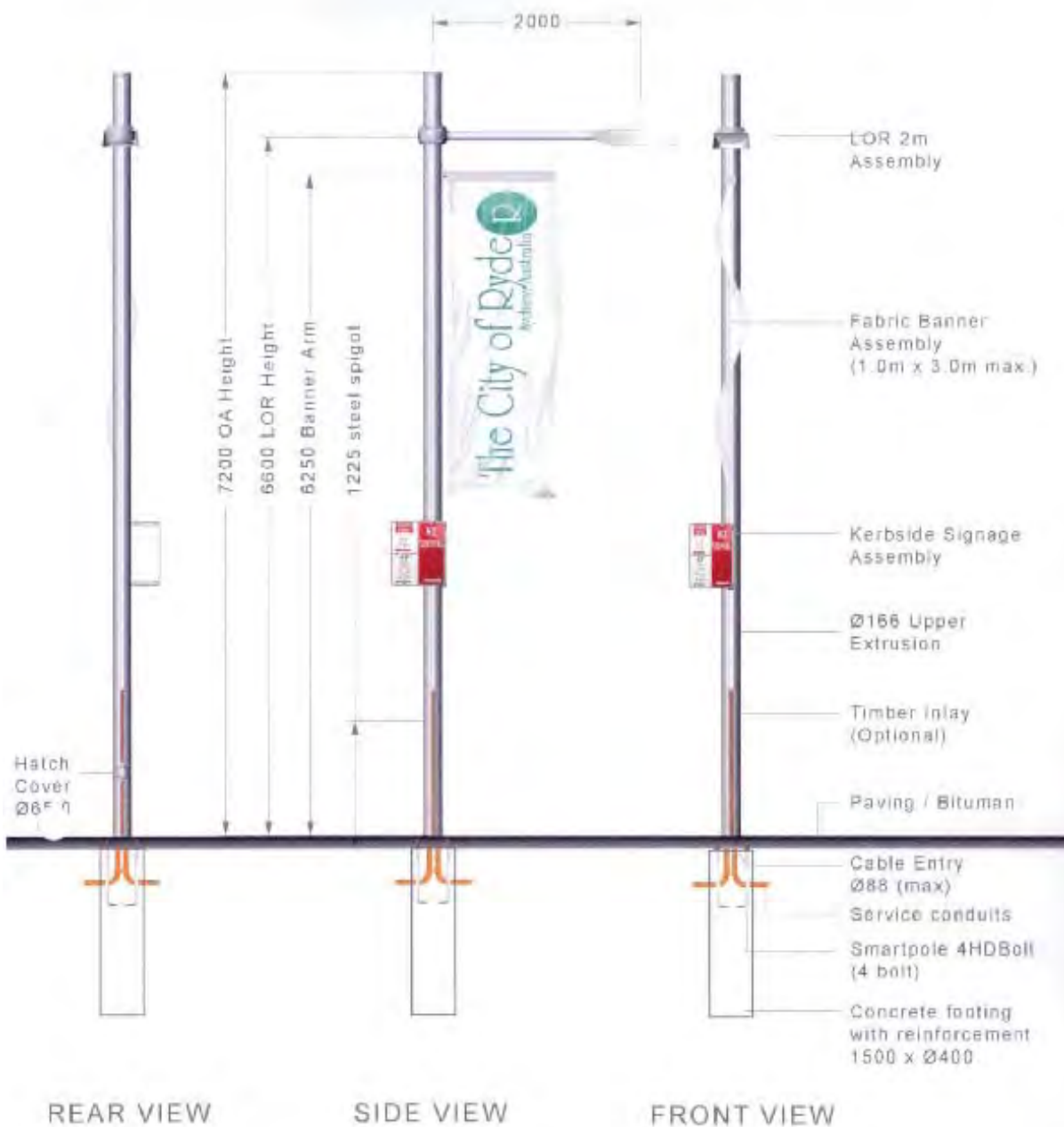
**STREETSCAPE**  
projects  
Version - 7 July 05



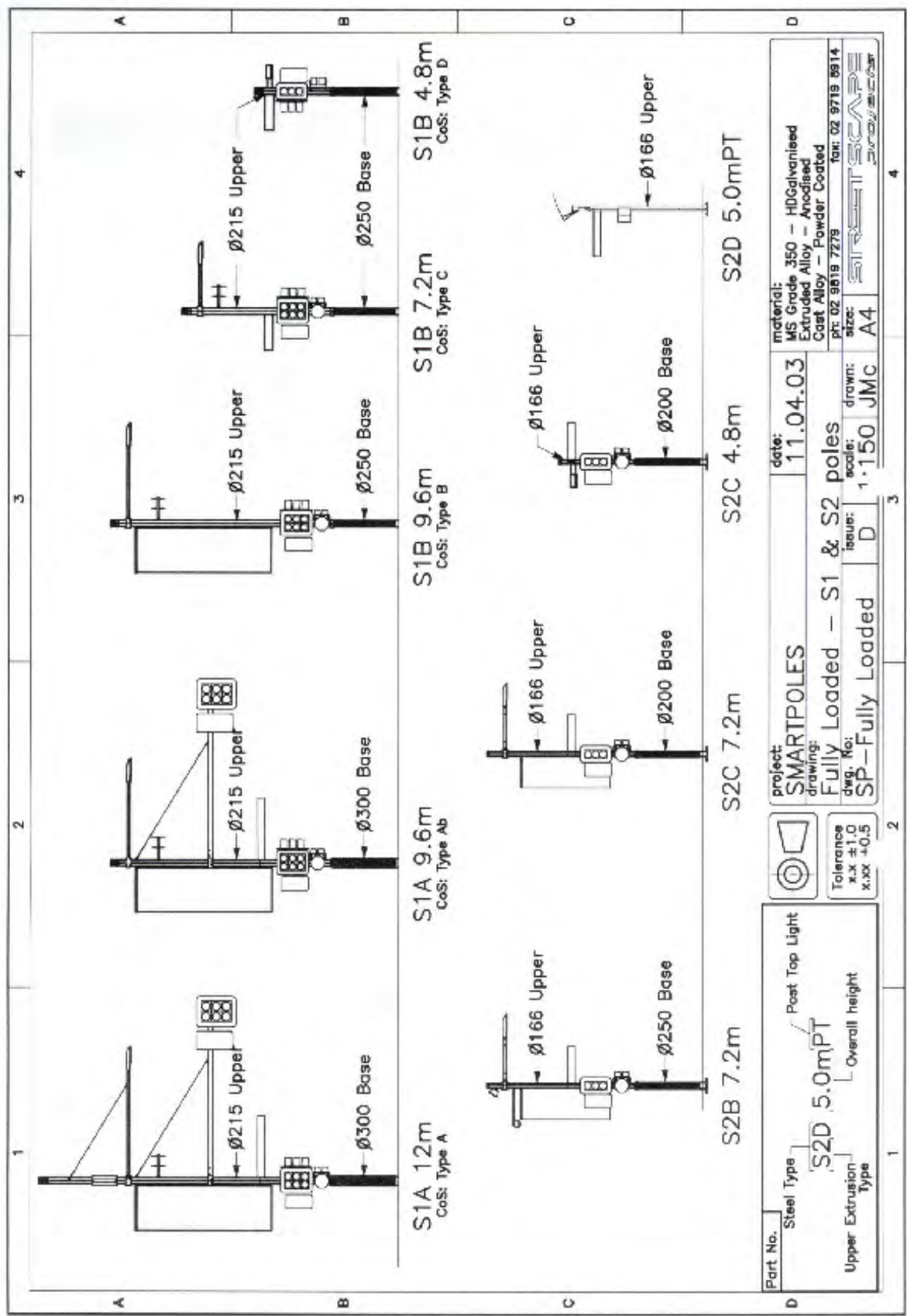
## S2C 7.2m Smartpole



## SZU 7.2m Low Impact



**STREETSCAPE**  
projects  
Version 7 July 05





## B - Executive Summary Ryde Park Masterplan

### BACKGROUND:

Ryde Park is a key civic space in the Top Ryde Precinct located close to major transport routes including Blaxland Road, Victoria Road and Lane Cove Road. The park contributes a significant portion of open space within the Ryde precinct. It is positioned in close proximity to the Top Ryde Shopping Centre, near the junction of two (2) regional roads, Victoria Road and Devlin Street, and at the edge of the Buffalo Creek catchment with its links to the Lane Cove River.

Ryde Park accommodates a variety of facilities and uses including cricket, soccer, rugby union, rugby league; heritage items; mature trees; playground; half-court basketball; amenities buildings; car parking; seats; lights and footpaths.

The Top Ryde Urban Village study indicates that there is the potential for an increase in the residential population surrounding Ryde Park by up to three thousand people. This future demand shall provide further pressure on the functioning of this park.

Additionally, the assets in the park are old and in need of upgrade, particularly in relation to furniture, pathways and the internal road system. The Old Ignatians Rugby Football Club (OIRFC) is also proposing significant improvements for the building and the main (playing field number one) sports oval.

Councillors adopted a Masterplan indicating upgrades to Ryde Park in July 2005. This Masterplan was based on a thorough analysis of the existing park, a review of historical documents pertaining to the use and design of the park and consultation with park users and the local community to determine their needs in open space recreation. The following information regarding the Masterplan is from the final Masterplan report.

**AIMS AND OBJECTIVES:****PURPOSE OF THE MASTERPLAN PROJECT**

The overall aim of the Masterplan was 'to enhance and improve Ryde Park with attention to its original design which over the years may have become diluted through numerous small alterations and additions'. The Masterplan is to become the framework for upgrades and future use of Ryde Park.

**OBJECTIVES**

The objectives for this project were defined as follows:

- To conserve the clarity of the original design of Ryde Park.
- To achieve a clear link between Top Ryde and Ryde Park.
- To protect and interpret the historic values and features of Ryde Park for visitors and the local people alike.
- To encourage the local residents and visitors to Ryde to use Ryde Park.
- To optimise the natural values of the park, which will in turn improve the experience for those who frequent it.
- To maximise Ryde Park's profile within the local community.
- To ensure an acceptable level of safety is provided for all who frequent Ryde Park.
- To provide seating and amenities appropriate to the character and heritage values of the park.
- To optimise use of all areas of the park.
- To ensure major routes through the park are universally accessible.
- To provide opportunities for a greater variety of usage, relevant to the local residents and stakeholders through community consultation.

**KEY ISSUES AND OPPORTUNITIES**

A number of issues and opportunities have become evident during the compiling of the Masterplan report. The major ones relate to:

- Ryde Park's profile within the immediate and larger community.
- Pedestrian and vehicular access and circulation throughout the park.
- The need for a greater awareness to be paid to equal access across the park.
- The need for safety issues to be addressed.

## RYDE PARK MASTERPLAN

### KEY PROPOSALS

The Masterplan proposes the following to occur in Ryde Park:

- A clear entrance on the corner of Blaxland Road and Princes Street announces Ryde Park to those driving from Victoria Road onto Top Ryde, allowing expansive views across Oval One (1) and glimpsed views across the park. Traffic issues relating to the Blaxland and Victoria Road intersection are beyond this scope of works and shall be considered at a future stage.
- Entrances have been formalised to give Ryde Park a greater presence in the locality and an enhanced sense of arrival.

The link road through the park has been removed and returned to a footpath and cycleway only. All vehicular circulation works independently. This link path is intended to be included as a jogging/ fitness route. A direct cycleway route is proposed from Princes Street through to Argyle Avenue.

- Investigations regarding a link to Top Ryde from Ryde Park shall be considered in the future Local Environment Plan (LEP).

- A central focal space connects the different areas of activity within the park whilst a new pavilion placed adjacent provides a built focal element, which houses the new amenities. It shall be designed with drainage issues in mind

The existing Playground and basketball court are relocated closer to the new central space to avoid dividing the park.

- The heritage listed Rotunda is emphasised by colourful floral displays and an opened expanse of lawn.

- Extensive native tree plantings emphasise the link to the remnant Blue Gum High Forest.

- A new picnic/ recreation area replaces the disused hockey field and allows for unstructured activity and relaxation. It shall be a flexible space in regard to activity use, avoiding noise impacts on neighbouring residents. A possible activity for this space could be a kid's bike track.

- Safety issues such as the seating atop steep slopes have been eliminated through careful siting, whilst close attention has been paid to ensure all paths throughout the park are universally accessible wherever possible.

- Toilets in Ryde Park will address the issue of loiterers by being visually prominent.

Car parking has been moved to the periphery of the park, allowing for a pedestrian focused park, free of vehicle congestion. The number of parking spaces remains the same (84 spaces). Design options shall be investigated further in relation to screening the car park from adjoining neighbours and negating anti-social issues.

- Decking is inserted between the Bowling Club and Ryde Park to act as an intermediary space to link these areas.

- The concrete grandstand foundation on the southern side of the oval will be removed and this seating will be replaced around the current pavilion with a set of grassed terraces stepping down to the oval.

- Drainage issues to be addressed in the general upgrade of Ryde Park. As overland stormwater flows through Ryde Park, safe egress and access through these zones is essential.



## IMPLEMENTATION OF MASTERPLAN

The adopted Masterplan has been broken down into stages to allow for a realistic approach to the implementation of the proposed upgrades.

### STAGE 1

The following components of the masterplan can be implemented in the short term.

1. Finalise Plan of Management
2. The formalisation of entries
3. The main link path is established from Princes Street to Argyle Avenue with minor associated paths
4. Rotunda area, paths and associated planting
5. New passive recreation area including barbeques and associated planting
6. Realigning oval to become more symmetrical
7. Native tree planting is undertaken
8. Nursery relocated and area integrated into the park
9. Link to Top Ryde including negotiations with School and Department of Education and Training shall be investigated in future LEP.
10. Upgrade Blaxland Road frontage
11. Major upgrade to main oval including drainage improvements, regrading etc.

### STAGE 2

The following components of the masterplan can be implemented within the medium term:

1. Central arrival space and pavilion containing new amenities
2. Remove vehicular access currently cutting through the centre of the park and relocate car parks to more accessible space on the periphery
3. Relocation and expansion of playground and basketball court
4. Potential to engage upgraded paths within the park to a pedestrian link to Top Ryde
5. Removing concrete grandstand foundation and replacing with a grassed terrace to engage with stairs leading from the new entry point at the southern corner of the park and the upgraded pavilion
6. Improved visual integration of Bowling Club with park, including construction of platforms between Bowling Club and the main oval for improved access and oval viewing.
7. Commence proceedings to purchase property (no. 28 Argyle Ave) adjoining existing park entry
8. Consider options in relation to the integration of Council owned properties into the park

**STAGE 3**

The following components of the masterplan may be long term goals for the park and implementation of these ideas will result in a more cohesive park:

1. Investigate a greater connection between the built form of the bowling greens and Ryde Park
2. Access into Ryde Park should engage with any redevelopment of the Top Ryde commercial precinct

# staging



## STAGE 1

The following components of the masterplan can be implemented in the short term.

1. Finalise Plan of Management
2. The formalisation of entries
3. The main link path is established from Princess Street to Argyle Avenue with minor associated paths
4. Run-down area, paths and associated planting
5. New passive recreation area including barbecues and associated planting
6. Realigning oval to become more symmetrical
7. Native tree planting is undertaken
8. Nursery relocated and area integrated into the park
9. Link to Top Ryde including negotiations with School and Department of Education and Training shall be investigated in future LEP.
10. Upgrade Blaxland Road frontage
11. Major upgrade to main oval including drainage improvements, regarding etc.



## STAGE 2

The following components of the masterplan can be implemented within the medium term:

1. Central arrival space and pavilion containing new amenities
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8. Consider options in relation to the integration of Council owned properties into the park



## STAGE 3

The following components of the masterplan may be long term goals for the park and implementation of these ideas will result in a more cohesive park:

1. Invest gate a greater connection between the built form of the bowling greens and Ryde Park
2. Access into Ryde Park should engage with any redevelopment of the Top Ryde commercial precinct



2006 City of Ryde - 2006  
Ryde Park Masterplan  
Prepared by:  
CLOUSTON ASSOCIATES  
10/06/06

6

## Ryde Park Landscape Masterplan

NS 756 Sk 6 • June 2005 Issue b



# masterplan

## KEY PROPOSALS

- A clear entrance on the corner of Black Lane Road and Princes Street announces Ryde Park to those driving from Victoria Road onto Top Ryde, allowing expansive views across Oval One (1) and glimpsed views across the park. Traffic issues relating to the flux and Victoria Road intersection are beyond the scope of works and shall be considered at a future stage.
- Entrances have been formalised to give Ryde Park a greater presence in the locality and an enhanced sense of arrival.
- The link road through the park has been removed and returned to a footpath and cycleway only. All vehicular circulation works independently. This link path is intended to be included as a jogging, fitness route. A direct cycleway route is proposed from Princes Street through to Argyle Avenue.
- Investigations regarding a link to Top Ryde from Ryde Park shall be considered in the Future Local Environment Plan (LTP).
- A central focal space connects the different areas of activity within the park whilst a new pavilion placed adjacent provides a built focal element, which houses the new amenities. It shall be designed with drainage issues in mind.
- The existing playground and basketball court are relocated closer to the new central space to avoid dividing the park.
- The heritage listed Rotunda is emphasised by colourful floral displays and an opened expanse of lawn.
- Extensive native tree plantings emphasise the link to the remnant Blue Gum High Forest.
- A new picnic/recreation area replaces the disused hockey field and allows for unrestricted activity and relaxation. It shall be a flexible space in regard to activity use, avoiding noise impacts on neighbouring residents. A possible activity for this space could be a kid's hockey track.
- Safety issues such as the seating atop slopes have been eliminated through careful siting, whilst close attention has been paid to ensure all paths throughout the park are universally accessible wherever possible.
- Toilets in Ryde Park will address the issue of toilets by being visually prominent.
- Car parking has been moved to the periphery of the park, allowing for a pedestrian focused park, free of vehicle congestion. The number of parking spaces remains the same (84 spaces). Design options shall be investigated further in relation to screening the car park from adjoining neighbours and negating anti-social issues.
- Docking is inserted between the Bowling Club and Ryde Park to act as an intermediary space to link these areas.
- The concrete grandstand foundation on the southern side of the oval will be removed and this seating will be replaced around the current pavilion with a set of grassed terraces stepping down to the oval.
- Drainage issues to be addressed in the general upgrades of Ryde Park. As overland stormwater flows through Ryde Park, safe egress and access through these zones is essential.



## Ryde Park Landscape Masterplan 3

NS 756 SK 3 • June 2005 Issue b



2005 Green City - 2006  
Ryde Park - 2007  
Ryde Park - 2008  
Ryde Park - 2009

100% Green City - 2006  
100% Green City - 2007  
100% Green City - 2008  
100% Green City - 2009



## 8. Small and Neighbourhood Centres

### 8.1 Public Domain Requirements

#### 8.1.1 Introduction

This section details the public domain elements required in Small and Neighbourhood Centres. The secondary purpose of this section is to provide guidance to owners on how new developments should respond to public domain requirements in Small and Neighbourhood Centres. All new developments should incorporate public domain and streetscape improvements.

#### Background

Small centres provide convenient access to a number of services and are generally located on busier local roads. They are used by passing motorists as well as local residents. Neighbourhood Centres are generally smaller and provide a limited number of services. They often provide affordable rental and a convenient location for small offices rather than shops.

In these centres there is an opportunity to make significant small scale changes to improve the public domain.

#### 8.1.2 Scope

The areas covered in this section are noted below.

Small Centres	Neighbourhood Centres
Cox's Road, North Ryde	Agincourt Rd, Marsfield
Putney village, Putney	Avon Rd, North Ryde
Boronia Park, Gladesville	Allars St, Denistone West

Blenheim Road, North Ryde	Blaxland Rd, Denistone East
Fiveways, Eastwood	Bowden St, Ryde
Meadowbank Station West, Meadowbank	Callaghan St, North Ryde
Midway, Ryde	Cobham Ave, Melrose Park
Trafalgar Place, Marsfield	David St, North Ryde
Balaclava Road, Marsfield	Doig Ave, Denistone East
	Epping Rd (cnr Paul St), North Ryde
	Flinders Rd, North Ryde
	Pittwater Rd, Gladesville
	Quarry Rd, Ryde
	Sager Pl, East Ryde
	Tennyson Rd, Putney
	Victoria Rd, Ryde
	Watts Rd, Ryde



### 8.1.3 Design Principles for Small and Neighbourhood Centres

#### 8.1.3.1 Destinations and Links

Small and Neighbourhood Centres are often local destinations and this attribute should be supported to ensure the long term viability of the centres. In addition links between these centres and other local destinations should be strengthened. It is important to recognise that the key assets of these retail centres are access and convenience. Therefore, suitable traffic flow and convenient parking are critical. Other features that can support a centre as a destination can include one or more of the following:

##### Open spaces

Open spaces may include urban parks, playgrounds, communal squares, widened footpaths, and retail courtyards.

##### Active Streets

Street activation occurs where land use, built form and public domain combine to attract people to an area. There are a number of land uses that encourage street activation including retail outlets, outdoor dining and community facilities. Building form can assist in street activation if the building is attractive and provides interest. At street level the shopfront should make people feel safe and welcome, and the public and private zones should be integrated. To achieve this, the shopfront should be visually open to the street as shown in the example below.



Activated shopfront

##### Community Services

The pedestrian connections will be located to access community amenities. These can be public institutions such as a school, library and community hall but equally these can be cafes, newsagents, medical practitioners, bakeries and so on that service the local community.

### **Pedestrian Access, Safety and Amenity**

Small and Neighbourhood centres are accessed by pedestrians because of their convenient location (although most trips are by car). There are a number of opportunities to extend the pedestrian network and improve the pedestrian amenity in Small and Neighbourhood Centres. New and upgraded pedestrian pathways will:

- Give local pedestrians better access to shops.
- Provide stronger links to other local facilities such as parks and playgrounds.
- Create more consistent, better quality public spaces around Small and Neighbourhood Centres.

### 8.1.4 Required Public Domain Elements

Improving the quality and consistency of the streetscape has a direct and immediate benefit for the public domain. The scope and general requirements for street trees, paving and street furniture are noted below. Refer to Council's Public Works Department for detailed specifications of these items. The detailed specifications should be considered when preparing development application documents.

#### Street trees

Street streets are required to Council's satisfaction for all new developments. Trees should be set out at approximately 8m centres. This spacing will depend on species chosen and the presence of obstructions such as driveways.

Street trees can have several environmental, climatic and aesthetic benefits. In Small and Neighbourhood centres trees should generally be selected on the basis of their form (aesthetics) and impact on the micro-climate (eg shade in summer, sun in winter).

Tree planting in Small and Neighbourhood centres should reflect the predominant existing street tree planting. Where there is no predominant existing street tree planting or the existing species are not appropriate Council will provide an alternative species.



Existing Street Trees – Doig Avenue



## **Paving**

The following paving types shall be provided to Council's satisfaction for all new developments:

1. In the Small Centres granite paving (Paving Type 2) will be used.
2. In the Neighbourhood centres pigmented concrete paving will be used.



Granite Paving (Paving Type 2) - Meadowbank

## **Street furniture**

The following street furniture shall be provided to Council's satisfaction for all new developments:

- Seating (Seat 01 and/or Bench 01)
- Bins (Bin 01)
- Bollards (Bollard 02)
- Bus shelters

Additional street furniture that may be considered includes:

- Tables
- Bike racks (01 or 02)
- Drinking fountains
- Café barriers

Refer to Council's Public Works Department for specific models and detailed specifications such as material, finish and installation.

## Environmental performance

The environment performance can be enhanced by using the following when selecting and detailing materials. For example:

- Recycled materials
- Functionally and aesthetically long lasting materials
- Materials that require minimal maintenance

Water Sensitive Urban Design (WSUD) should be incorporated into the design of public spaces.



Street trees integrating WUSD principles - Meadowbank

## Public Art

Public art can make an important contribution to the local community in a number of ways and is supported in Small and Neighbourhood centres. Public art should achieve one or more of the following:

- Provide visual enjoyment and interest to everyday activities and social gatherings.
- Highlight and enhance the local character of an area.
- Create a point of reference for the local community and visitors.

Artworks can be integrated with buildings and landscaping elements or be stand-alone objects. Opportunities exist to include custom made, artist designed furniture that will celebrate specific aspects of local character.

Semi-public art work associated with individual buildings is encouraged. The art can be in the form of art walls, screens or other building elements. It can provide exposure for the particular building but also interest for the community.





Integrated Public Art – Fiveways